

# HISTORIC PRESERVATION COMMISSION MEETING

CITY OF DAVENPORT, IOWA

TUESDAY, JANUARY 10, 2023; 5:00 PM

CITY HALL | 226 WEST 4TH STREET | COUNCIL CHAMBERS

## I. Call to Order

## II. Secretary's Report

- A. Consideration of the October 11, 2022 meeting minutes.

## III. Communications

## IV. Old Business

## V. New Business

- A. Case COA23-01: Request for exterior alteration at 624 West 5th Street. The Charles and Amelia (Ruch) McElroy House is located within the Hamburg Local Landmark Historic District. Duane Timm, petitioner. [Ward 3]
- B. Case COA23-02: Installation of accessible playground equipment at Vander Veer Park at 223 West Central Park Avenue. Vander Veer Park is a locally listed historic landmark. City of Davenport, petitioner. [Ward 5]
- C. Case COA23-03: Request for exterior alteration at 521 West 6th Street. The Dr. Gustav Hoepfner House and Office is located within the Hamburg Local Landmark Historic District. ILIOWA Holdings LLC, petitioner. [Ward 3]
- D. Case COA23-04: Request to install new garage doors at 625 West 7th Street. The Hans Goos House is located within the Hamburg Local Landmark Historic District. Brett Lutz, petitioner. [Ward 3]
- E. Case NRHP23-01: Review and provide a recommendation for the International Harvester Truck Sales and Service Station at 601 West 2nd Street to be listed on the National Register of Historic Places. [Ward 3]

## VI. Other Business

- A. Please Note: The February 14th and March 14th Historic Preservation Commission meetings will be held in the Davenport Police Department Community Room, 416 N. Harrison Street.

## VII. Open Forum for Comment

## VIII. Adjourn

## IX. Next Commission Meeting: February 14, 2023

City of Davenport  
Historic Preservation Commission

Department: Development & Neighborhood Services  
Contact Info: Matt Werderitch 563.888.2221

**Date**  
**1/10/2023**

**Subject:**  
Consideration of the October 11, 2022 meeting minutes.

**Recommendation:**  
Staff recommend approval of the October 11, 2022 meeting minutes.

**Background:**  
The October 11, 2022 meeting minutes are attached.

**ATTACHMENTS:**

Type	Description
▢ Backup Material	Meeting Minutes 10-11-22

**REVIEWERS:**

Department	Reviewer	Action	Date
City Clerk	Werderitch, Matt	Approved	12/19/2022 - 11:38 AM



# **HISTORIC PRESERVATION COMMISSION MEETING MINUTES**

## **CITY OF DAVENPORT, IOWA**

**TUESDAY, OCTOBER 11, 2022; 5:00 PM**

**CITY HALL | 226 WEST 4<sup>TH</sup> STREET | COUNCIL CHAMBERS**

### **I. Call to Order**

Chairperson McGivern called the meeting to order.

Present: McGivern, Franken, Powers, Miranda, Higgins, Lesthaeghe

Staff Present: Berkley, Werderitch

### **II. Secretary's Report**

#### **A. Consideration of the September 13, 2022 meeting minutes.**

Motion by Lesthaeghe, second by Franken, to approve the September 13, 2022 meeting minutes. Minutes were unanimously approved by voice vote (6-0).

### **III. Communications**

### **IV. Old Business**

### **V. New Business**

#### **A. Case COA22-14: Request for exterior alteration at 613 West 6th Street. Isaac and Mary Farber House is located in the Local Historic Hamburg District. Lenny DeSantiago, petitioner. [Ward 3]**

Werderitch provided an overview of the proposed fascia and soffit work. The Building Department instructed the owner to temporarily patch any openings to prevent animals from entering the home until a long term solution could be identified and approved by the Historic Preservation Commission.

Lenny DeSantiago was in attendance to answer questions.

Staff made a recommendation to approve the Certificate of Appropriateness for exterior alteration at 613 West 6th Street, subject to the following conditions:

1. Soffit and fascia boards shall be repaired rather than replaced where possible. Replacement boards shall be sized to match the existing soffit and fascia dimensions.
2. Remove damaged or deteriorated masonry paint only to the next sound layer using the gentlest method possible (e.g., handscraping) prior to repainting.
3. Apply compatible paint coating systems following proper surface preparation.
4. Repaint with colors that are historically appropriate to the building and district.

Per conversation with the petitioner, a separate Certificate of Appropriateness application shall be submitted for the reconstruction of the front porch and reviewed at a later meeting date.

The project was reviewed for conformance with the Standards for Review, Chapter 14.01.060C and D of the Davenport Municipal Code. The project meets the following standards:

1. Every reasonable effort shall be made to make the minimal number of changes necessary to maintain a designated property in a good state of repair, thereby minimizing the impact of the proposed alteration.
2. Deteriorated architectural features should, where possible, be repaired rather than replaced. Where the severity of deterioration requires replacement, the new feature shall match the old in design, color, texture and other visual qualities and, where possible, materials. Replacement of missing features shall be substantiated by documentary, physical or pictorial evidence.
3. Activities that cause deterioration of a designated property and its architectural features shall be discouraged. In those cases where the damage would be irreversible, such as sand blasting and wet blasting fire-hardened bricks, the activities shall be prohibited. If cleaning is to be done, the gentlest means possible shall be encouraged.

Motion by Powers, second by Miranda, to approve staff recommendation and conditions for Case COA22-14. Motion to approve staff recommendation passed by a roll call vote (6-0).

- B. Case COA22-15: Request for the installation of a new roof at 615 Marquette Street. The St. Joseph's Rectory is a locally listed historic landmark. Ameripro Roofing, petitioner, on behalf of The 180 Zone Inc. [Ward 3]

Werderitch presented the history of the St. Joseph Rectory, which has been substantially altered over the years. The applicant is proposing to install a new asphalt shingle roof.

Brian Chisholm, Ameripro Roofing, spoke on behalf of the property owner. The installation will require minor flashing and mechanical work.

Staff made a recommendation to approve the Certificate of Appropriateness to install a new roof at 615 Marquette Street in accordance with the submitted material.

The project was reviewed for conformance with the Standards for Review, Chapter 14.01.060C and D of the Davenport Municipal Code. The project meets the following standards:

1. Every reasonable effort shall be made to make the minimal number of changes necessary to maintain a designated property in a good state of repair, thereby minimizing the impact of the proposed alteration.
2. Deteriorated architectural features should, where possible, be repaired rather than replaced. Where the severity of deterioration requires replacement, the new feature shall match the old in design, color, texture and other visual

qualities and, where possible, materials. Replacement of missing features shall be substantiated by documentary, physical or pictorial evidence.

Motion by Franken, second by Miranda, to approve staff recommendation and conditions for Case COA22-15. Motion to approve staff recommendation passed by a roll call vote (6-0).

VI. Other Business

VII. Open Forum for Comment

VIII. Adjourn

Motion by Higgins, second by Lesthaeghe, to adjourn the meeting. Motion passed by a voice vote (6-0). The meeting adjourned at 5:11 pm.

IX. Next Commission Meeting: November 8, 2022

City of Davenport  
Historic Preservation Commission

Department: Development & Neighborhood Services  
Contact Info: Matt Werderitch 563.888.2221

**Date**  
**1/10/2023**

**Subject:**

Case COA23-01: Request for exterior alteration at 624 West 5th Street. The Charles and Amelia (Ruch) McElroy House is located within the Hamburg Local Landmark Historic District. Duane Timm, petitioner. [Ward 3]

**Recommendation:**

A recommendation is made to approve the Certificate of Appropriateness for exterior alteration at 624 West 5th Street in accordance with the submitted material, subject to the following conditions:

1. The porch shall be attached to the structure using the least invasive means possible to best preserve the brick exterior.

The project was reviewed for conformance with the Standards for Review, Chapter 14.01.060C of the Davenport Municipal Code. The project meets the following standards:

1. New additions and related new construction shall not be discouraged when such improvements do not destroy historic material and such design is compatible with the size, massing, scale, color, materials and character of the property, neighborhood and district, if applicable.
2. A proposed alteration, construction, reconstruction or addition should not negatively impact the scale of the designated property.

**Background:**

The Charles and Amelia (Ruch) McElroy House is a contributing structure within the Hamburg Local Landmark Historic District. The Vernacular Greek Revival home was built in 1874. Building materials include a stone foundation, brick, and a front gable roof with asphalt shingles. Alterations to the home include a two-story bay window added to the west elevation.

**Porch Addition**

The property owner was awarded money through Davenport's Dream Plus program to construct a porch on the northeast corner of the house. The applicant received approval from the Historic Preservation Commission to construct a similar porch at 630 West 5th Street at its April 12, 2022 meeting. The proposed design closely replicates the prior approval on the home next door.

The property owner believes the original structure had a covered porch along the northeast elevation that was removed at an unknown date. Staff have been unable to confirm, since the historic inventory sheet does not mention a rear porch.

The applicant is hoping to reconstruct the covered porch by creating a new roofline below the second floor window. The porch will not extend beyond the east wall of the home. The new roof will have shingles installed to match the existing home. Copper flashing will be used where the shingles meet the brick exterior.

New gutters and downspouts will be installed along the roofline of the porch matching the existing style, material, and profile of the main structure.

- The porch columns are 6x6 treated wood with prefinished metal wrap and preformed and prefinished moldings. New decorative wood brackets are proposed below the roofline of the porch and above the column capitals. The applicant has provided an example of a home built around the same time period with decorative brackets that will be used as a model.

The property inventory sheet identifies the architectural style of the home as "Vernacular Greek Revival". The proposed wood porch brackets appear to be more of an Italianate or Queen Anne architectural style. A simpler bracket or column capital, which extends to the roofline, may be more fitting with a Vernacular Greek Revival style. However, given the location on the east and north elevation, the design will not significantly detract from the historic character of the Hamburg Historic District.

- New concrete footing and foundation wall will be poured to create the base of the porch. The deck will be constructed with composite 6 inch wide plank floor on 2x6 treated wood joists.

#### ATTACHMENTS:

Type	Description
▢ Backup Material	Porch Plans
▢ Backup Material	Background Materials & Photos
▢ Backup Material	Application
▢ Backup Material	Historic Property Inventory Sheet
▢ Backup Material	Historic Preservation Ordinance-Review Standards

#### REVIEWERS:

Department	Reviewer	Action	Date
Community Planning & Economic Development	Werderitch, Matt	Approved	1/6/2023 - 9:24 AM





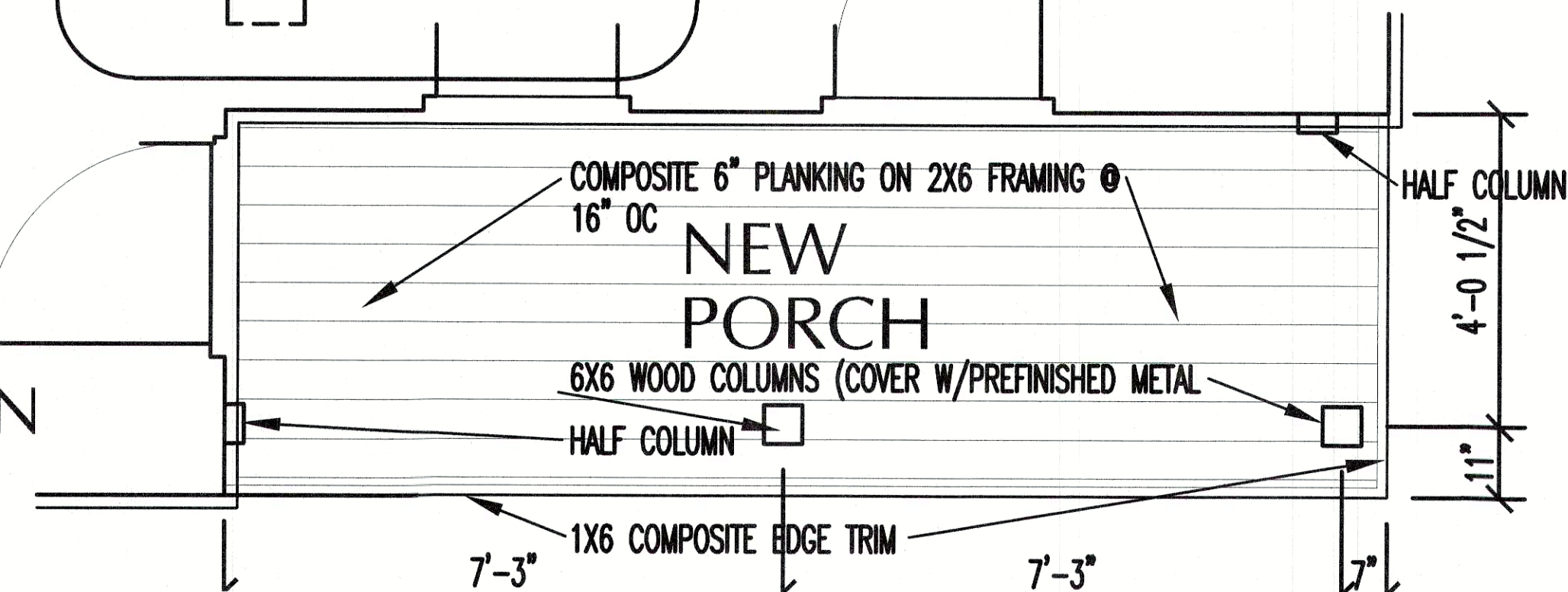
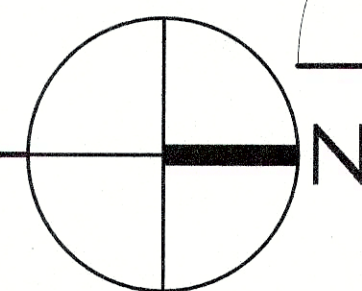
PARTIAL EAST ELEVATION  
PARTIAL WEST ELEVATION OPPOSITE HAND  
SCALE: 1/4"=1'-0"

- NEW PORCH
- FLASHING/COUNTERFLASHING (COPPER)
- ASPHALT SHINGLES (TO MATCH EXISTING)  
ON 15# ROOFING FELT ON 3/4" EXT  
GRADE PLYWOOD ON 2X6 FRAMING @  
24" OC
- PREFINISHED MTL GUTTER & DOWNSPOUTS  
(TO MATCH PROFILE AND COLOR OF EXISTING)  
ON WOOD FASCIA BOARD (PAINT)
- 1X6 WOOD RAKE TRIM (PAINT)
- VERTICAL WOOD SIDING (PAINT)
- 6X6 WOOD COLUMNS - COVER WITH PREFINISHED  
METAL
- 12" DIAM X 48" DEEP CONC POSTHOLE FOOTINGS



NORTH ELEVATION  
SCALE: 1/4"=1'-0"

FLOOR PLAN  
SCALE: 1/4"=1'-0"



EAST PORCH ADDITION - GOMEZ/TIMM RESIDENCE

624 WEST 5TH STREET

DAVENPORT, IOWA

NOVEMBER 5, 2022

1.0



# Charles & Amelia (Ruch) McElroy House

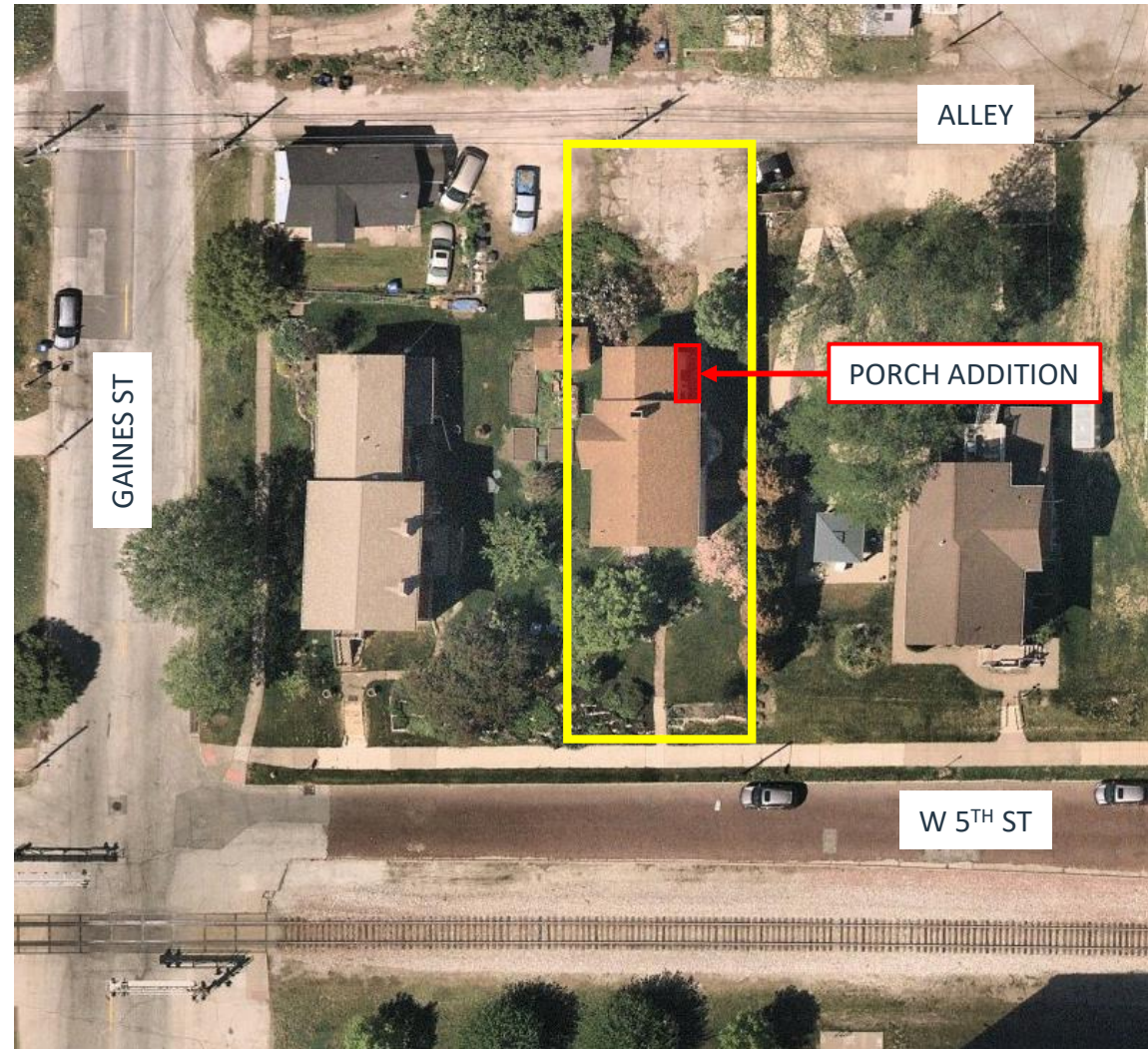
- Hamburg Local Landmark Historic District
- Constructed Date: 1874
- Designed by Local Builder T.W. McClelland
- Architectural Style: Vernacular Greek Revival
- Roof: Gable Front with Asphalt Shingles
- Materials: Brick on Stone Foundation
- Non-Historic Alteration: 2 Story Bay Window Added on West Elevation





# Charles & Amelia (Ruch) McElroy House

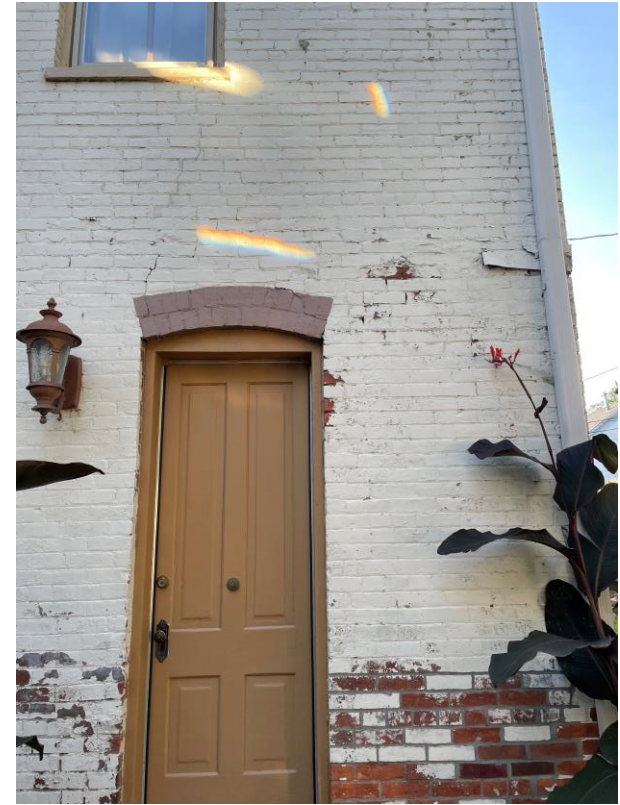
DAVENPORT  
IOWA | USA





# Porch Addition

DAVENPORT  
IOWA | USA

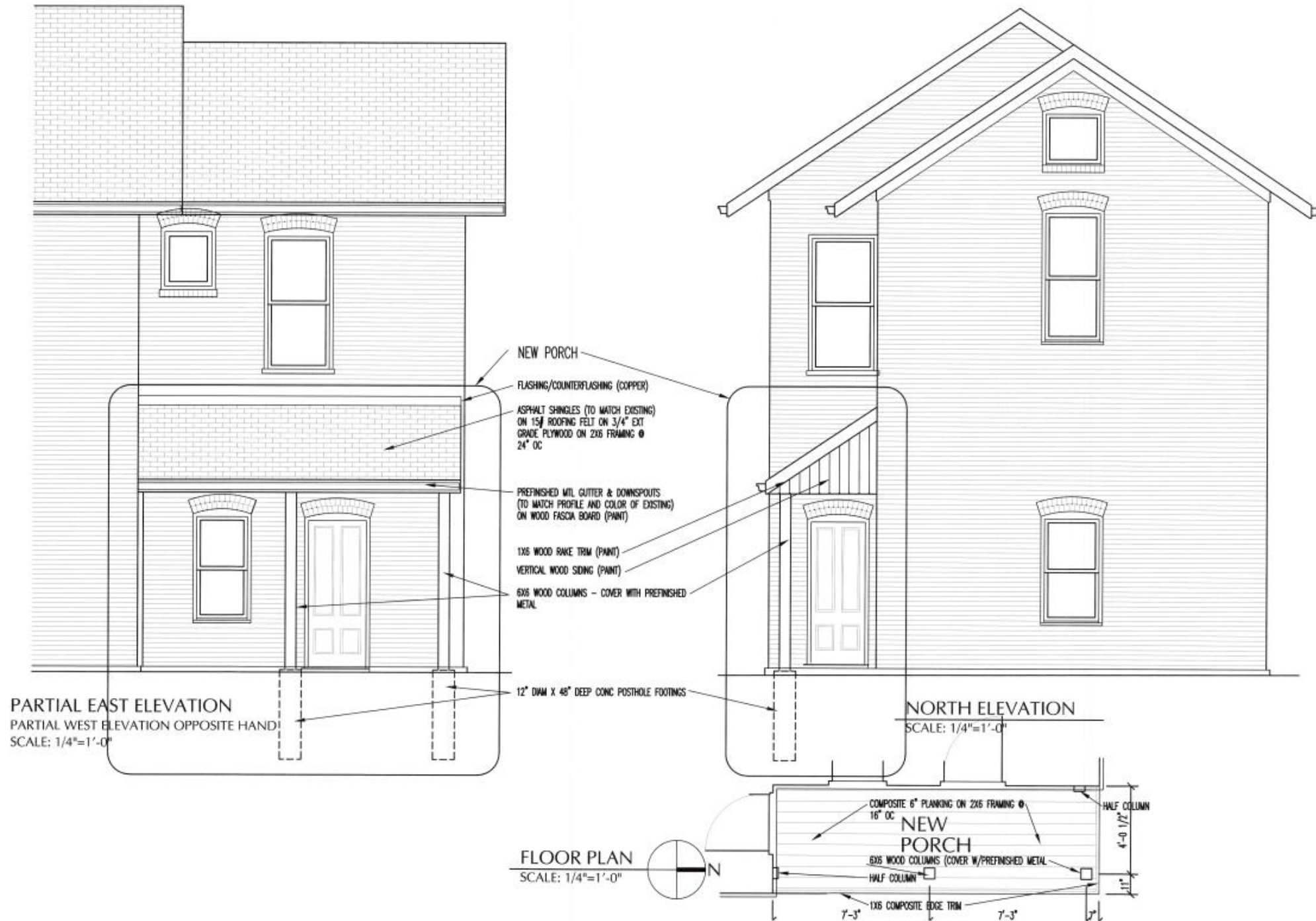




# Project Scope

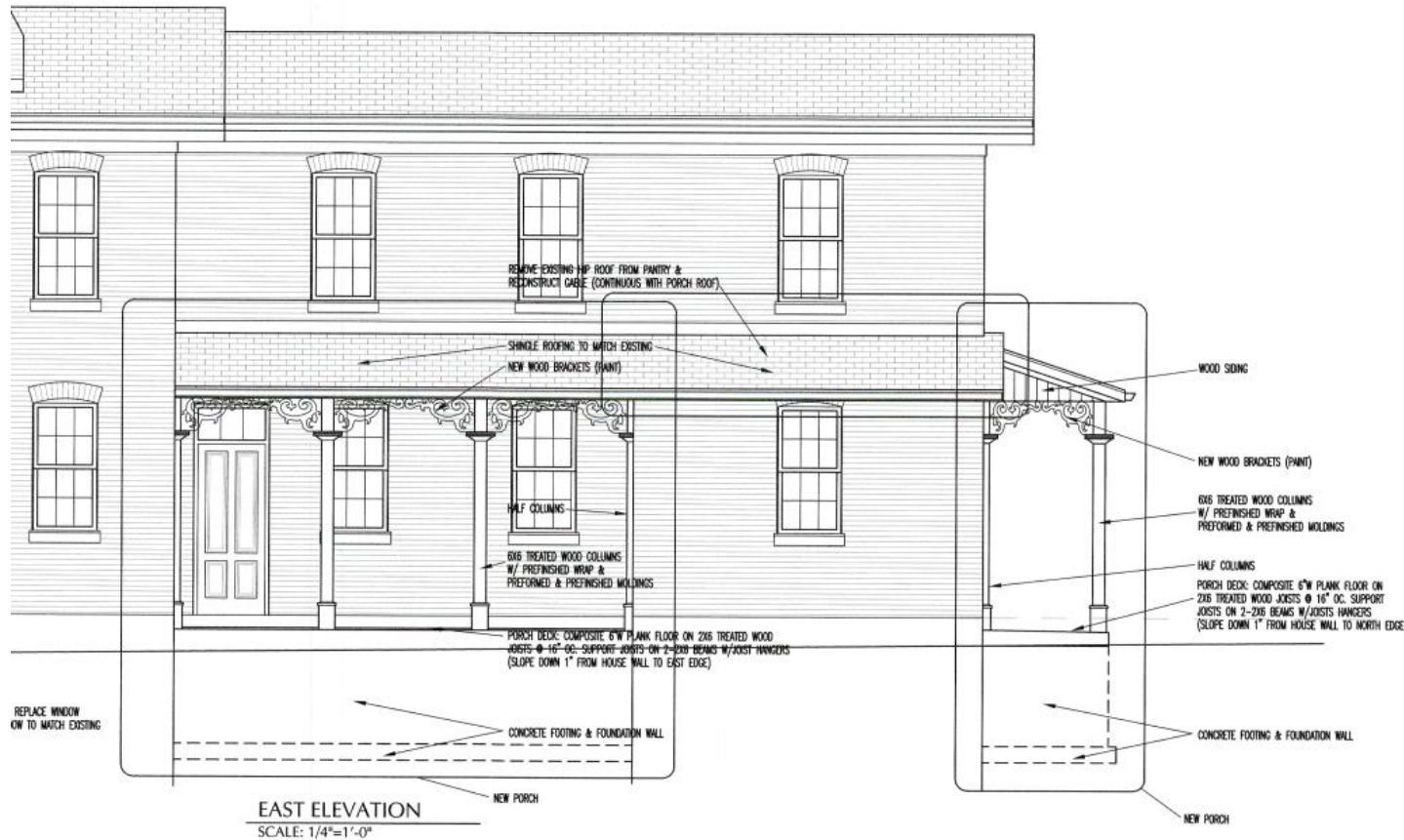
## Building Materials

- Asphalt Shingles
- Cooper Flashing
- Gutters to Match Existing
- Wood Fascia Boards
- Wood Trim
- Vertical Wood Siding
- Wood Columns Covered with Prefinished Metal
- Composite Plank Flooring





# Porch Brackets



**630 W 5<sup>th</sup> Street**

**Porch Addition Approved on 4/12/22**

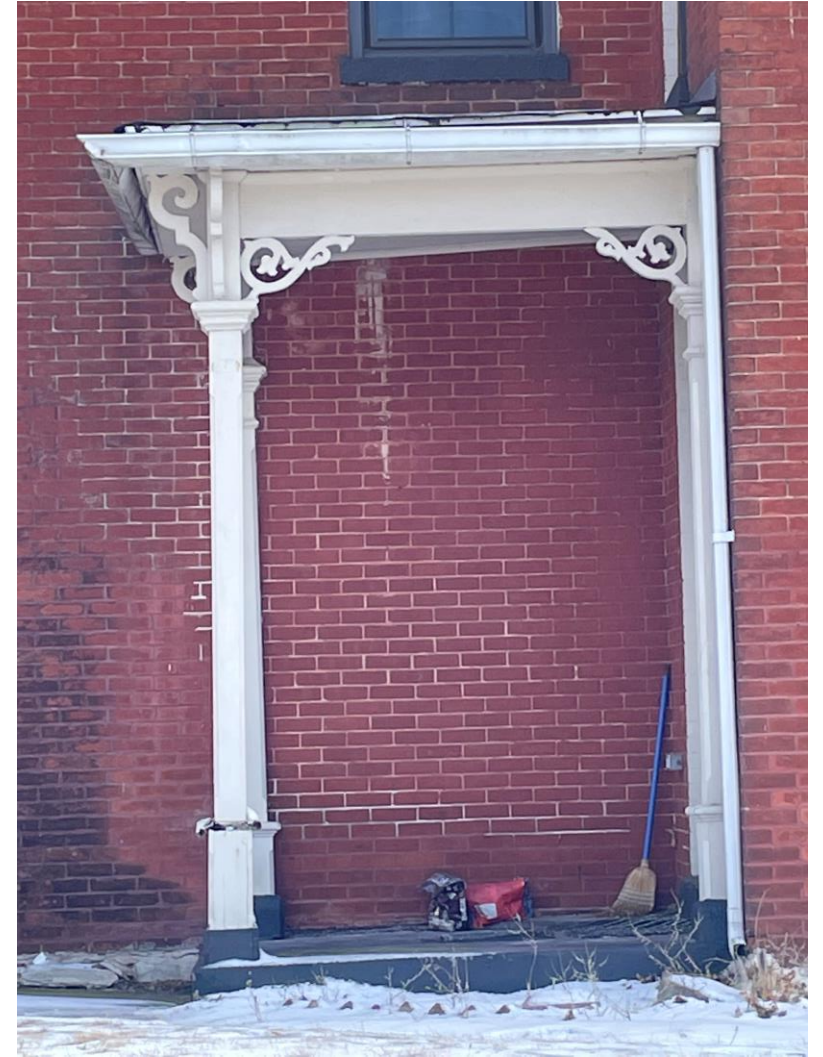


**Example Porch Brackets**



# Porch Brackets

DAVENPORT  
IOWA | USA







1200 East 46<sup>th</sup> Street  
Davenport, Iowa 52807  
(563) 326-6198  
[planning@davenportiowa.com](mailto:planning@davenportiowa.com)

Complete application can be emailed to [planning@davenportiowa.com](mailto:planning@davenportiowa.com)

Property Address\* 624 W. 5<sup>th</sup> St, Davenport, IA 52801

\*If no property address, please submit a legal description of the property.

### Applicant (Primary Contact)

Name: Duane Timm  
Company:   
Address: 630 W. 5<sup>th</sup> St  
City/State/Zip: Davenport, IA 52801  
Phone: 563-210-5987  
Email: bigdaddydtimm@yahoo.com

### Owner (if different from Applicant)

Name: Duane Timm & Jeffrey Gomez  
Company:   
Address:   
City/State/Zip:   
Phone:   
Email:

### Engineer (if applicable)

Name:   
Company:   
Address:   
City/State/Zip:   
Phone:   
Email:

### Architect (if applicable)

Name: John Gere  
Company:   
Address:   
City/State/Zip:   
Phone: 563-529-1227  
Email:

### Attorney (if applicable)

Name:   
Company:   
Address:   
City/State/Zip:   
Phone:   
Email:

### Application Form Type:

#### Plan and Zoning Commission

- Zoning Map Amendment (Rezoning) ☐  
Planned Unit Development ☐  
Zoning Ordinance Text Amendment ☐  
Right-of-way or Easement Vacation ☐  
Voluntary Annexation ☐

#### Zoning Board of Adjustment

- Zoning Appeal ☐  
Special Use ☐  
Hardship Variance ☐

#### Design Review Board

- Design Approval ☐  
Demolition Request in the Downtown ☐  
Demolition Request in the Village of East Davenport ☐

#### Historic Preservation Commission

- Certificate of Appropriateness ☒  
Local Landmark Nomination ☐  
Demolition Request ☐

#### Administrative

- Administrative Exception ☐  
Health Services and Congregate Living Permit ☐

## Historic Resource:

- ☒ Local Hamburg Historic District
- ☐ Iowa Soldier's Orphans' Historic District
- ☐ Marycrest College Historic District
- ☐ Individually Listed Local Historic Landmark

**Not sure if you have a Historic Resource?** You can click [here](#) to access the City's GIS Map. Click the layers icon toward the top right of the page. Click the Planning Layers dropdown. Turn the Historic Resources layer on by checking the box.

Historic Resources requiring a Certificate of Appropriateness are mapped with a  or .

If you are unsure, please contact the Community Planning and Economic Development staff at (563) 326-6198 or [planning@davenportiowa.com](mailto:planning@davenportiowa.com) for assistance.

## When is a certificate of appropriateness required?

Prior to the commencement of the work.

## What type of activity requires the approval of a certificate of appropriateness?

Any activity requiring a building or sign permit, except demolition, that would change the exterior architectural appearance of a structure. Examples include new construction, exterior alterations, relocations, reconstructions and infill development.

## Submittal requirements

- Please contact Planning staff at (563) 326-6198 or [planning@davenportiowa.com](mailto:planning@davenportiowa.com) so we can help you determine what exactly is required to be submitted.
- Incomplete applications will not be accepted.

### Submittal requirements for all types of requests:

- The completed application form.
- A work plan that accurately and completely describes the work to be done.
- Color digital photographs depicting the building elevations and proposed construction.

### Submittal requirements for specific types of requests:

#### Minor alterations to existing buildings and new and replacement signs (all of the above and):

- Specifications, including dimensions, material used and color of the material.
- A rendering of the proposed alteration as depicted on the existing building.
- Samples of the materials, including the color, along with scaled, accurately colored elevations for any proposed sign and/or sign package.

#### Minor additions, site improvements and outdoor storage areas (all of the above and):

- A dimensioned site plan, including the locations of any proposed or existing buildings on the subject parcel and on surrounding parcels.
- A preliminary grading plan showing before and after grades at two-foot contour intervals, where deemed necessary by the development official.
- Outdoor storage areas shall be reflected in the elevation drawings submitted and shall show their relationship to the building elevations as well as the materials and treatment proposed that would accurately reflect the screening of the storage areas.
- A landscape plan.

Major additions and new buildings (all of the above and):

- Reproductions of building or site information found in the historical surveys if applicable
- A verifiable legal description, or a land survey.
- A map showing the existing topography of other properties at two-foot contour intervals, extending one hundred feet from the subject parcel.
- Elevation drawings, in color and drawn to scale, of the front, sides, rear, and roof lines of all proposed buildings or structures, illustrating the appearance and treatment of required screening elements for roof-mounted equipment, where deemed necessary by the development official.
- A materials board containing samples of each type of exterior building materials.

**Formal Procedure****(1) Application:**

- Prior to submission of the application, the applicant shall correspond with Planning staff to discuss the request, potential alternatives and the process.
- The submission of the application does not constitute official acceptance by the City of Davenport. Planning staff will review the application for completeness and notify the applicant that the application has been accepted or additional information is required. Inaccurate or incomplete applications may result in delay of required public meetings.

**(2) Historic Preservation consideration of the request:**

- Only work described in the application may be approved by the Commission.
- If the Commission determines there is insufficient information to make a proper judgment on the application, it may continue the application as long a period of 60 days has not elapsed from an accepted application. This time period does not apply if the applicant requests the continuance.

**(3) After the Historic Preservation Commission's decision:**

- If approved, a certificate of appropriateness does not constitute a City permit or license and does vest against any other land development regulation or regulatory approval. You will need to contact Davenport Public Works and other regulatory agencies regarding permits and/or licenses.
- If approved, a certificate of appropriateness will expire one year from the date of approval unless a building permit is obtained within such period. The Zoning Administrator may grant an extension for a period of validity longer than one year. An applicant may apply in writing for an extension of time at any time prior to the date of expiration.
- The applicant may appeal the Historic Preservation Commission's determination to the City Council. A written appeal along with payment of \$75.00 must be submitted to the Zoning Administrator within thirty calendar days of the Historic Preservation Commission's decision.

Applicant: Quane E. Jimm Date: 12/15/22

By typing your name, you acknowledge and agree to the aforementioned submittal requirements and formal procedure and that you must be present at scheduled meetings.

Received by: Matt Wardenitch Date: 12-16-22  
Planning staffDate of the Public Meeting: 1-10-23Meetings are held in City Hall Council Chambers located at 226 West 4<sup>th</sup> Street, Davenport, Iowa.

### Work Plan

Please describe the work being performed. Please note that only work described in the application may be approved by the Board.

Jeff and I are pleased to be approved by the City of Savenport for the DreamPlus Program.

We would like to rebuild a side porch on the back NE side of the house.

I have owned this house since 1996 and the porch was long removed by that date.

The addition of this porch will bring the house back to its 1872 (when built) appearance.

I have included pictures of what the area where the porch had been. Also a picture of a porch that could be a model of what we would like to rebuild.

The brackets will be similar but different than what was approved to rebuild the porch at 630 W. 5<sup>th</sup> St by Savenport's H.P.C.



Hamburg Historic District (amended)

Name of Property

Scott County, Iowa

County and State

Address State #, Map #, FS # 2016 district status 1983 district status	Historic Name	significant date/s architectural style/type	Materials of walls, foundations, roof
	History of property (owners/residents, census data including birthplace of self (and parents), Sanborn map info), Current use		Architectural data (architect/builder, features, modifications), Garage data
<b>624 W 5th St</b>  State #82-00479  Map #004 Field Site #A-10 Parcel #G0052-04  Updated district status: 1 contributing building (A, C)  1983 NRHP status: Non- contributing	<b>Charles and Amelia (Ruch) McElroy House</b>	c.1874 Gable-front - 2 story, 3 bay (McClelland type) (2 story)	Walls: brick (solid) Foundation: stone Roof: gable-front - asphalt shingles
	1860s-70s - lot owned with Lot 1 to west by John Ruch - daughter Amelia married Charles McElroy - sold this part of lot in 1874; 1874-1881 - Charles and Amelia McElroy (Charles E. McElroy & Co - leather, hides, pelts, etc) 1880 census: McElroy, Charles (31, salesman) - born in US - PA (US - PA, US - PA); wife: Amelia (30) - born in US - OH (US - PA, US - PA) 1881-1887 - Henry F. and Eliza Moeller (H.F. Moeller Manufacturing Co - box factory); Nov 1887 - John Stahmer; 1887-1891 - Wulf and Margaretha Hahn; 1891-1894 - Hahn heirs - Blunk et al 1910 Sanborn map: house - 2 story - extant 1910 census: Blunck, Theodore (69, retired) - born in Germany (Germany, Germany); wife: Wilhemina (58) - born in US - IA (Germany, Germany) 1894-1926 - F. Theodore and Wilhelmine Blunck (American Preserves Co, Davenport Turnhall Holding Co), also Herman and Amelia Blunck in 1910s; 1927-1942 - owned by Adolph Fanth - used as rental (1935 - Fred and Margaret Tindall) 1942-1955 - Fred and Margaret Tindell (refrigerator repair), also in 1945 - 624 1/2: Pearl and Ruby Waite; 1955-1960s - owned by Harry and Lily Wessel 1956 Sanborn map: house - 2 story - extant Current use: single family house - rental		Architect/builder: - Porch: entry concrete steps Windows: 2/2 wood windows - segmental brick arch lintels with keystones and shoulders Architectural details: double-door entry with transom, first story angled bay window on east, two story angled bay window on west, circular gable window, decorative gable window on rear Modifications: Historic: c.1888 - two-story bay window added on west (between 1886 and 1892 maps); Non-historic: - Garage: none Other site features:
<b>630 W 5th St</b>  State #82-00480  Map #005 Field Site #A-11 Parcel #G0052-03  Updated district status: 1 contributing building (A, C)  1983 NRHP status: Contributing (key)	<b>John and Elizabeth (Cumber) Ruch House</b>	c.1867 Gable-front - 2 story, 3 bay (McClelland type) (2 story)	Walls: brick (solid) Foundation: stone Roof: gable-front - asphalt shingles
	c.1867-1903 - John Ruch (pork packer, later partner with brother Henry in brick manufacturing) - wife Elizabeth (here 1867-1869), then Caroline (1870), and then wife Mariah Josephine (1879-1903); brother Henry Ruch also lived here (brick manufacturer) 1880 census: Ruch, John (53, pork packer) - born in US - PA (Switzerland, US - PA); wife: Mariah (35) - born in US - MA (Hungary, US - PA) 1903-1913 - Mariah J. Ruch - also three other families lived here 1910 Sanborn map: house - 2 story - extant 1910 census: Ruch, Maria J (61) - born in US - MA (Austria, US - PA); wife: three other families here too (all born in US) - born in US 1913-1915 - owned by Dakota A. Whiford (1915 - rooms - Leona Carney and A.E. Fisher); 1918-1923 - series of owners (Elizabeth Hess, Ford Harrington, Thomas Agar, George Roggenkamp, Thomas Agar); 1923-1938 - Frank H. and Anna Petersen - lived in one unit 1938-1946 - Anna Petersen - lived in one unit - 4 apts by 1945; 1947-1951 - H. Wildasin - rental; 1951-1956 - Oscar Gilbert - rental; 1956-1968 - owned by Harry and Lilly Wessel - used as rental; 1968 - Lily Wessel 1956 Sanborn map: apartments (4 units) - 2 story - extant Current use: single family house - owner occupied		Architect/builder: McClelland, Thomas W. (contractor) (attributed) Porch: entry concrete steps Windows: 2/2 wood windows - segmental brick arch lintels with keystones Architectural details: full length first story windows, first story square bay window on east Modifications: Historic: c.1888 - first story bay window added on east - noted as "glass" in 1892 (between 1886 and 1892 maps); Non-historic: 1956 - converted to four apartments; 1962 - front porch removed, concrete steps installed; 2007 - three windows restored Garage: (former carriage house converted to duplex - see 511 Gaines) Other site features:
<b>511 Gaines St</b>  State #82-01824  Map #006 Field Site #A-12 Parcel #G0052-02  Updated district status: 1 contributing building (A)  1983 NRHP status: Contributing	<b>Ruch Carriage House (converted to duplex)</b>	c.1867 Victorian carriage house (1 1/2 story)	Walls: brick (solid) Foundation: stone Roof: side gable
	1867-1903 - carriage house owned by John Ruch - lived at 630 W. 5th St 1903-1913 - carriage house for Mariah J. Ruch - lived at 630 W. 5th with three other families listed 1910 Sanborn map: outbuilding - 1 1/2 story - extant 1918 - 511 Gaines first appears as separate address with owner living here - Ford J. and Laura Harrington - owned Lot 1 with 630 W. 5th from 1918-21; 1921-1938 - owned by F.H. and Anna Petersen (all of Lot 1) - used as rental unit, with apts in house also 1938-1946 - owned by Anna Petersen (widow) - split property in 1946 and sold house at 630 W. 5th St - retained 511 Gaines and lived here (listed here through 1955, died in 1976) - noted as 2 unit dwelling in 1956 1956 Sanborn map: apartment - 1 1/2 story - extant Current use: single family house - owner occupied		Architect/builder: - Porch: entry steps (concrete) Windows: 6/6 wood windows (some, mix) - brick segmental arch lintels Architectural details: segmental brick arches indicating earlier openings - large one on north along alley Modifications: Historic: 1910s - remodeled from carriage house to rental housing (2 units by 1940s); Non-historic: - Garage: none Other site features:

## **Historic Preservation Ordinance**

### **Section 14.01.060 Certificate of Appropriateness Review Process**

**C. Commission review process** - Standards for review. In considering an application for a certificate of appropriateness, the commission shall be guided by the following general standards in addition to any other standards or guidelines established by ordinance for a local landmark or historic district. In all cases, these standards are to be applied in a reasonable manner, taking into full consideration the issue of economic feasibility and other technical considerations.

1. Every reasonable effort shall be made to make the minimal number of changes necessary to maintain a designated property in a good state of repair, thereby minimizing the impact of the proposed alteration; and
2. The removal, alteration or concealing of distinguishing exterior architectural features and historic material of a designated property should be avoided when possible; and
3. All designated property shall be recognized as a product and physical record of its time, place and use. Changes that create a false sense of historical development, such as adding conjectural architectural features shall be discouraged; and
4. Most properties change over time, and those changes that have acquired architectural and/or historical significance in their own right shall be recognized, respected and retained; and
5. Distinctive architectural features, construction techniques and/or examples of craftsmanship that characterize a designated property shall be treated with due consideration; and
6. Deteriorated architectural features should, where possible, be repaired rather than replaced. Where the severity of deterioration requires replacement, the new feature shall match the old in design, color, texture and other visual qualities and, where possible, materials. Replacement of missing features shall be substantiated by documentary, physical or pictorial evidence; and
7. Activities that cause deterioration of a designated property and its architectural features shall be discouraged. In those cases where the damage would be irreversible, such as sand-blasting and wet blasting fire-hardened bricks, the activities shall be prohibited. If cleaning is to be done, the gentlest means possible shall be encouraged; and
8. Known significant archeological resources possibly affected by a proposed activity shall be protected and preserved. If such resources must be disturbed, mitigation measures shall be undertaken; and
9. New additions and related new construction shall not be discouraged when such improvements do not destroy historic material and such design is compatible with the size, massing, scale, color, materials and character of the property, neighborhood and district, if applicable.

**D. Design criteria to implement review standards.** When the commission is considering an application for a certificate of appropriateness, it shall consider the following architectural design criteria, or elements of design as they relate to the standards for review prescribed in Section 14.01.040C.

1. Height. The height of any proposed addition, construction or reconstruction should be compatible with the designated property and the surrounding structures, if located within a designated historic district; and
2. Proportions. The proportions (width versus height relationship) between doors and windows should be compatible, if not replicated, with the architectural design and character of the designated property; and
3. Scale. A proposed alteration, construction, reconstruction or addition should not negatively impact the scale of the designated property or district; and
4. Materials. Historic or original architectural features, or replacement elements which in all ways replicated the original, should be repaired whenever possible; and
5. Relationship of building masses and spaces. The relationship of a structure within a designated historic district to the rear, side and front yards between it and surrounding structures should be compatible; and
6. Roof shape. The roof design and shape should remain consistent with its original configuration and character; and
7. Site improvements. Landscaping and other site improvements, including off-street parking, should have as minimal of an impact as possible to the designated property's original plan/layout and its visual character.

City of Davenport  
Historic Preservation Commission

Department: Development & Neighborhood Services  
Contact Info: Matt Werderitch 563.888.2221

**Date**  
**1/10/2023**

**Subject:**

Case COA23-02: Installation of accessible playground equipment at Vander Veer Park at 223 West Central Park Avenue. Vander Veer Park is a locally listed historic landmark. City of Davenport, petitioner. [Ward 5]

**Recommendation:**

A recommendation is made to approve the Certificate of Appropriateness for the installation of accessible playground equipment at Vander Veer Park in accordance with the submitted material.

The project was reviewed for conformance with the Standards for Review, Chapter 14.01.060C of the Davenport Municipal Code. The project meets the following standard:

- New additions and related new construction shall not be discouraged when such improvements do not destroy historic material and such design is compatible with the size, massing, scale, color, materials and character of the property, neighborhood and district, if applicable.

**Background:**

Vander Veer Park was originally the site of the Scott County Fairgrounds and was located on the edge of the City. The City purchased the property in 1885 and renamed it Central Park. It was renamed Vander Veer Park in 1911 in honor of A.W. Vander Veer, a Park Board commissioner from 1890 to 1911. Vander Veer Park was the first major park established by the City. The park was the City's best example of a landscape planning effort and is representative of Davenport's "City Beautiful" improvements in the late 19th and early 20th centuries.

The City of Davenport is planning to expand Gabe's Play Village, an ability-inclusive playground, at Vander Veer Park with the installation of a new ADA swing. The play equipment is located in the center of the park, between the parking lot and lagoon. The swing will be installed on the west corner of the playground. There will be no disturbance to the existing trees or landscaping.

The swing will be bordered by a new concrete curb and installed over a poured-in-place rubber playground surface. The swing is handicap accessible and enables two individuals to ride at a time. The overall structure is 9'7" in height and has a metal frame.

The proposed design will be sympathetic with the botanical theme of the park. The equipment will also provide an additional opportunity for children with disabilities to recreate.

**ATTACHMENTS:**

Type	Description
□ Backup Material	Application
□ Backup Material	ADA Swing Plan Set
□ Backup Material	Picture & Location
□ Backup Material	Background Material & Photos

- ▣ Backup Material
- ▣ Backup Material
- ▣ Backup Material

History of Vander Veer Park  
Vander Veer Park Site Inventory Sheet  
Historic Preservation Ordinance-Review  
Standards

**REVIEWERS:**

Department	Reviewer	Action	Date
Community Planning & Economic Development	Werderitch, Matt	Approved	1/6/2023 - 9:42 AM



**CITY OF DAVENPORT**  
Development & Neighborhood  
Services – Planning  
1200 W. 46<sup>th</sup> St  
Davenport, IA 52807

Office 563.326.6198  
planning@davenportiowa.com

APPLICATION FOR

## CERTIFICATE OF APPROPRIATENESS

HISTORIC PRESERVATION COMMISSION

RESOURCE TYPE	SUBMITTAL DATE	MEETING DATE
Individual Landmark	12/30/2022	01/10/2023

SITE ADDRESS & PARCEL NUMBER and/or HISTORIC NAME

**Vander Veer Park, 215 West Central Park**

BRIEF OVERVIEW OF THE PROJECT (not a scope of work)

Addition of a fully accessible swing

**APPLICABILITY** PRIOR to any work on applicable Historic Resources:

**A Certificate of Appropriateness must be submitted & approved PRIOR to the commencement of the following:**

- Any Building or Sign Permit changing the exterior (except demo)
- New construction/Addition or exterior alteration of a structure
- Sign installation or alteration

Demolition of any local or national historic resources shall require a Historic Demolition Request Application

**ALL SUBMITTALS SHALL INCLUDE:**

**SUBMITTED**

- |  |                                     |
|--|-------------------------------------|
| Full Scope of Work (SOW) attached as a .PDF<br>all work & materials shall be described & itemized/listed in detail | <input type="checkbox"/>            |
| Photos or renderings of all existing building/sign façades   | <input checked="" type="checkbox"/> |
| Proposed color building/sign elevations to scale<br>rendered showing existing and/or proposed building materials   | <input type="checkbox"/>            |
| Material specs: type, dimensions, color & manufacturer   | <input type="checkbox"/>            |
| <b>MINOR &amp; MAJOR ADDITIONS, SITE IMPROVEMENTS, &amp; NEW BUILDINGS SHALL INCLUDE ADDITIONAL ITEMS*:</b>        |                                     |
| Dimensioned Site Plan (proposed & existing buildings/site items)   | <input type="checkbox"/>            |
| Grading Plan with 2 foot intervals (if needed)   | <input type="checkbox"/>            |
| Mechanical Screening shall be shown  | <input type="checkbox"/>            |
| Materials Board of sample building materials proposed  | <input type="checkbox"/>            |

\* Major Additions & New Buildings may require more extensive information

**Formal Procedure**

Application Fee: **NONE**

(1) Application:

- Prior to submission of the application, the applicant shall correspond with Planning staff to discuss the request, potential alternatives and the process.
- The submission of the application does not constitute official acceptance by the City of Davenport. Planning staff will review the application for completeness and notify the applicant that the application has been accepted or additional information is required. Inaccurate or incomplete applications may result in delay of applicant's scheduled meetings.

(2) Scope of Commission's Consideration:

- Only work described in the application may be approved.
- If insufficient information exists to make a proper judgment on the application, the Commission may continue the consideration a maximum of 60 days, excluding applicant's continuances.

(3) Post Commission Ruling:

- An approved Cert. of Appropriateness does not constitute a City permit or license and does vest against any other land development regulation or regulatory approval. Applicant must contact necessary development authorities.
- COA approval expires one year from the date of approval unless a building permit is obtained within such period. An applicant may apply in writing for an extension of time at any time prior to the date of expiration.
- Appeals to determinations are \$75 made to City Council and shall be in writing submitted to the Zoning Administrator within 30 calendar days of Commission's decision.

Submit this form with attachments to: [planning@davenportiowa.com](mailto:planning@davenportiowa.com)

### APPLICANT INFORMATION

APPLICANT NAME

City of Davenport Parks and Recreation

ADDRESS

700 West River Drive

CITY, STATE, ZIP

Davenport, IA 52802

PHONE

(563) 328-7275

SECONDARY PHONE

(563) 888-2220

E-MAIL ADDRESS

### Acceptance of Applicant

I, the undersigned, certify that the information on this application to the best of my knowledge is true and correct. I further certify that I have a legal interest in the property in question, and/or that I am legally able to represent all other persons or entities with interest in this property, and acknowledge formal procedure and submittal requirements.

In addition to the application fee, I understand I am responsible for attendance at the meeting as shown on the historic preservation commission calendar. The City reserves the right to require further site studies as necessary.

**Betsy Tubbs**

Type Applicant's Name

Applicant's Signature

12/30/2022

Date

### DEVELOPMENT TEAM

#### Property Owner

City of Davenport

Address

226 W 4th St, Davenport

Phone

(563) 326-7711

Secondary Phone

E-Mail Address

betsy.tubbs@davenportiowa.com

#### Project Manager/Other

Address

Phone

Secondary Phone

E-Mail Address

Development and Neighborhood Services  
1200 East 46th St. Davenport, Iowa 52807

T | 563.326.6198  
E | [planning@davenportiowa.com](mailto:planning@davenportiowa.com)

[davenportiowa.com/DNS](http://davenportiowa.com/DNS)  
revised Jan. 2023



# VANDER VEER ADA SWING

## DAVENPORT PARKS AND RECREATION

### CITY OF DAVENPORT, IOWA

VANDER VEER PARK  
215 W. CENTRAL PARK  
DAVENPORT, IA. 52803



PROJECT LOCATION MAP

### SHEET INDEX

G000	COVER SHEET
C000	GENERAL NOTES AND SPECIAL CONDITIONS
C101	SITE PLAN
C201	GRADING AND EROSION CONTROL PLAN
C501	CONSTRUCTION DETAILS

### GENERAL INFORMATION

CONSTRUCTION NOT SPECIFICALLY DETAILED OR SPECIFIED WITHIN THE PLANS OR IN THE PROJECT MANUAL SHALL CONFORM TO THE LATEST EDITION OF SUDAS STANDARD SPECIFICATIONS AND THE LATEST DAVENPORT SUDAS SUPPLEMENTAL SPECIFICATIONS.

IOWA CODE 480, UNDERGROUND FACILITIES INFORMATION, REQUIRES VERBAL NOTICE TO IOWA ONE-CALL 1-800-292-8989, NOT LESS THAN 48 HOURS BEFORE EXCAVATING, EXCLUDING WEEKENDS AND HOLIDAYS.

THE CONTRACTOR SHALL PROVIDE TRAFFIC AND PEDESTRIAN CONTROL MEASURES (SIGNS, BARRICADES, FLAGGERS, ETC.) IN COMPLIANCE WITH PART VI OF THE MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES (MUTCD) LATEST EDITION.

IT IS THE CONTRACTOR'S RESPONSIBILITY TO ASCERTAIN EXISTING FIELD CONDITIONS BEFORE BIDDING ON THIS PROJECT, ORDERING MATERIALS, OR BEGINNING CONSTRUCTION AND SHALL NOTIFY ENGINEER OF ANY DISCREPANCIES OR CONFLICTS.



THE UTILITIES AS SHOWN ON THIS DRAWING WERE DEVELOPED FROM THE INFORMATION AVAILABLE. THIS IS NOT IMPLIED NOR INTENDED TO BE THE COMPLETE INVENTORY OF UTILITIES IN THIS AREA. IT IS THE CLIENT'S/CONTRACTOR'S RESPONSIBILITY TO VERIFY THE LOCATION OF ALL UTILITIES (WHETHER SHOWN OR NOT) AND PROTECT SAID UTILITIES FROM ANY DAMAGE.

	I HEREBY CERTIFY THAT THIS ENGINEERING DOCUMENT WAS PREPARED BY ME OR UNDER MY DIRECT PERSONAL SUPERVISION AND THAT I AM A DULY LICENSED PROFESSIONAL ENGINEER UNDER THE LAWS OF THE STATE OF IOWA.
	(SIGNATURE) _____ (DATE) 12-16-2022
	PRINTED OR TYPED NAME KRISTIN E. CRAWFORD
	LICENSE NUMBER 19664
	MY LICENSE RENEWAL DATE IS DECEMBER 31, 2023
PAGES OR SHEETS COVERED BY THIS SEAL: ENTIRE SUBMITTAL	

SHIVE-HATTERY  
ARCHITECTURE+ENGINEERING  
2144 56TH AVENUE WEST  
BETTENDORF, IOWA 52722  
563.635.7300 | SHIVE-HATTERY.COM

SEAL

VANDER VEER PARK ADA  
SWING  
DAVENPORT PARKS AND RECREATION  
215 W CENTRAL PARK AVE DAVENPORT IA 52803

PRELIMINARY  
- NOT FOR  
CONSTRUCTION

REVIEW  
ISSUE DATE: 12/16/2022  
PROJECT NUMBER: 2132204210

G000



GENERAL NOTES

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SITE PREPARATION NOTES

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- 16
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- 18
- 19

ADA SWING NOTES

- 1
- 2
- 3
- 4
- 5

QUANTITIES AND ESTIMATE REFERENCE NOTES TABLE

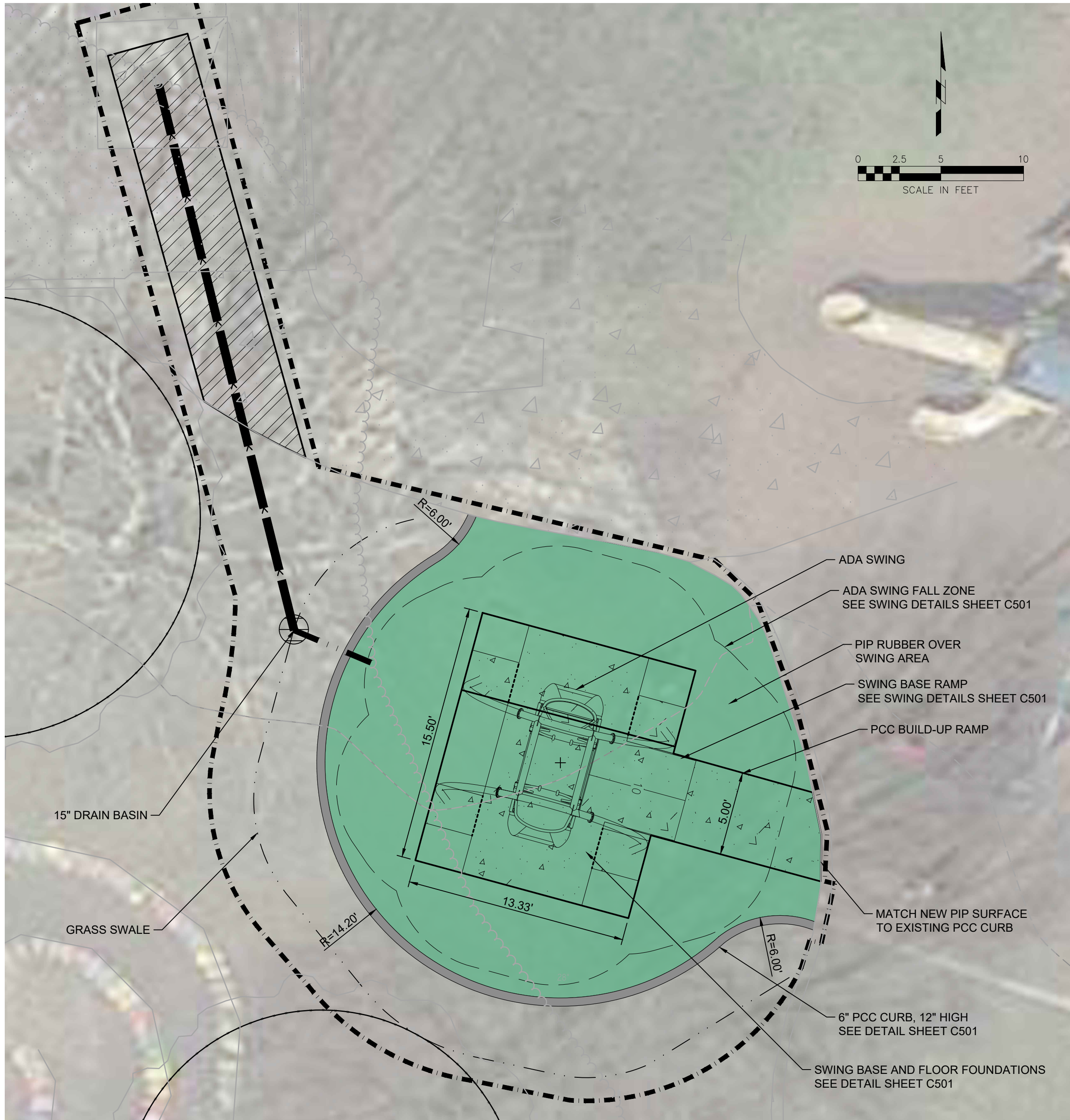
Item No.	Bid Item	Total Quantity	Unit	Item Notes
1	Topsoil Strip, 4-inch	150	SY	This bid item shall be defined by SUDAS Section 2010 and the Davenport Supplemental Specifications with the following additions: Strip topsoil in areas where playground surface is being constructed and the disturbed area for grading purposes. Estimated quantity based on the grading limits and paid for based on plan quantity.
2	Excavation, Class 10	50	CY	This bid item shall be defined by SUDAS Section 2010 with the following additions: Item will be paid based on plan quantity.
3	Storm Sewer, HDPE, 6-inch	34	LF	Comply with SUDAS Section 4020 and the City of Davenport Supplemental Specifications.
4	Corrugated Subdrain, 4"	90	LF	Comply with SUDAS Section 4020 and the City of Davenport Supplemental Specifications.
5	Area Drain, 15" Basin	1	EA	Nyloplast Area Drain, OAE. Dome grate incidental to the item.
6	Connection to Existing Storm Structure	1	LS	Item includes pavement removal and replacement with PCC pavement (~15 SY) and connection of 6" HDPE to Storm structure
7	We-Go-Swing	1	LS	We-go-Swing w/ 1 Accessible Bay DB only Model 277778A, includes all equipment, footings and foundations, installed.
8	Drainable Subbase, 5"	75	SY	This bid item shall be defined by SUDAS Section 2010 with the following additions: Place under Poured-in-Place surface.
9	PCC Pad, 4"	75	SY	Comply with SUDAS Section 7010 and the City of Davenport Supplemental Specifications. Payment will be at the contract unit price per square yard.
10	Concrete Perimeter Curb, 12"	96	LF	Comply with SUDAS Section 7010 and the City of Davenport Supplemental Specifications. Payment will be at the contract unit price per lineal foot.
11	Poured-in-Place Top surface and SBR layer	665	SF	1/2" Top Surface with 1-1/4" SBR layer. 75% Bright Green 25% Black with Aliphatic Binder Option. Payment will be at the contract unit price per square foot.
12	Erosion Control, Complete	1	LS	This bid item shall be defined by SUDAS Section 9040 and the Davenport Supplemental Specifications. A Davenport COSECO permit (no fee) will be required for this project. Temporary Seeding shall be incidental to the item. Payment will be made at the contract lump sum price.
13	Seeding, Fertilizing, and Mulching for Hydraulic Seeding	150	SY	This bid item shall be defined by SUDAS Section 9010 with the following additions: Seeding shall be Type 1, Permanent Lawn Mixture. This bid item is for seeding on disturbed areas not covered by pavement. Payment will be at the unit price per square yard of area seeded, mulched, and fertilized.
14	Construction Layout	1	LS	This item shall be defined by SUDAS Section 11,010. This includes all survey and staking necessary for construction including setting grades for paving and setting grades necessary to establish drainage. Payment for this item will be made at the contract lump sum price.
15	Construction Contingency, \$10,000	1	LS	This bid item is for compaction testing performed by an independent testing laboratory hired by the Contractor, as specified in SUDAS 2010. This item may also be used for other unforeseen circumstances which shall only by used with prior authorization and written approval of the Engineer. This item shall be bid per lump sum in the amount of \$10,000. Payment shall be made based upon approved individual incidences.



P:\projects\10012022\10012022\Drawings\C101 Site Plan.dwg  
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- LEGEND
- DISTURBANCE LIMITS
  - PIP SURFACE



SITE PLAN

PRELIMINARY  
- NOT FOR  
CONSTRUCTION

DRAWN BY	SLB
APPROVED BY	KEC
ISSUED FOR	REVIEW
ISSUE DATE	12/16/2022
PROJECT NUMBER	2132204210
FIELD BOOK	#####

VANDER VEER PARK ADA  
SWING

DAVENPORT PARKS AND RECREATION  
215 W CENTRAL PARK AVE DAVENPORT IA 52803

SHIVE-HATTERY  
ARCHITECTURE+ENGINEERING

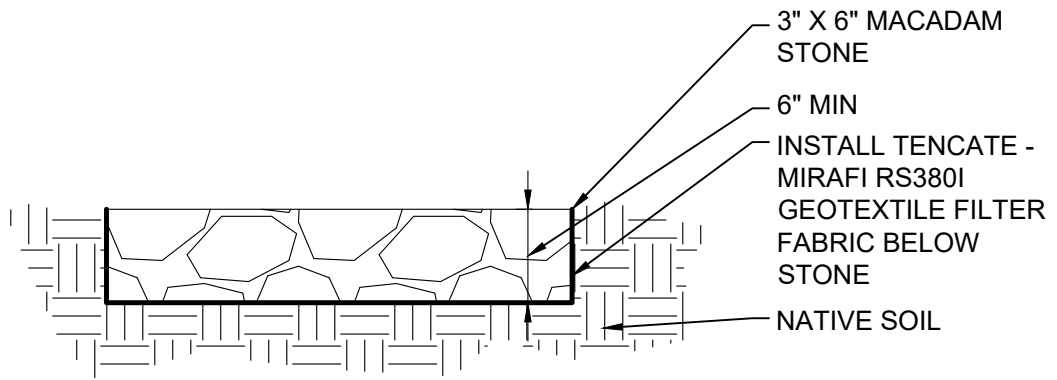
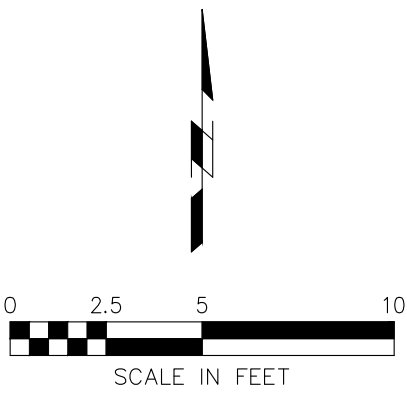
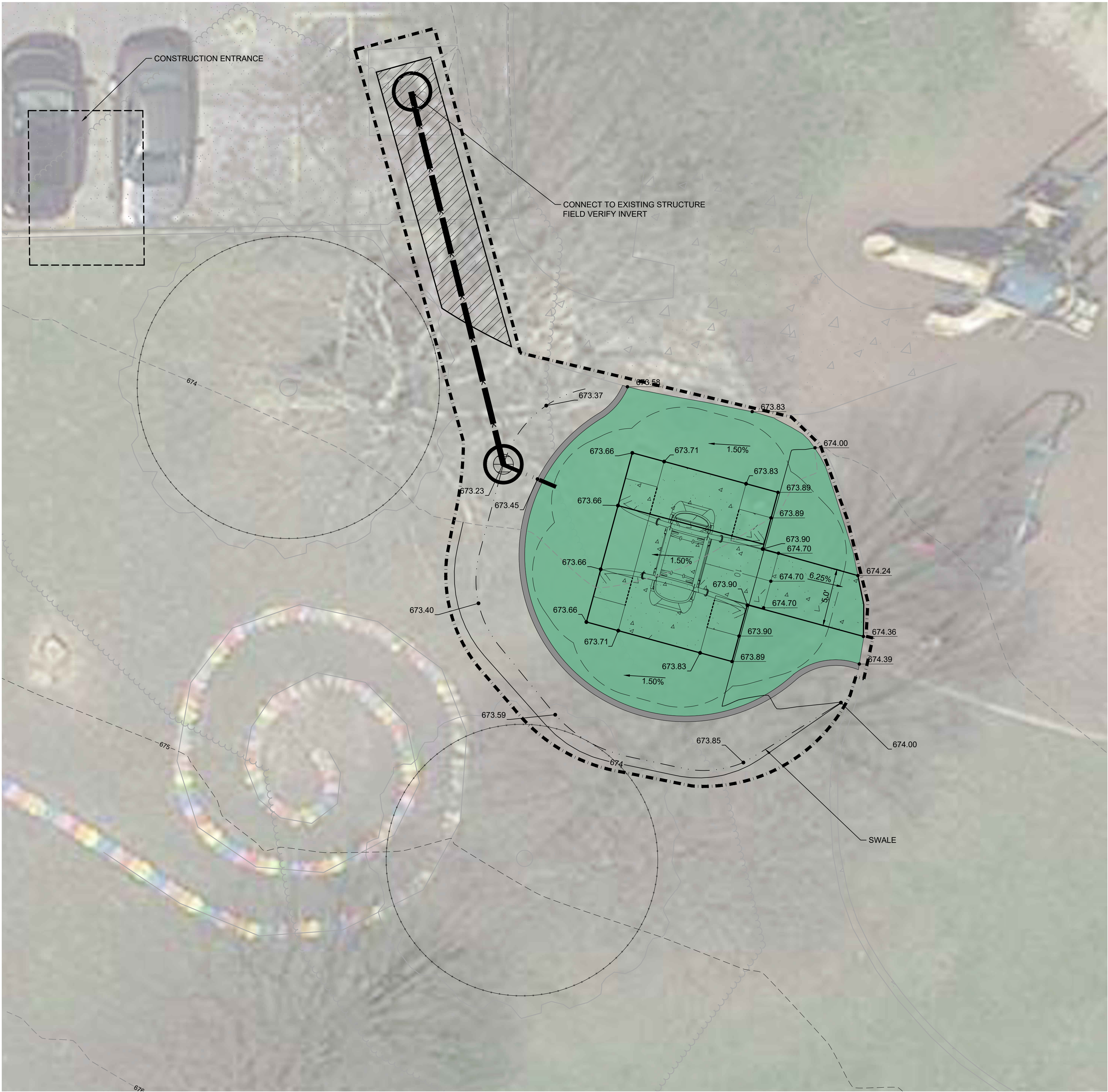
SEAL

2144 56TH AVENUE WEST  
BETTENDORF, IOWA 52722  
563.635.7300 | SHIVE-HATTERY.COM

C101

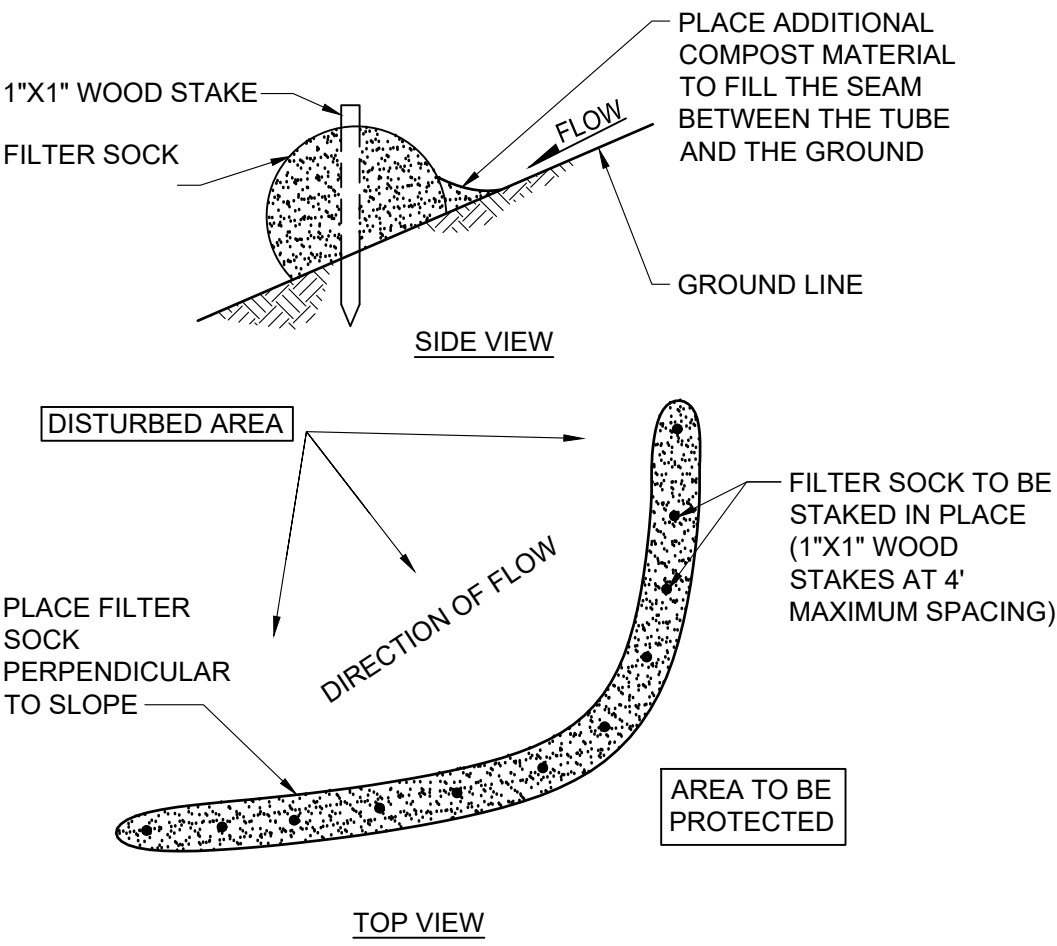


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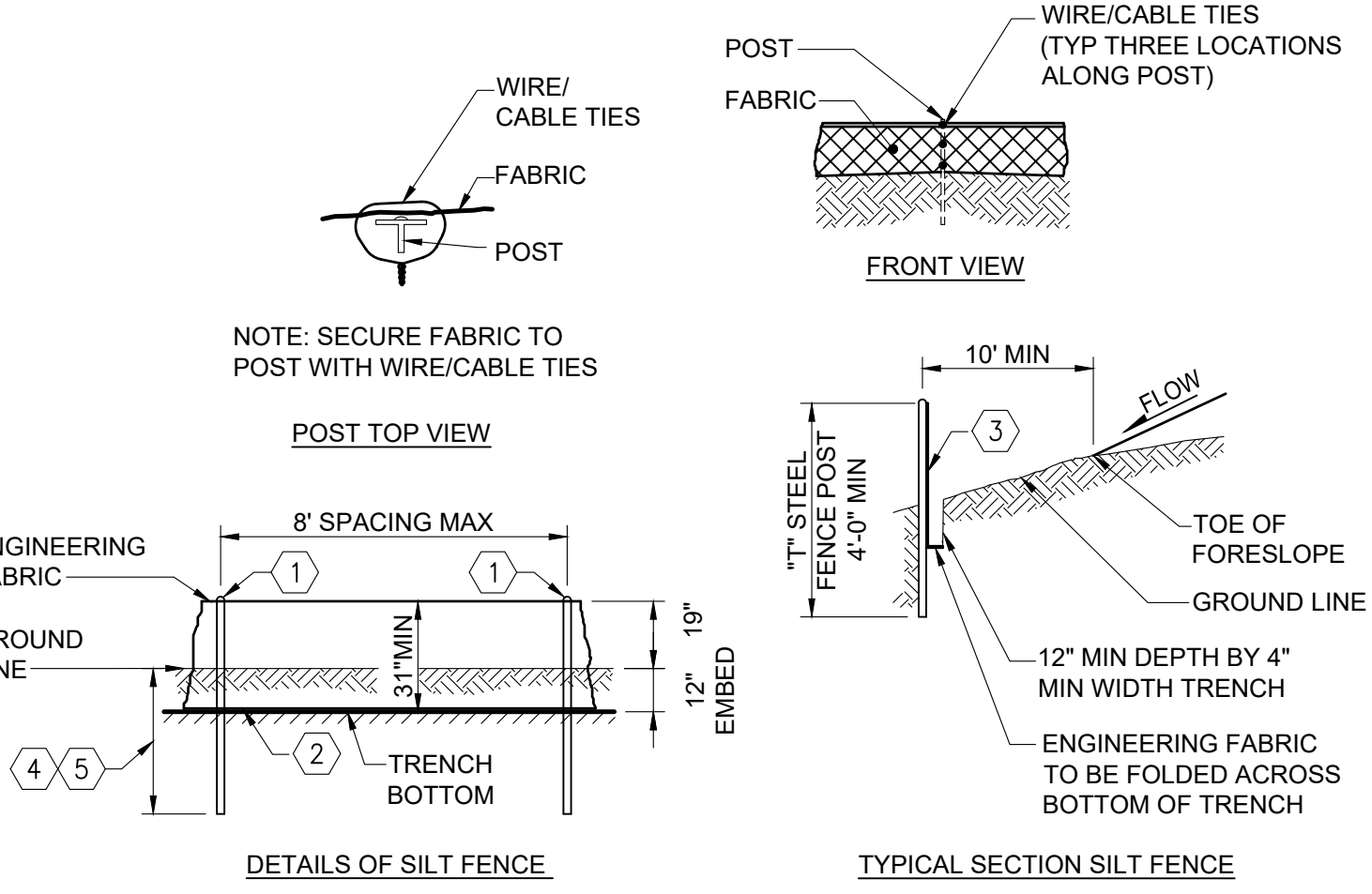
### STABILIZED CONSTRUCTION ENTRANCE/ CONTRACTOR STAGING AND LAYDOWN AREA (AS REQUIRED)

3 NOT TO SCALE



### 2 FILTER SOCK DETAIL

NOT TO SCALE



#### GENERAL NOTES:

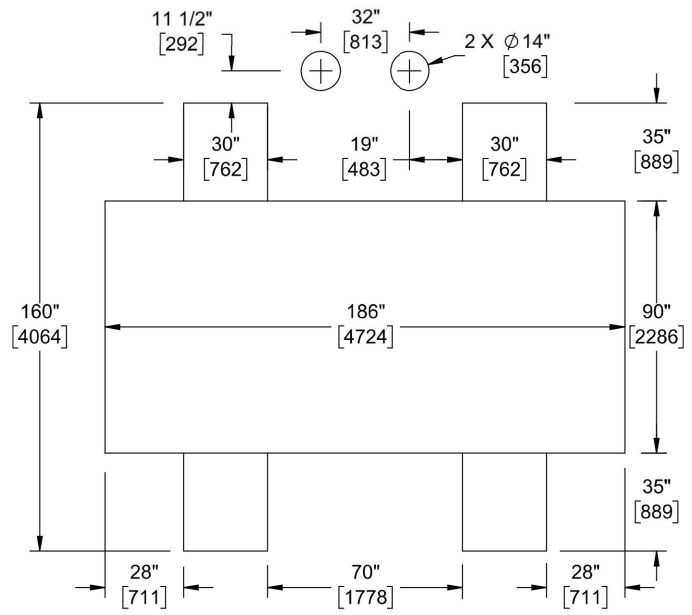
- 1 SECURE ENGINEERING FABRIC USING WIRE OR CABLE TIES AT TOP, MIDDLE AND BOTTOM OF EXPOSED STEEL POST.
- 2 FOR MANUAL/TRENCH INSTALLATION, ENGINEERING FABRIC TO BE FOLDED ACROSS BOTTOM OF TRENCH. SEE TYPICAL SECTION OF SILT FENCE.
- 3 ENGINEERING FABRIC SHALL HAVE A MINIMUM 36" WIDTH.
- 4 FOR MACHINE INSTALLATION, POSTS SHALL BE EMBEDDED 28" BELOW GROUND LINE. ALL COMPACTION SHALL BE ACCOMPLISHED BY DRIVING OVER EACH SIDE OF SILT FENCE 2-4 TIMES WITH A RUBBER-TIRED VEHICLE.
- 5 FOR MANUAL/TRENCH INSTALLATION, POSTS SHALL BE EMBEDDED 28" BELOW THE TRENCH BOTTOM. ALL COMPACTION SHALL BE ACCOMPLISHED WITH A MECHANICAL OR PNEUMATIC TAMPER.

### 1 SILT FENCE DETAIL

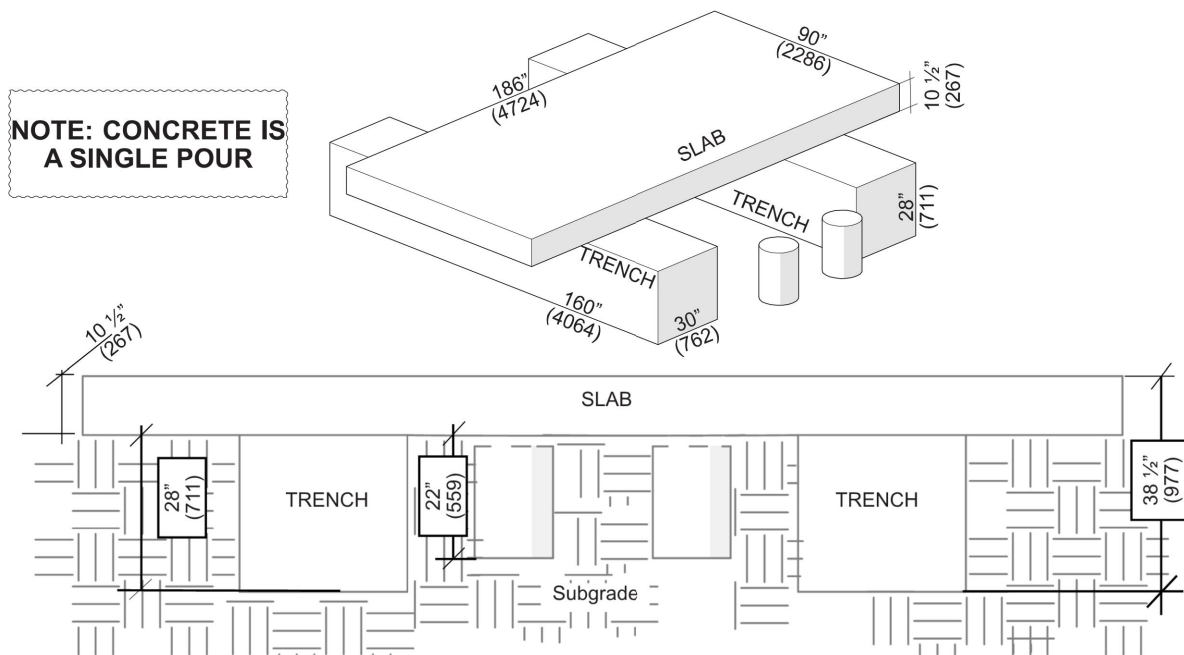
NOT TO SCALE



## CONCRETE FOOTING CHART



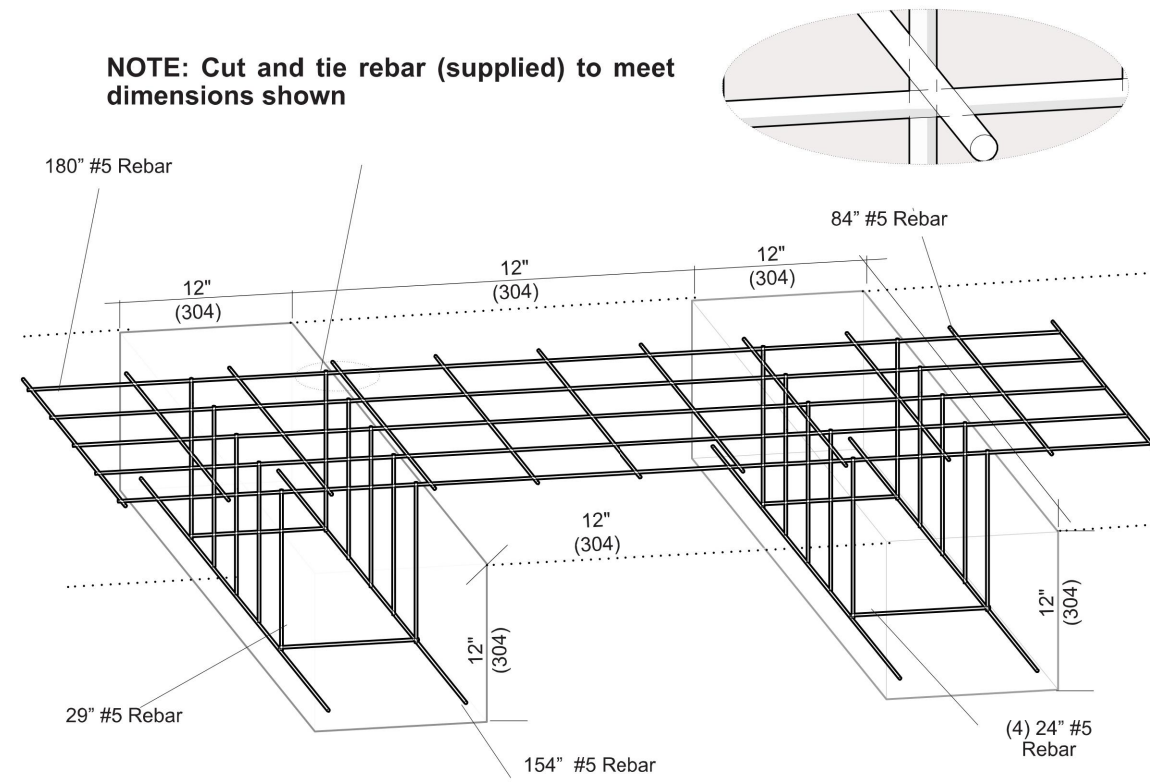
**NOTE: CONCRETE IS  
A SINGLE POUR**



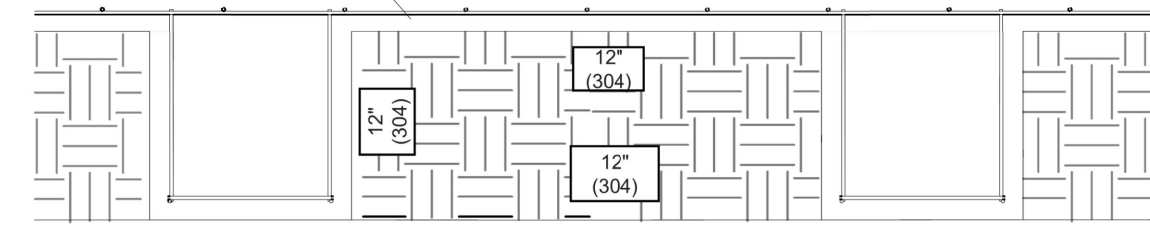
Freestanding Play **We-Go-Swing™ 277778** Page 5  
 601 7th Street South, Delano, Minnesota 55328-8605 888-574-4678 LSI Install Help 888-438-6574 LSI Direct 763-972-5200 Int. FAX (763) 972-3185  
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## FOOTING CHART

**NOTE: Cut and tie rebar (supplied) to meet dimensions shown**



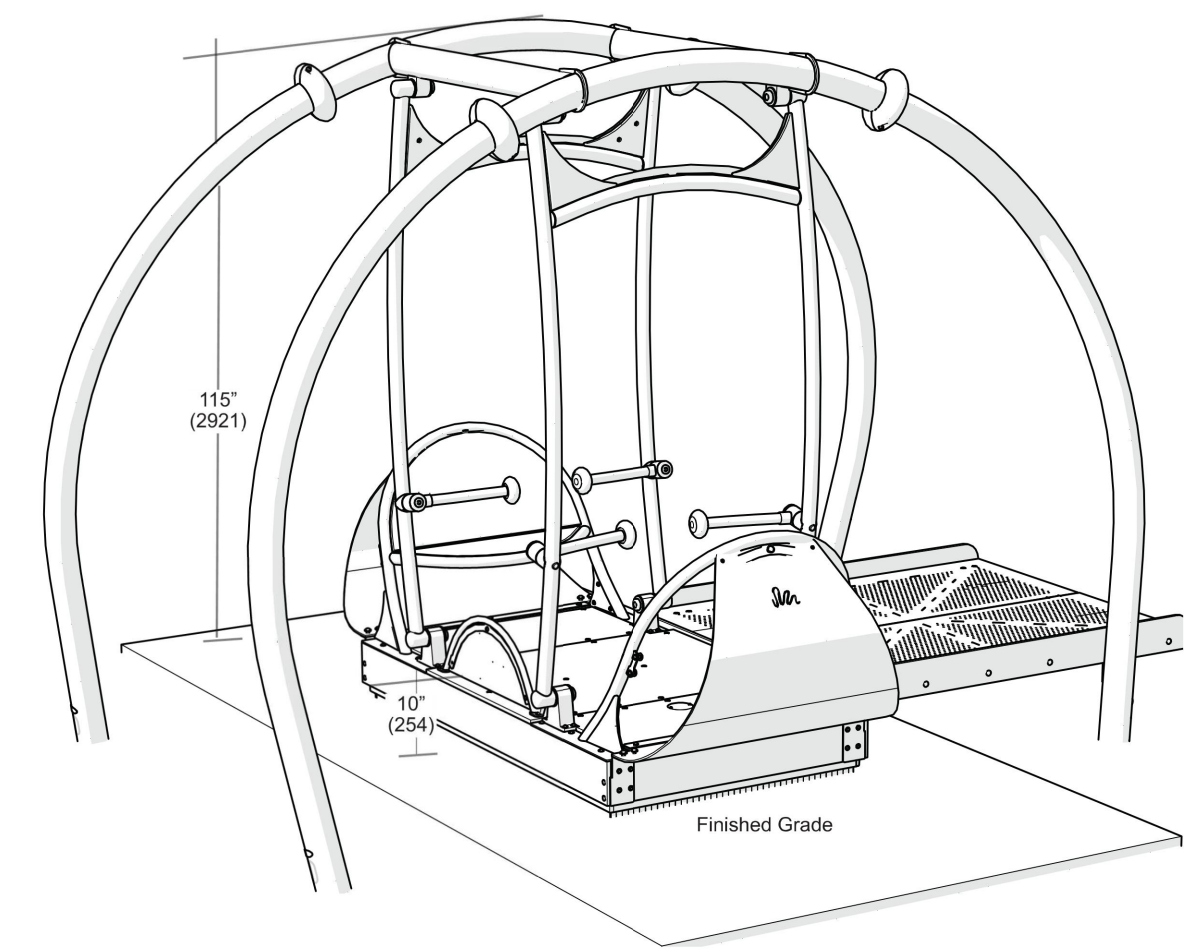
**NOTE: This is the font for notes**



**Freestanding Play** **We-Go-Swing™ 277778** **Page 6**

601 7th Street South, Delano, Minnesota 55328-8605 888-574-4678 LSI Install Help 888-438-6574 LSI Direct 763-972-5200 Int. FAX (763) 972-3185  
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**SAFETY NOTE**  
Choose a protective surfacing material that has a Critical Height Value of at least the height of the Highest Accessible Part/Fall Height of the adjacent equipment. (Ref. ASTM F1487.)

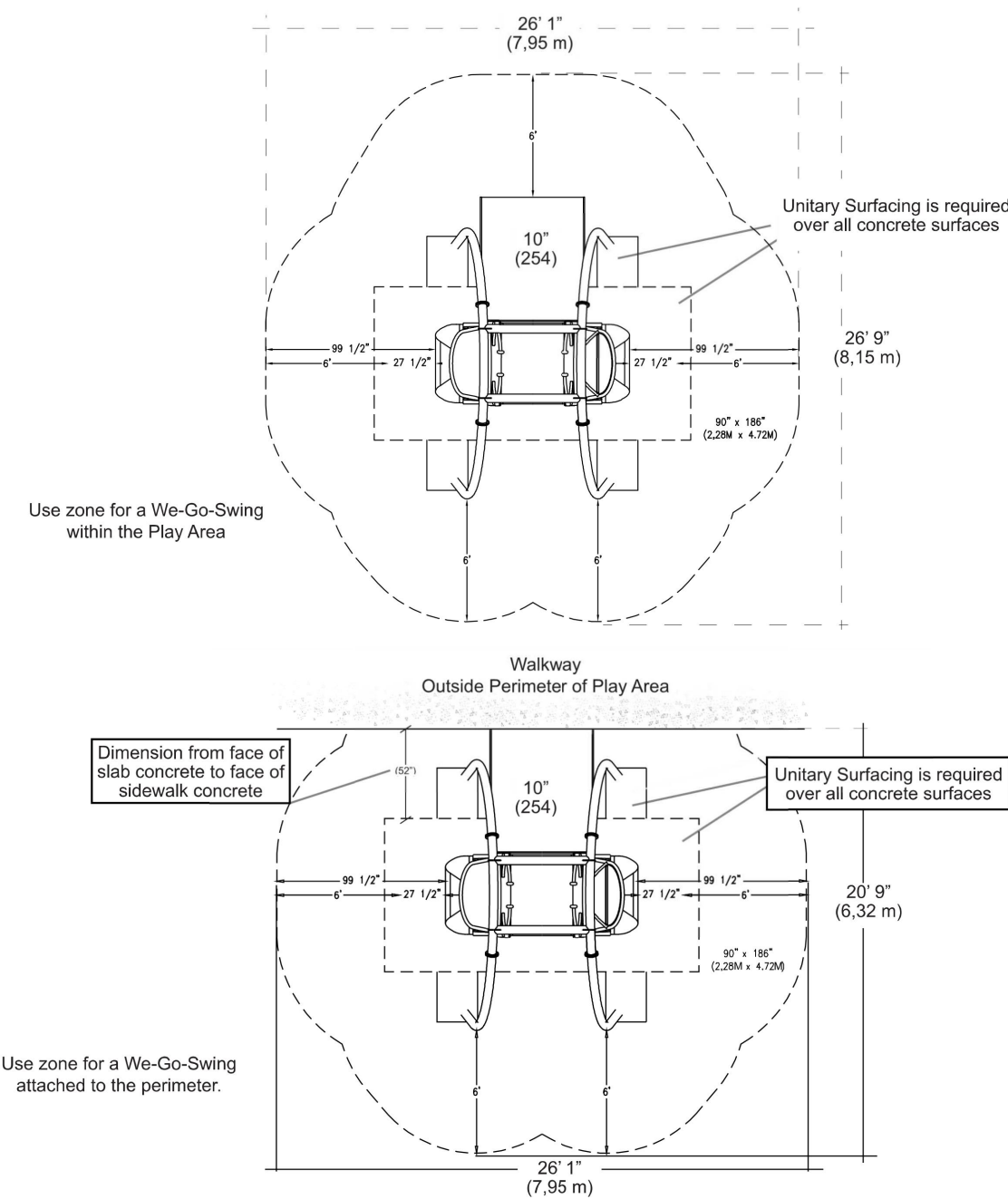


**Freestanding Play** **We-Go-Swing™ 277778** **Page 1**

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**SAFETY NOTE**  
Choose a protective surfacing material that has a Critical Height Value of at least the height of the Highest Accessible Part/Fall Height of the adjacent equipment. (Ref. ASTM F1487.)

## SITE LAYOUT

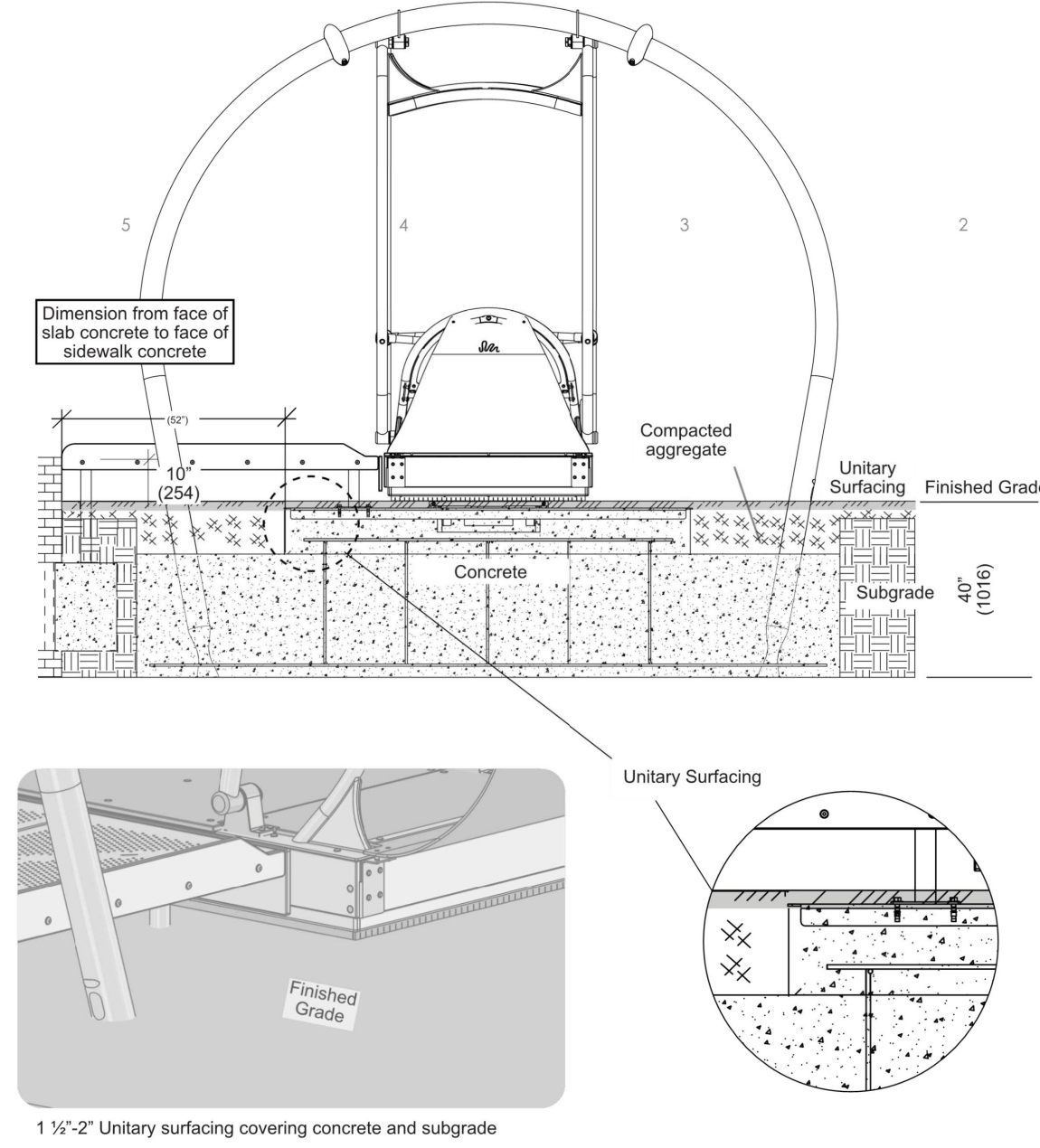


Freestanding Play **We-Go-Swing™ 277778** Page 2

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Choose a protective surfacing material that has a Critical Height Value of at least the height of the Highest Accessible Part/Fall Height of the adjacent equipment. (Ref. ASTM F1487.)

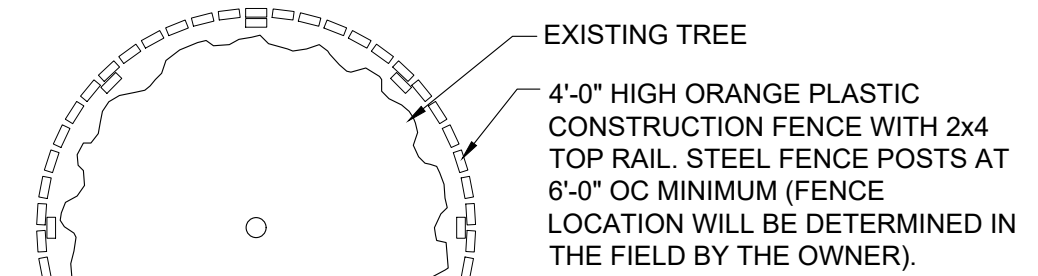
## UNITARY SURFACING CHART



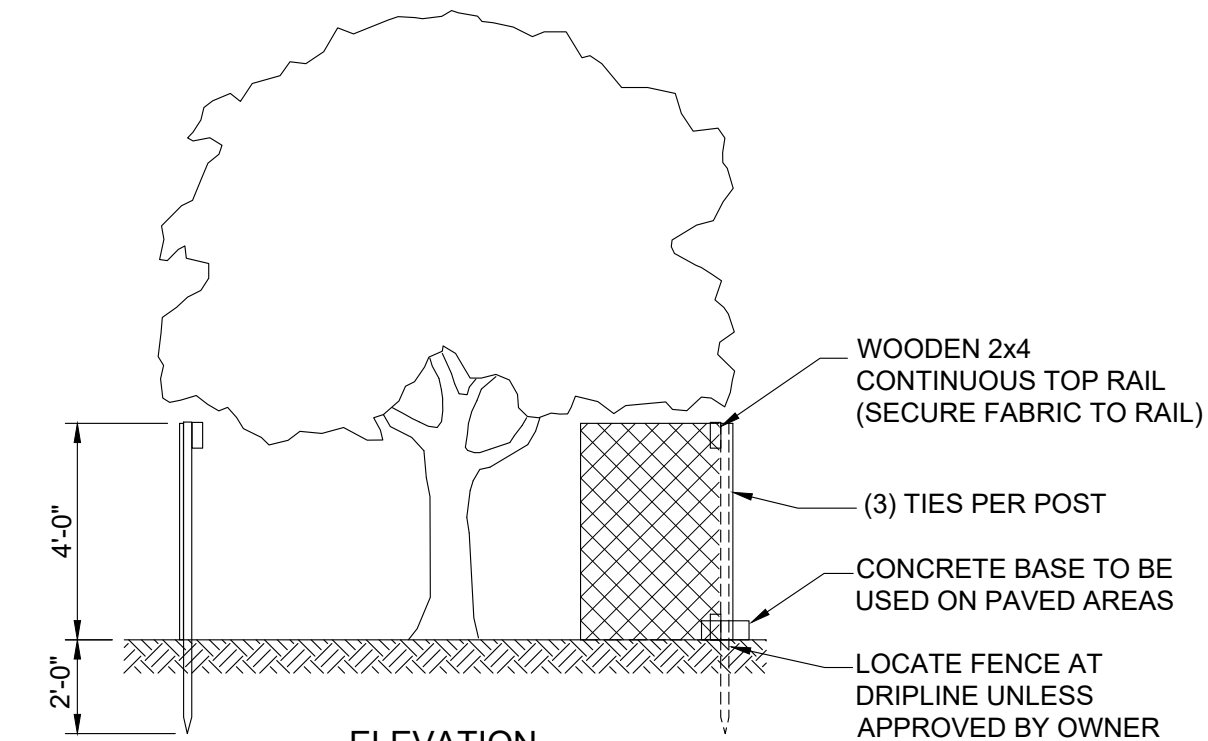
**Freestanding Play** **We-Go-Swing™ 277778** **Page 4**

601 7th Street South, Delano, Minnesota 55328-8605 888-574-4678 LSI Install Help 888-438-6574 LSI Direct 763-972-5200 Int. FAX (763) 972-3185  
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TYPICAL ALL TREES INSIDE WORK AREA EXCEPT  
FOR THOSE SHOWN WITH CHAIN LINK FENCE



## PLAN



ELEVATION

## 2 ADA SWING DETAILS

## 1 TREE PROTECTION FENCE LAYOUT



---

Parks Department  
Vander Veer ADA Swing  
Engineer's Estimate of Cost

	Bid Item	Qty	Unit	Unit Price	Total Cost
1	Topsoil Strip, 4-inch	150	SY	\$ 5.00	\$ 750.00
2	Excavation, Class 10	50	CY	\$ 15.00	\$ 750.00
3	Storm Sewer, HDPE, 6-inch	34	LF	\$ 50.00	\$ 1,700.00
4	Corrugated Subdrain, 4"	90	LF	\$ 8.00	\$ 720.00
5	Area Drain, 15" Basin	1	EA	\$ 2,500.00	\$ 2,500.00
6	Connection to Existing Storm Structure	1	LS	\$ 8,000.00	\$ 8,000.00
7	We-Go-Swing	1	LS	\$ 50,000.00	\$ 50,000.00
8	Drainable Subbase, 5"	75	SY	\$ 40.00	\$ 3,000.00
9	PCC Pad, 4"	75	SY	\$ 70.00	\$ 5,250.00
10	Concrete Perimeter Curb, 12"	96	LF	\$ 40.00	\$ 3,840.00
11	Poured-in-Place Top surface and SBR layer	665	SF	\$ 25.00	\$ 16,625.00
12	Erosion Control, Complete	1	LS	\$ 3,000.00	\$ 3,000.00
13	Seeding, Fertilizing, and Mulching for Hydraulic Seeding	150	SY	\$ 50.00	\$ 7,500.00
14	Construction Layout	1	LS	\$ 3,000.00	\$ 3,000.00
15	Construction Contingency, \$10,000	1	LS	\$ 10,000.00	\$ 10,000.00
<b>SUBTOTAL</b>					<b>\$ 116,635.00</b>

## Unitary Surfacing Slope



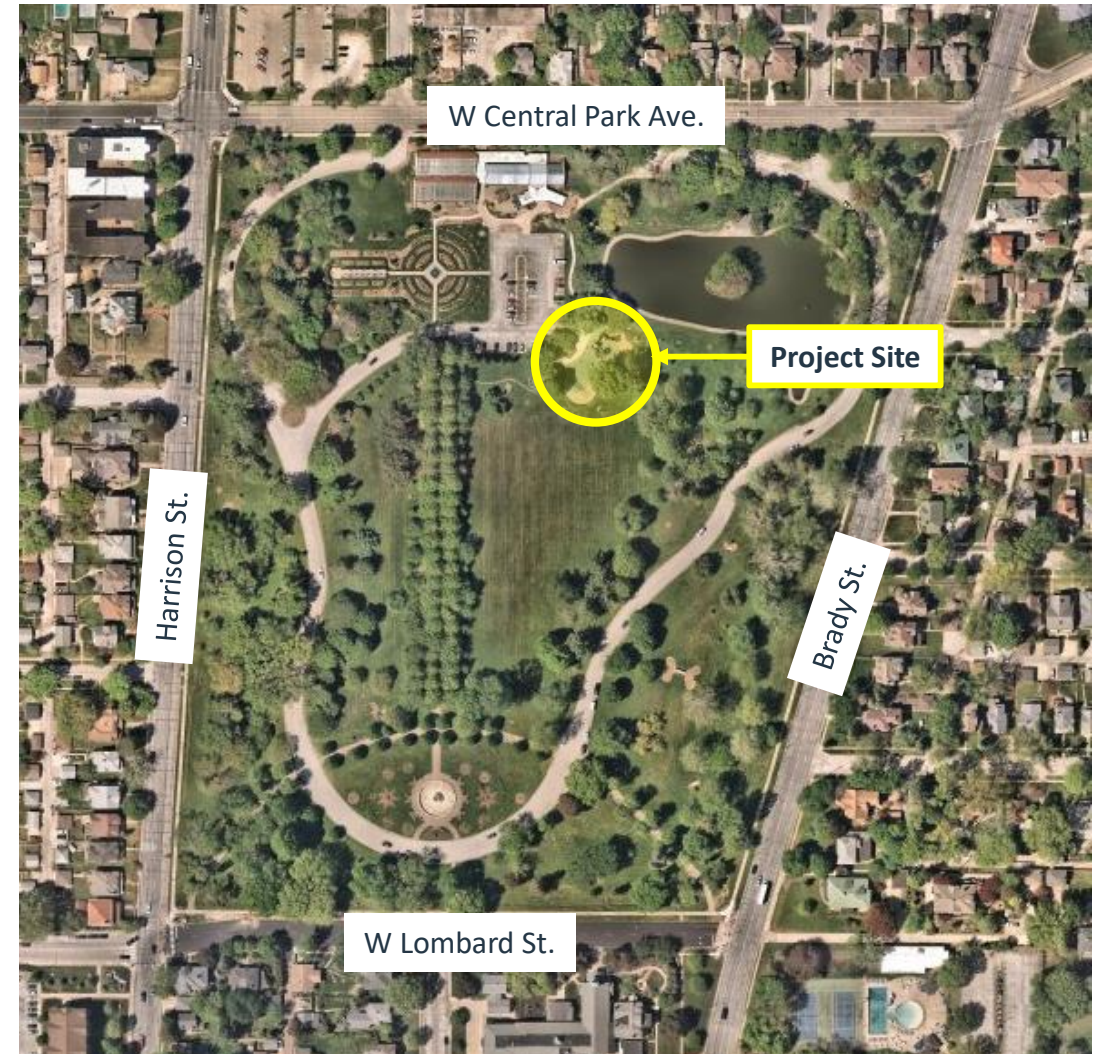






# Vander Veer Park

- Established in 1885
- 33 Acre Park
- Original site of the Scott County Fairgrounds
- **Significance:** City's most important example of landscape planning.



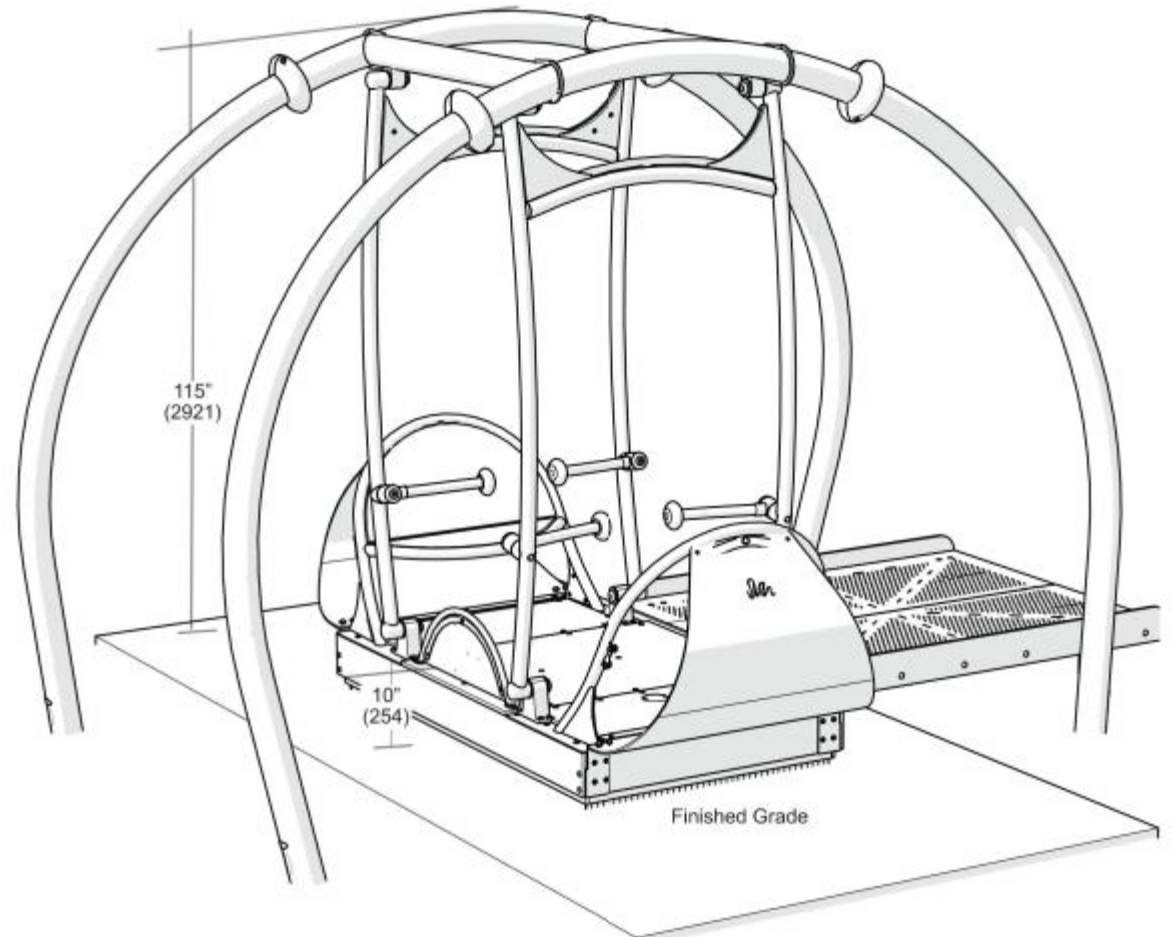


# Project Scope

- Install new ADA accessible swing
- Height: 9'7"
- Poured-in-place rubber playground surface
- Perimeter in concrete curb



**DAVENPORT**  
IOWA | USA





DAVENPORT  
IOWA | USA



## THE HISTORY OF VANDER VEER PARK

Originally this thirty-three acre tract of land, now known as Vander Veer Park, was a fair grounds. There was a half mile race track that ran where the present conservatory is now located.

However, because of the rise from the fairgrounds, it only lasted as a race track for a few years.

The property was then acquired by the Davenport Park Board and dedicated as a park in 1890. It was originally called "Central Park" because it was in the center of the city. The park was like the one in New York City, and was patterned after it in many ways.

The original conservatory was opened in 1897. I was told that the first green house was built on the other side of Central Park Avenue in 1902, and then the west green-house was added in 1910.

The park was named Vander Veer Park in honor of A.W. Vander Veer shortly after his death in 1911. He served as the Park Board's first Secretary. He also donated an elaborate iron fountain at the south entrance which is now closed. Mr. Vander Veer died in office, and naming the park after him was a way to honor him.

An old drawing of the Park, now stored in a park office closet, showed three lagoons in the park, and a grandstand on the Brady Street side. There was also a Wishing Well in the park. The lagoons were drained after a child drowned in one of them. Presently a stone bridge in that area of the park marks the spot where two of the lagoons met. Other features that are gone today include a pavilion where "Teddy" Roosevelt and William Jennings Bryant spoke. It stood where the rose garden is now. When we planted roses we occasionally came upon some of the old cinder sidewalks.



The History of Vander Veer Park - p. 2

There was a wading pool near the southeast corner of the park, which has also been filled in. A display of birds and monkeys was in the old building on the northwest corner of Main Street and Central Park Avenue. This building later served as offices for many years until about 1980 when it was torn down.

There were several other fountains, including one known as the "Horse Fountain" which was especially for horses and dogs. It was located where the present parking lot is located. It was erected in 1897, but gone by 1935. The ornate cast iron fountain at the former entrance where Main Street came into the park was torn down in the 1940's and sold for scrap. The present lighted fountain was erected later on. It features a colored light display.

The old conservatory was condemned and torn down in 1953. The present structure was built in 1954 and dedicated in 1955.

The rose garden was started by the Tri-City Mens' Rose Club in 1945, under the guidance of the late Dr. A. A. Plagmann and John Hanssen. The gardens were renovated a few years ago.

We now have a volunteer organization called "Friends of Vander Veer", which was formed in 1991. The organization was instrumental in installing the present rose garden, and the aerators in the lagoon.

At one time the rose garden contained 2,500 rose plantings. Every year new varieties come from the different rose companies that hybridize roses. These are Hybrid tea roses, floribundas, grandifloras, climbing roses and shrub roses.

I have been associated with Vander Veer Park since 1964, and was put in charge of the rose garden in 1965. I worked under three Park Superintendents - Emil Plambeck, Richard Slatery and Wayne Boyer. I received much of my training from Ralph Clayton who was the head horticulturist. He retired in 1971, and after that I was put in complete charge of all the outside gardens. I was made Head Gardener over all the Vander Veer Horticultural area in 1975, and filled that position until my retirement in 1983.

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founded in 1851, which although annexed to Davenport proper in 1857 has retained an identity of its own. Much of the Fulton area was platted by 1857, with replats in 1867 and 1871. From the 1850's to the turn of the century, the river bank on the south, extending from East Davenport nearly to the Chicago, Rock Island and Pacific Railroad bridge, was dominated by extensive lumber milling, brewing, furniture manufacture, and woolen milling. The Fulton area was home to many who made their living thereby. The wealthier residents occupied the southern portion, from East River Drive to about 11th Street, their large Victorian houses rising one above the other (each with a good view of the river) along Bridge, College and Mississippi Avenues. Immediately South of 11th Street, Mount Ida Female Seminary, located at College and 11th Streets, provided a focal point for development of another residential neighborhood which, like much of East Davenport proper, was inhabited by laborers who worked in the mills along the river. Beginning around 1895, the southeast corner of the Fulton Addition was redeveloped, with a small park on the bluff overlooking the Mississippi River and a number of very large, somewhat ostentatious residences for rising Davenport businessmen and second generations of entrepreneurial and industrial families whose fortunes had begun in the Victorian era.

3. Vander Veer Park. Until the turn of the century, Davenport's physical development was characterized by continual platting and replatting of areas included in city boundaries by 1857. This resulted in a relatively congested, and extremely varied, architectural landscape. Toward the end of the 19th century, however, Davenport finally broke through its traditional "pedestrian city" boundaries, encouraged by a period of prosperity and population growth, and facilitated by electrification and extension of its street railway system. The principal direction of expansion was north, along the Harrison-Main-Brady stem to what had been the Scott County Fairgrounds, 21 blocks north of the river. Development of the fairgrounds as a public park (originally named Central Park) in the 1880's and 1890's fostered residential development in the area as well, primarily for the center- and upper-middle classes. This development extended east of the park, again facilitated by streetcar lines, nearly to the Milwaukee Railroad track, which ran south into East Davenport and to the river. The effective southern edge of this new development was Locust Street, still a major east-west thoroughfare, which marked the upper edge of the old Le Claire Reserve to the south. It was characterized by fairly closely spaced single-family dwellings of comfortable size and stylistic eclecticism, epitomized in the area immediately around Vander Veer Park, which was given added prestige by establishment of the Outing Club southeast of the park in 1891 and construction of a new club house in 1905.



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Vander Veer Park Historic District

**Description:**

The Vander Veer Park historic district is located 22 blocks due north of the Mississippi River at the head of the Harrison-Main-Brady stem which rises out of Davenport's central business district. The focal point of the district is Vander Veer Park, a trapezoid of some 33 acres bounded by Harrison, Central Park Ave., Brady and Lombard streets. This attractive and scrupulously maintained park includes a large greenhouse, duck pond, fountain, botanical gardens, walking paths and a small playground, along with large expanses of grass and towering shade trees.

Facing the park on all four sides are middle- and upper- middle-class houses dating almost exclusively from the 1895-1915 period. Although varying in style from the late Queen Anne to Tudor Revival, they exhibit remarkable consistency of size, spacing and setback, and thus create an attractive frame for the park itself. The architectural theme thus established is carried south along two blocks of Main Street, which intersects Lombard at the south end of the park. Southeast of the park, the theme is extended along the short block of Temple Lane, which is not oriented toward the Park, but rather toward the park-like grounds of the Davenport Outing Club.

Along four-lane Harrison Street, houses are uniformly set back on flat, terraced lots some 5-6 feet above street level. Here, the hipped-roof "foursquare" predominates in frame houses covered with stucco, facebrick or clapboard, particularly below Rusholme. North of Rusholme are several large Craftsman-inspired dwellings, two Tudor style houses, a late Georgian Revival house, and, at the northwest corner of the Park (at Central Park Ave.) an extremely large apartment building in a medieval/Tudor style.

The north side of Vander Veer Park is bounded by Central Park Avenue (this being the park's original name). The houses here range from large bungalows to late Queen Anne and Prairie-inspired versions of the foursquare. This variety continues along Brady, which as U.S. Highway 61 is the city's principal north-south route connecting the central business district with Interstate 80 some 5 miles to the north. Lombard Street, which runs along the south side of the park, is dominated by the multi-structure complex of St. Paul Lutheran church, constructed in 1950-52 to conform to the Georgian Revival style of the E.C. Mueller House (2136 Brady), which is incorporated into the complex as administrative offices.

The pastoral atmosphere of Vander Veer Park and the attractive solidity of its architectural frame are marred only by the density and complexity of

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traffic patterns. A large amount of traffic, including heavy trucks, is routed in and out of the city on Brady Street, with southbound traffic moving via Harrison into the CBD. The primary response of homeowners has been to erect heavy plantings of thick shrubbery and evergreens, which although pleasant to observe in many cases wholly obscure the houses behind them. The traffic pattern of U.S. 61 has been altered in 1984, with north-south traffic is split on alternating sides of the park.

**Significance:**

1. Along Brady Street, which is Davenport's most important and heavily-traveled north-south route, the district functions as the northbound traveller's last, and southbound traveller's first, strong impression of place. North of the district, Brady Street quickly becomes a modern commercial strip almost 5 miles long, indistinguishable from thousands of similar strips that form the introduction to many American towns and cities. As one drives into the city from the north, the first real intimation that one is in Davenport, rather than someplace else, comes at the encounter with Vander Veer Park and the large houses, each slightly advanced beyond the next, along the east side of Brady Street. This transition from modern commercial to turn-of-the-century residential is quite abrupt, and has no counterpart along any of the other major vehicular paths to the city center. Interestingly, the major urban "theme" announced by Vander Veer Park and its architectural frame is that of many large shade trees, which are one of the "old" city's most notable features. Apart from the CBD and industrial areas, nearly all Davenport's 19th and early 20th century neighborhoods are to a greater or lesser extent shaded, from the "central city" area of lower Le Claire's reserve to the wealthy neighborhood of McClellan Heights. The Vander Veer Park district also suggests another prominent theme, that of irregular and often rugged topography which is characteristic of much of the city from 4th street north to about 12th street.

2. Within the district, Vander Veer Park itself is significant as perhaps the city's most important example of landscape planning, and representative of parks and civic improvements that were prominent features of the city's late 19th and early 20th century municipal development. Originally site of the Scott County Fairgrounds, the area between Harrison and Brady north of Lombard was acquired by the city in 1885. Renamed Central Park in 1890, the area further reflected the inspiration of its New York namesake in its "naturalistic" landscaping and multiple uses, which included not only a "glasshouse" and floral gardens, but a large pond (ideal for skating in winter), picnic pavilion, picturesque bridge (the latter two no longer



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extant) and "winding macadam driveways" which found favor, by 1905, with "pleasure-seeking automobilists." Vander Veer Park was the first major park established by the City of Davenport. With the organization of the Board of Parks Commissioners in 1890, additional acquisitions followed: Riverview Terrace and Prospect parks in 1894, and the 33-acre Fejervary Park, on the city's west side, in 1902. This program preceded several other major projects in city beautification, most notably inauguration of a large-scale riverfront improvement that included the Petersen Memorial Music Pavilion, Dillon Fountain, and Union Station between 1911 and 1931. During the 1930's, the Civil Works Administration crews altered the plan of Vander Veer Park by removing corner entrances at Brady and Lombard, and at Harrison and Lombard. The early street lights and iron fountain were replaced with modern light standards and a stone-and-concrete, electrically-operated fountain.

3. Establishment of Central Park (renamed Vander Veer Park in 1911 in honor of an early Parks Commissioner), inevitably increased the desirability and cost of adjacent land for residential development. The Central Park and Central Park Second additions (on the east side of Brady) were platted in 1891 and 1896, respectively, followed by the Outing Club and Temple Lane additions on the south, and two Norwood Park additions west of Harrison after 1900. Development of the area, as well as neighborhoods further east, was considerably enhanced by electrification of Davenport's streetcar system (1888) and its expansion into residential districts beyond the old city core. The Brady Street line, from 2nd Street to Central Park, was in fact one of the city's first streetcar routes, having been established in 1870 as a horsecar line when Central Park was still the county fairgrounds. By the late 19th century, the line offered an easy commute to a growing central business district for businessmen and middle-class office workers.

The relative affluence of the district's inhabitants was reflected, in part, by the number of residences designed by prominent local architects, three of whom lived near the park. Rudolph Clausen, heir to a family architectural practice begun in 1870, lived in a relatively modest Tudor-style house at 2330 Harrison. His firm (Clausen & Burrows) designed the nearby H.G. Pape house (2326 Harrison). The firm of Temple and Burrows is represented in the Georgian Revival Mueller House (2136 Brady) across Brady from the Outing Club (another of the firm's designs). Another architect, Edward S. Hammatt, best known for his institutional designs for Trinity Episcopal Parish in the 1880's, built a residence in the district as well (2313 Brady). The third architect was Gustav Hanssen (124 Rusholme), who also contributed a pair of unusual neoclassical-inspired houses on Brady (2317 and 2319 Brady). Hanssen worked in partnership with

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Dietrich Harfst. Unlike Hanssen, who appears to have favored neoclassical forms, Harfst seems to have preferred the Craftsman idiom, as exemplified in the Henry Deutsch (2101 Main) and C.E. Hanssen (2322 Harrison) houses. A third house attributed to the firm is the Henry Heubotter house (2116 Main), a rambling version of the American foursquare.

These houses are among the most significant in the Vander Veer Park district, illustrating the eclecticism that characterized much of Davenport's domestic architecture at the turn of the century; and also the ways in which local architects adapted national trends to their own, and their clients' tastes. Other important houses, the architects of which are not yet known, include the excellent Tudor style Harvey Jones House (2221 Brady) and an outstanding Prairie house (2207 Brady). The three houses on Temple Lane, facing the Outing Club, are also noteworthy, combining a common hipped-roof, rectangular form with Mission, Prairie and neo-Georgian influences, respectively. The E.P. Adler (2104 Main) and William Weise (2110 Brady) houses, located about a block apart, also have this rectilinear character. Remarkably similar in their exterior treatment, they combine Prairie and neo-Georgian features and were likely the work of the same architect.



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Vander Veer Park Historic District

**Verbal Boundary Description**

Beginning at northeast corner of property at 2525 Brady St.; thence southwest along rear property lines of properties fronting on Brady Street, crossing E. Dover Court, E. Denison, and E. Rusholme to alley behind 2207 and 2221 Brady St.; south along this alley to left turn of alley; then east along alley (which parallels north property lines of properties on north side of Temple Lane) to northeast corner of property at 3 Temple Lane; then south along east property line of 3 Temple Lane and the Outing Club to the southeast corner of the Outing Club property; then west to Brady Street; then north on Brady Street to W. High St.; then west on W. High St. to alley between Harrison and Main Streets; then north along this alley to southeast corner of property at 227 W. Lombard; then west along rear property lines of properties fronting on W. Lombard to Harrison St.; then north on Harrison to W. Lombard; then west on W. Lombard to alley between Harrison and Ripley streets; then north along this alley, crossing W. Rusholme and W. Dover Court to W. Central Park Ave.; then east on W. Central Park Ave. to Harrison; then north on Harrison to alley between W. Central Park Ave. and W. Columbia Ave.; then east along this alley, crossing Main, Sheridan and Fair streets to Brady Street; then south on Brady to E. Central Park Ave.; then east on E. Central Park Ave. to beginning.

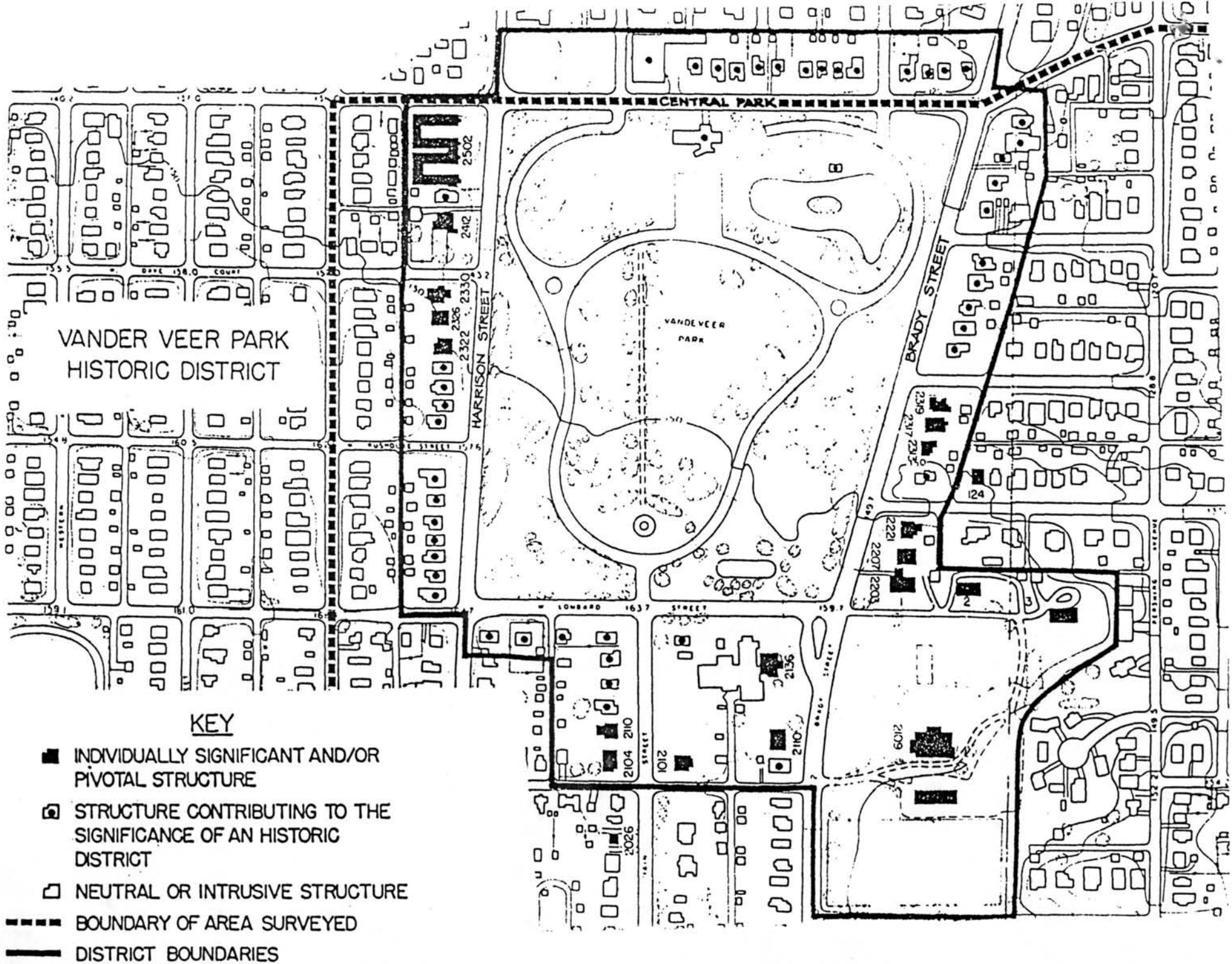
**UTM References:**

- A 15/702520 4602000
- B 15/702620 4601560
- C 15/702520 4601320
- D 15/702380 4601310
- E 15/702060 4601460
- F 15/702060 4601980

70-81 acres

FLORIDA, Duval County, Jacksonville, House at 7246 San Carlos (San Jose Estates TR), 7246 San Carlos (04/10/85)  
FLORIDA, Duval County, Jacksonville, House at 7246 St. Augustine Road (Truman House) (San Jose Estates TR), 7246 St. Augustine Rd. (04/10/85)  
FLORIDA, Duval County, Jacksonville, House at 7249 San Pedro (San Jose Estates TR), 7227 San Pedro Rd. (04/10/85)  
FLORIDA, Duval County, Jacksonville, House at 7288 San Jose Boulevard (San Jose Estates TR), 7288 San Jose Blvd. (04/10/85)  
FLORIDA, Duval County, Jacksonville, House at 7306 St. Augustine Road (San Jose Estates TR), 7306 St. Augustine Rd. (04/10/85)  
FLORIDA, Duval County, Jacksonville, House at 7317 San Jose Boulevard (San Jose Estates TR), 7317 San Jose Blvd. (04/10/85)  
FLORIDA, Duval County, Jacksonville, House at 7330 Ventura Avenue (San Jose Estates TR), 7330 Ventura Ave. (04/10/85)  
FLORIDA, Duval County, Jacksonville, House at 7356 San Jose Boulevard (San Jose Estates TR), 7356 San Jose Blvd. (04/10/85)  
FLORIDA, Duval County, Jacksonville, House at 7400 San Jose Boulevard (San Jose Estates TR), 7400 San Jose Blvd. (04/10/85)  
FLORIDA, Duval County, Jacksonville, San Jose Administration Building (San Jose Estates TR), 7423 San Jose Blvd. (04/10/85)  
FLORIDA, Duval County, Jacksonville, San Jose County Club (San Jose Estates TR), 7529 San Jose Blvd. (04/10/85)  
FLORIDA, Duval County, Jacksonville, San Jose Hotel (San Jose Estates TR), 7400 San Jose Boulevard (04/10/85)  
FLORIDA, Okeechobee County, Okeechobee, Freedman-Raulerson House, 600 S. Parrott Ave. (04/11/85)  
FLORIDA, Palm Beach County, West Palm Beach, Mickens House, 801 4th St. (04/11/85)  
FLORIDA, St. Lucie County, Ft. Pierce, Cresthaven, 239 S. Indian River Dr. (04/11/85)  
  
ILLINOIS, Carroll County, Milledgeville vicinity, Steffens, Joseph, House, Off of Elkhorn Rd. (04/10/85)  
ILLINOIS, Cook County, Kenilworth, Wild Flower and Bird Sanctuary in Mahoney Park, Sheridan Rd. (04/10/85)  
  
INDIANA, Huntington County, Huntington, German Reformed Church, 202 Etna Ave. (04/11/85)  
  
IOWA, Cherokee County, Cherokee, Cherokee Public Library (Public Library Buildings in Iowa TR), 215 S. 2nd St. (04/09/85)  
IOWA, Dubuque County, Dubuque, Andrew-Ryan House, 1375 Locust (04/11/85)  
IOWA, Henry County, Mt. Pleasant, Allen, G. W. S., House, 207 E. Henry St. (04/11/85)  
IOWA, Jones County, Scotch Grove vicinity, Corbett's/Eby's Mill Bridge, Spans Maquoketa River, Scotch Grove Township (04/11/85)  
IOWA, Mahaska County, Oskaloosa, McMullin, Major James W., House, 403 First Ave. East (04/11/85)  
IOWA, Pottawattamie County, Council Bluffs, Haymarket Commercial Historic District, S. Main St. (04/11/85)  
IOWA, Scott County, Davenport, McHarg, Joseph S., House (Davenport MRA), 5905 Chapel Hill Rd. (04/09/85)  
IOWA, Scott County, Davenport, Schricker, John, House (Davenport MRA), 5418 Chapel Hill Rd. (04/09/85)  
IOWA, Scott County, Davenport, Vander Veer Park Historic District (Davenport MRA), Roughly bounded by Temple Lane, W. Central Park Ave., Brady, High, and Harrison Sts. (04/09/85)









## **Historic Preservation Ordinance**

### **Section 14.01.060 Certificate of Appropriateness Review Process**

**C. Commission review process** - Standards for review. In considering an application for a certificate of appropriateness, the commission shall be guided by the following general standards in addition to any other standards or guidelines established by ordinance for a local landmark or historic district. In all cases, these standards are to be applied in a reasonable manner, taking into full consideration the issue of economic feasibility and other technical considerations.

1. Every reasonable effort shall be made to make the minimal number of changes necessary to maintain a designated property in a good state of repair, thereby minimizing the impact of the proposed alteration; and
2. The removal, alteration or concealing of distinguishing exterior architectural features and historic material of a designated property should be avoided when possible; and
3. All designated property shall be recognized as a product and physical record of its time, place and use. Changes that create a false sense of historical development, such as adding conjectural architectural features shall be discouraged; and
4. Most properties change over time, and those changes that have acquired architectural and/or historical significance in their own right shall be recognized, respected and retained; and
5. Distinctive architectural features, construction techniques and/or examples of craftsmanship that characterize a designated property shall be treated with due consideration; and
6. Deteriorated architectural features should, where possible, be repaired rather than replaced. Where the severity of deterioration requires replacement, the new feature shall match the old in design, color, texture and other visual qualities and, where possible, materials. Replacement of missing features shall be substantiated by documentary, physical or pictorial evidence; and
7. Activities that cause deterioration of a designated property and its architectural features shall be discouraged. In those cases where the damage would be irreversible, such as sand-blasting and wet blasting fire-hardened bricks, the activities shall be prohibited. If cleaning is to be done, the gentlest means possible shall be encouraged; and
8. Known significant archeological resources possibly affected by a proposed activity shall be protected and preserved. If such resources must be disturbed, mitigation measures shall be undertaken; and
9. New additions and related new construction shall not be discouraged when such improvements do not destroy historic material and such design is compatible with the size, massing, scale, color, materials and character of the property, neighborhood and district, if applicable.

**D. Design criteria to implement review standards.** When the commission is considering an application for a certificate of appropriateness, it shall consider the following architectural design criteria, or elements of design as they relate to the standards for review prescribed in Section 14.01.040C.

1. Height. The height of any proposed addition, construction or reconstruction should be compatible with the designated property and the surrounding structures, if located within a designated historic district; and
2. Proportions. The proportions (width versus height relationship) between doors and windows should be compatible, if not replicated, with the architectural design and character of the designated property; and
3. Scale. A proposed alteration, construction, reconstruction or addition should not negatively impact the scale of the designated property or district; and
4. Materials. Historic or original architectural features, or replacement elements which in all ways replicated the original, should be repaired whenever possible; and
5. Relationship of building masses and spaces. The relationship of a structure within a designated historic district to the rear, side and front yards between it and surrounding structures should be compatible; and
6. Roof shape. The roof design and shape should remain consistent with its original configuration and character; and
7. Site improvements. Landscaping and other site improvements, including off-street parking, should have as minimal of an impact as possible to the designated property's original plan/layout and its visual character.



City of Davenport  
Historic Preservation Commission

Department: Development & Neighborhood Services  
Contact Info: Matt Werderitch 563.888.2221

**Date**  
**1/10/2023**

**Subject:**

Case COA23-03: Request for exterior alteration at 521 West 6th Street. The Dr. Gustav Hoepfner House and Office is located within the Hamburg Local Landmark Historic District. ILIOWA Holdings LLC, petitioner. [Ward 3]

**Recommendation:**

A recommendation is made to approve the structural elements identified in the Certificate of Appropriateness Application at 521 West 6th Street. However, Staff recommend the Commission provide direction on acceptable and consistent building materials.

The Commission should review the project for conformance with the Standards for Review, Chapter 14.01.060C of the Davenport Municipal Code. The following are applicable criteria:

1. Every reasonable effort shall be made to make the minimal number of changes necessary to maintain a designated property in a good state of repair, thereby minimizing the impact of the proposed alteration.
2. Deteriorated architectural features should, where possible, be repaired rather than replaced. Where the severity of deterioration requires replacement, the new feature shall match the old in design, color, texture and other visual qualities and, where possible, materials. Replacement of missing features shall be substantiated by documentary, physical or pictorial evidence.

**Background:**

The Dr. Gustav Hoepfner House and Office is listed as a contributing structure in the Hamburg Historic District. The residence was constructed in 1874 and has primarily been used as a rental. The one-story building with an exposed basement on the rear currently contains four apartment units.

There have been three additions placed on the structure throughout the years, which have diminished the original character of the home. As a result, there is a mismatch of windows styles and siding types. The lack of architectural continuity is most apparent on the south (rear) elevation.

**Request:**

The home owner received funding from Davenport's DREAM Program for exterior repairs and improvements. The following scope of work is proposed:

1. Install pre-hung insulated metal door with all needed modifications to the opening. Include new lockset, hardware, and dead bolt lock. Door will feature a window and be residential in character.
2. Construct an exterior wall with studs 16" on center, 1/2" sheathing and bracing. Cover with proper waterproof membrane and install vinyl siding as necessary. The wall is located under the south deck.
3. Replace rotted and deteriorated soffit and fascia to allow for aluminum wrap installation.
4. Cover fascia and soffit with aluminum, using V-groove soffit.
5. Wrap window and door casings with aluminum. Caulk all joints.

6. Cover each front porch canopy ceiling with aluminum. Wrap or install siding for front façade of canopy.

**Staff's Response:**

The proposed scope of work does not appear to diminish the character of the original structure, since the alterations are largely performed on the south basement wall. It is difficult to determine whether the materials being replaced are from the building's initial construction in 1874. The existing basement door is not original.

- Since there have been several additions to the home, there is a mismatch of wood, vinyl, and asbestos siding. The applicant is proposing to replace the deteriorated wood siding around the basement door with vinyl. The total wall spans approximately four feet in width, with the door covering a majority of the area. An engineered wood siding may be a more appropriate material given the scope of the project.

- Similar to the siding, there is also a mismatch of soffit and fascia materials. Sections of the building have aluminum wrap, while the original structure appears to have wood. While the installation of aluminum soffit and fascia will bring uniformity to the structure, a wood or engineered wood product may be more appropriate with the character of the district.

- Staff believe the proposed alterations will not negatively impact the historic character of the Hamburg District. The improvements will bring the property into a good state of repair.

**ATTACHMENTS:**

Type	Description
▣ Backup Material	Background Materials & Photos
▣ Backup Material	Application
▣ Backup Material	Work Plan
▣ Backup Material	Historic Property Inventory Sheet
▣ Backup Material	Historic Preservation Ordinance-Review Standards

**REVIEWERS:**

Department	Reviewer	Action	Date
Community Planning & Economic Development	Werderitch, Matt	Approved	1/6/2023 - 2:08 PM



# Dr. Gustav Hoepfner House & Office

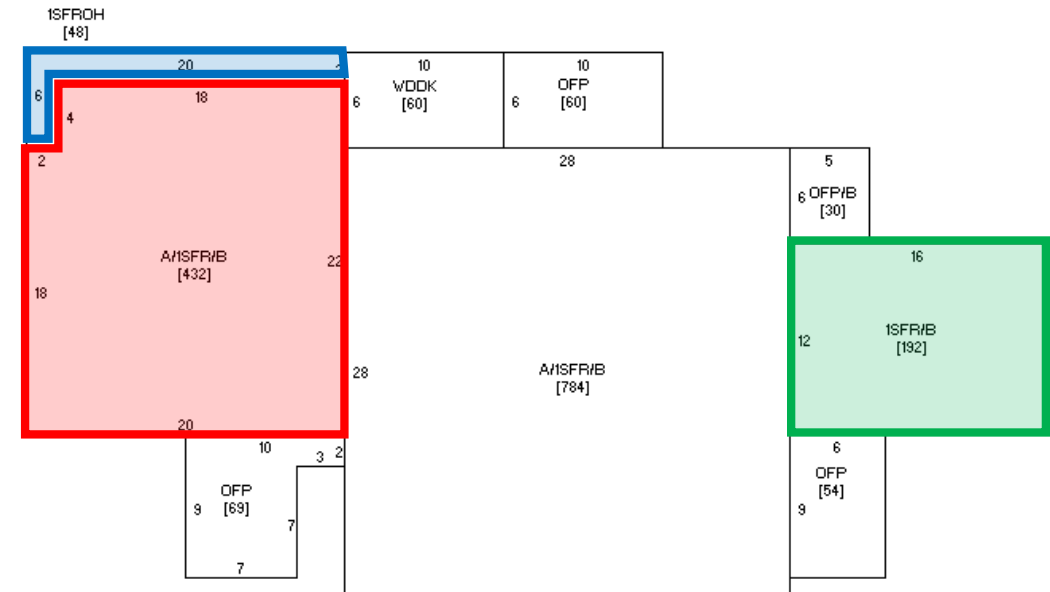
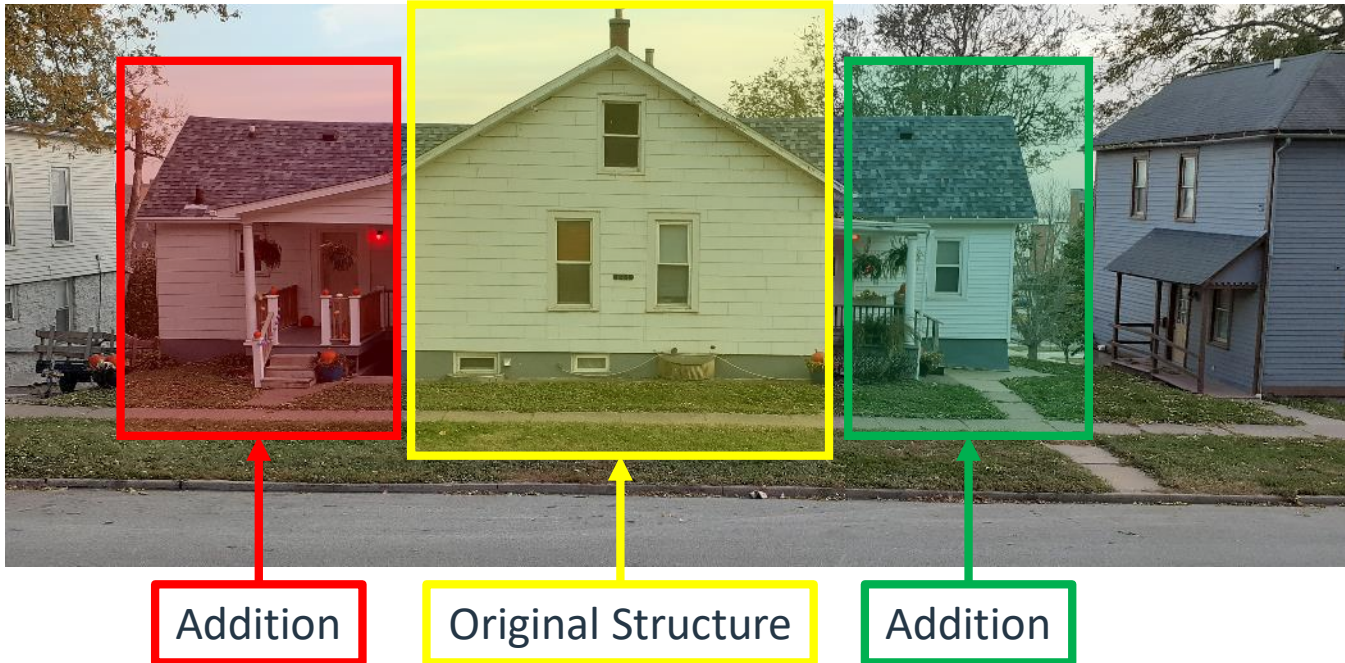
- Built in 1874
- Contributing Structure in the Hamburg Historic District
- Current Use: 4 Apartment Units
- Received DREAM Program Funding





# Previous Alterations

- There have been 3 additions to the home throughout the years, which have diminished the original character of the residence.
- As a result, there is a mismatch of windows styles, siding types & building materials.
- The lack of architectural continuity is most apparent on the south (rear) elevation.





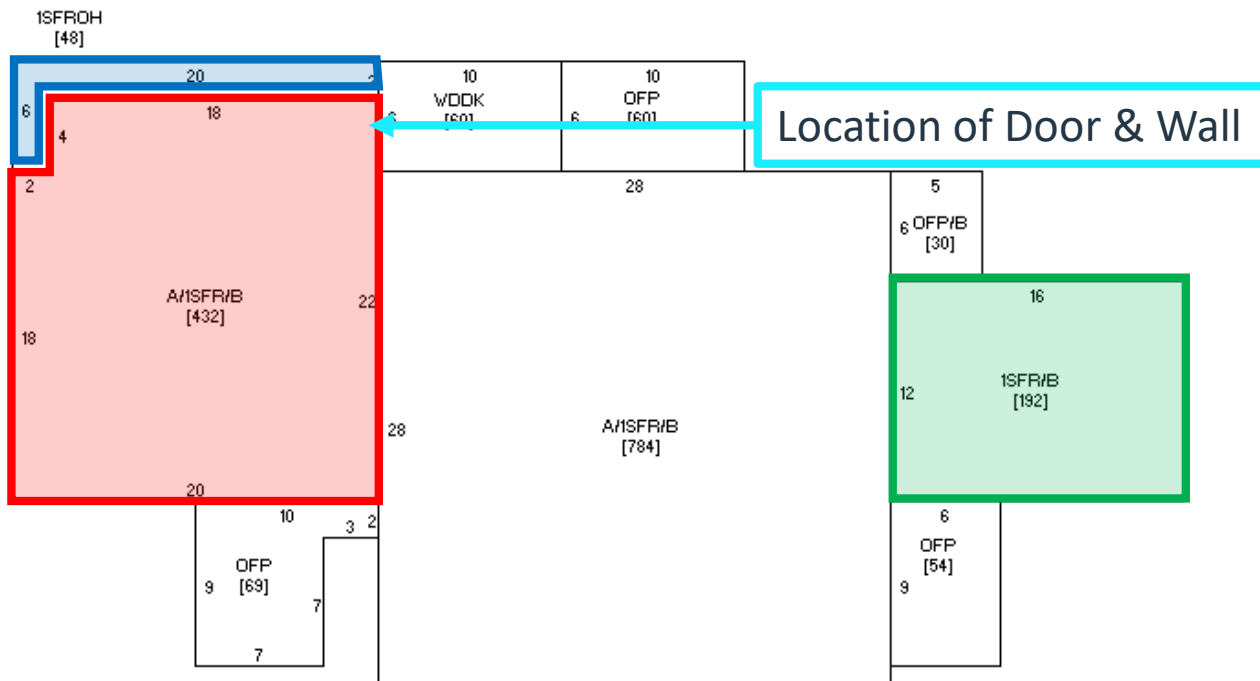
# Project Scope

1. Install pre-hung insulated metal door with all needed modifications to the opening. Include new lockset, hardware, and dead bolt lock. Door will feature a window and be residential in character.
2. Construct an exterior wall with studs 16" on center, 1/2" sheathing and bracing. Cover with proper waterproof membrane and install vinyl siding as necessary. The wall is located under the south deck.
3. Replace rotted and deteriorated soffit and fascia to allow for aluminum wrap installation.
4. Cover fascia and soffit with aluminum, using V-groove soffit.
5. Wrap window and door casings with aluminum. Caulk all joints.
6. Cover each front porch canopy ceiling with aluminum. Wrap or install siding for front façade of canopy.



# Wall Reconstruction & Door Replacement

- South Elevation; Basement.
- Located underneath the rear porch.
- Not part of the original historic structure.



Replacement Door



Existing Door & Wall



# Siding Materials

Mismatch of Siding:

1. Wood
2. Asbestos
3. Vinyl

The applicant is proposing to replace the deteriorated wood siding around the basement door with vinyl.

Wall surface is approximately 4'

An engineered wood siding may be a more appropriate material given the scope of the project.





# Soffit & Fascia

- Mismatch of Soffit and Fascia Materials: Wood & Aluminum
- Sections of the building have aluminum wrap, while the original structure appears to have wood.
- While the installation of aluminum soffit and fascia will bring uniformity to the structure, a wood or engineered wood product may be more appropriate with the character of the district.







Complete application can be emailed to [planning@davenportiowa.com](mailto:planning@davenportiowa.com)

**Property Address\***

\*If no property address, please submit a legal description of the property.

**Applicant (Primary Contact)**

Name:	<input type="text"/>
Company:	<input type="text"/>
Address:	<input type="text"/>
City/State/Zip:	<input type="text"/>
Phone:	<input type="text"/>
Email:	<input type="text"/>

*Application Form Type:*

**Plan and Zoning Commission**

Zoning Map Amendment (Rezoning)	<input type="checkbox"/>
Planned Unit Development	<input type="checkbox"/>
Zoning Ordinance Text Amendment	<input type="checkbox"/>
Right-of-way or Easement Vacation	<input type="checkbox"/>
Voluntary Annexation	<input type="checkbox"/>

**Owner** (if different from Applicant)

Name:	<input type="text"/>
Company:	<input type="text"/>
Address:	<input type="text"/>
City/State/Zip:	<input type="text"/>
Phone:	<input type="text"/>
Email:	<input type="text"/>

**Zoning Board of Adjustment**

Zoning Appeal	<input type="checkbox"/>
Special Use	<input type="checkbox"/>
Hardship Variance	<input type="checkbox"/>

**Design Review Board**

Design Approval	<input type="checkbox"/>
Demolition Request in the Downtown	<input type="checkbox"/>
Demolition Request in the Village of East Davenport	<input type="checkbox"/>

**Engineer** (if applicable)

Name:	<input type="text"/>
Company:	<input type="text"/>
Address:	<input type="text"/>
City/State/Zip:	<input type="text"/>
Phone:	<input type="text"/>
Email:	<input type="text"/>

**Historic Preservation Commission**

Certificate of Appropriateness	<input type="checkbox"/>
Local Landmark Nomination	<input type="checkbox"/>
Demolition Request	<input type="checkbox"/>

**Architect** (if applicable)

Name:	<input type="text"/>
Company:	<input type="text"/>
Address:	<input type="text"/>
City/State/Zip:	<input type="text"/>
Phone:	<input type="text"/>
Email:	<input type="text"/>

**Administrative**

Administrative Exception	<input type="checkbox"/>
Health Services and Congregate	
Living Permit	<input type="checkbox"/>

**Attorney** (if applicable)

Name:	<input type="text"/>
Company:	<input type="text"/>
Address:	<input type="text"/>
City/State/Zip:	<input type="text"/>
Phone:	<input type="text"/>
Email:	<input type="text"/>

## Historic Resource:

- ☐ Local Hamburg Historic District
- ☐ Iowa Soldier's Orphans' Historic District
- ☐ Marycrest College Historic District
- ☐ Individually Listed Local Historic Landmark

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Historic Resources requiring a Certificate of Appropriateness are mapped with a  or .

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## When is a certificate of appropriateness required?

Prior to the commencement of the work.

## What type of activity requires the approval of a certificate of appropriateness?

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## Submittal requirements

- Please contact Planning staff at (563) 326-6198 or [planning@davenportiowa.com](mailto:planning@davenportiowa.com) so we can help you determine what exactly is required to be submitted.
- Incomplete applications will not be accepted.

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- A work plan that accurately and completely describes the work to be done.
- Color digital photographs depicting the building elevations and proposed construction.

### Submittal requirements for specific types of requests:

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- Specifications, including dimensions, material used and color of the material.
- A rendering of the proposed alteration as depicted on the existing building.
- Samples of the materials, including the color, along with scaled, accurately colored elevations for any proposed sign and/or sign package.

#### Minor additions, site improvements and outdoor storage areas (all of the above and):

- A dimensioned site plan, including the locations of any proposed or existing buildings on the subject parcel and on surrounding parcels.
- A preliminary grading plan showing before and after grades at two-foot contour intervals, where deemed necessary by the development official.
- Outdoor storage areas shall be reflected in the elevation drawings submitted and shall show their relationship to the building elevations as well as the materials and treatment proposed that would accurately reflect the screening of the storage areas.
- A landscape plan.



Major additions and new buildings (all of the above and):

- Reproductions of building or site information found in the historical surveys if applicable
- A verifiable legal description, or a land survey.
- A map showing the existing topography of other properties at two-foot contour intervals, extending one hundred feet from the subject parcel.
- Elevation drawings, in color and drawn to scale, of the front, sides, rear, and roof lines of all proposed buildings or structures, illustrating the appearance and treatment of required screening elements for roof-mounted equipment, where deemed necessary by the development official.
- A materials board containing samples of each type of exterior building materials.

**Formal Procedure**

(1) Application:

- Prior to submission of the application, the applicant shall correspond with Planning staff to discuss the request, potential alternatives and the process.
- The submission of the application does not constitute official acceptance by the City of Davenport. Planning staff will review the application for completeness and notify the applicant that the application has been accepted or additional information is required. Inaccurate or incomplete applications may result in delay of required public meetings.

(2) Historic Preservation consideration of the request:

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- If the Commission determines there is insufficient information to make a proper judgment on the application, it may continue the application as long a period of 60 days has not elapsed from an accepted application. This time period does not apply if the applicant requests the continuance.

(3) After the Historic Preservation Commission's decision:

- If approved, a certificate of appropriateness does not constitute a City permit or license and does vest against any other land development regulation or regulatory approval. You will need to contact Davenport Public Works and other regulatory agencies regarding permits and/or licenses.
- If approved, a certificate of appropriateness will expire one year from the date of approval unless a building permit is obtained within such period. The Zoning Administrator may grant an extension for a period of validity longer than one year. An applicant may apply in writing for an extension of time at any time prior to the date of expiration.
- The applicant may appeal the Historic Preservation Commission's determination to the City Council. A written appeal along with payment of \$75.00 must be submitted to the Zoning Administrator within thirty calendar days of the Historic Preservation Commission's decision.

Applicant:  Date:

By typing your name, you acknowledge and agree to the aforementioned submittal requirements and formal procedure and that you must be present at scheduled meetings.

Received by:  Date:

Planning staff

Date of the Public Meeting:

Meetings are held in City Hall Council Chambers located at 226 West 4<sup>th</sup> Street, Davenport, Iowa.

### ***Work Plan***

Please describe the work being performed. Please note that only work described in the application may be approved by the Board.

Add additional pages in needed.



Hi Matt,

Regarding your questions:

1. Is the new metal door and exterior wall construction only taking place on the rear of the building? (Shown as Door 5 in your photo)  
YES. ONLY ON THE REAR OF THE BUILDING.
2. Aside from new soffit and fascia, are there any alterations to the front of the home facing 6<sup>th</sup> Street?  
THE CEILING ON EACH FRONT PORCH WILL BE CAPPED IN ALUMINUM.
3. What siding material is being used? Aluminum, wood, vinyl, composite, etc.?  
VINYL.
4. Is the siding limited to the location where the exterior wall is being repaired or is the entire home receiving new siding?  
LIMITED TO THE LOCATION WHERE THE EXTERIOR WALL IS BEING REPAIRED.
5. I am unable to locate the spec for the metal door. The metal door should look residential, like the image below. Please confirm the design.  
I HAVE ATTACHED A PIC OF THE CURRENT AND PROPOSED DOORS. SINCE THE CURRENT DOOR HAS A WINDOW, WE WANTED THE NEW DOOR TO HAVE A WINDOW, TOO.

Hamburg Historic District (amended)

Name of Property

Scott County, Iowa

County and State

Address State #, Map #, FS # 2016 district status 1983 district status	Historic Name	significant date/s architectural style/type	Materials of walls, foundations, roof
	History of property (owners/residents, census data including birthplace of self (and parents), Sanborn map info), Current use		Architectural data (architect/builder, features, modifications), Garage data
<b>521 W 6th St</b>  State #82-00548  Map #030 Field Site #A-21 Parcel #G0053-09  Updated district status: 1 contributing building (A), 1 non-contributing building  1983 NRHP status: Contributing	<b>Dr. Gustav Hoepfner House and Office</b>	c.1874 Gable-front with side ell (1 story)	Walls: frame - asbestos Foundation: stone (stucco) Roof: gable-front with side gable - asphalt shingle
	1874-1884 - Dr. Gustav Hoepfner - physician and office - continued to own through 1892 - used as rental 1880 census: Hoepfner, Gus (43, physician) - born in Germany - Holstein (Germany - Holstein, Germany - Holstein) 1892-1896 - Otto Albrecht, Jr and wife Clara; 1896-1906 - owned by Clara Albrecht Klenze - used as rental 1910 Sanborn map: house - 1 story - extant 1910 census: Sass, E.W. (29, commercial traveler - ironware) - born in US - IA (Germany, Germany); wife: Emilie (25) - born in US - IA (US - IA, US - IA) 1906-1920 - E.W. and Emilie Sass (commercial traveler); 1920-1929 - owned by Katherina Wunschel - used as rental - 3 families here in 1925; 1929-1940 - owned by various banks/organizations - used as rental; 1940-1944 - owned by Carl A. Buenga 1944-1949 - owned by Kosta Alexoff - four families here in 1945; 1949-1960s - owned by Rosalie and John H. Pruett - noted with 4 apts in 1956 1956 Sanborn map: apartments (4 units) - 1 story - extant Current use: apartments (4 units)		Architect/builder: - Porch: entry porch - round wood column, later wood steps Windows: 6/6 wood window on rear gable, other 1/1 vinyl windows Architectural details: exposed basement on rear Modifications: Historic: -; Non-historic: 1957 - asbestos siding installed Garage: detached - one car Date: c.1951 Walls: concrete block Foundation: concrete Roof: gable-front - asphalt shingles Notes: three-light wood window in gable Other site features: modern block retaining wall along rear of lot (4 ft)
<b>529 Western Ave</b>  State #82-02707  Map #031 Field Site #A-22 Parcel #G0053-08  Updated district status: 1 contributing building (A)  1983 NRHP status: Contributing	<b>Charles and Charlotte French House</b>	c.1860 cross gable (L-plan) (2 story)	Walls: frame - wood Foundation: stone Roof: cross gable - asphalt shingles
	c.1856 - Charles and Charlotte French moved to Davenport from Andover, MA; listed here by 1861-1864 (bought in 1862) - French's express; 1864 - Charlotte died; Charles remarried - Ruth; 1870s-1890 - Charles and Ruth French (French's Express) 1880 census: French, Charles (65, express man) - born in US - MA (US - MA, US - MA); wife: Ruth (61) - born in US - NY (US - NY, US - NY) 1891-1925 - Tomson and Adeline French (French's Express Line), also two other families/lodgers here in 1910; 1926-1934 - various owners, ending in Blue Grass Savings Bank 1910 Sanborn map: house - 2 story - extant 1910 census: French, Tomson (48, transfer company) - born in US - MA (US - MA, US - MA); wife: Adeline (48) - born in US - IL (US - IN, US - IN) 1934-1941 - owned by Blue Grass Savings Bank - used as rental 1941-1956 - Theodore P. and Ada M. Steckel (1940s - constable) 1956 Sanborn map: house - 2 story - extant 2000s - became church - Community of Christ - City Center Ministry Current use: church		Architect/builder: - Porch: porch on south - round columns Windows: 1/1 wood winodws - wood surrounds Architectural details: house faces west/south, corner tower at SW junction of L-plan with porch, simple gable-roof form Modifications: Historic: west part is addition; Non-historic: - Garage: none Other site features: stone retaining wall in south/rear yard (2 ft)



## **Historic Preservation Ordinance**

### **Section 14.01.060 Certificate of Appropriateness Review Process**

**C. Commission review process** - Standards for review. In considering an application for a certificate of appropriateness, the commission shall be guided by the following general standards in addition to any other standards or guidelines established by ordinance for a local landmark or historic district. In all cases, these standards are to be applied in a reasonable manner, taking into full consideration the issue of economic feasibility and other technical considerations.

1. Every reasonable effort shall be made to make the minimal number of changes necessary to maintain a designated property in a good state of repair, thereby minimizing the impact of the proposed alteration; and
2. The removal, alteration or concealing of distinguishing exterior architectural features and historic material of a designated property should be avoided when possible; and
3. All designated property shall be recognized as a product and physical record of its time, place and use. Changes that create a false sense of historical development, such as adding conjectural architectural features shall be discouraged; and
4. Most properties change over time, and those changes that have acquired architectural and/or historical significance in their own right shall be recognized, respected and retained; and
5. Distinctive architectural features, construction techniques and/or examples of craftsmanship that characterize a designated property shall be treated with due consideration; and
6. Deteriorated architectural features should, where possible, be repaired rather than replaced. Where the severity of deterioration requires replacement, the new feature shall match the old in design, color, texture and other visual qualities and, where possible, materials. Replacement of missing features shall be substantiated by documentary, physical or pictorial evidence; and
7. Activities that cause deterioration of a designated property and its architectural features shall be discouraged. In those cases where the damage would be irreversible, such as sand-blasting and wet blasting fire-hardened bricks, the activities shall be prohibited. If cleaning is to be done, the gentlest means possible shall be encouraged; and
8. Known significant archeological resources possibly affected by a proposed activity shall be protected and preserved. If such resources must be disturbed, mitigation measures shall be undertaken; and
9. New additions and related new construction shall not be discouraged when such improvements do not destroy historic material and such design is compatible with the size, massing, scale, color, materials and character of the property, neighborhood and district, if applicable.

**D. Design criteria to implement review standards.** When the commission is considering an application for a certificate of appropriateness, it shall consider the following architectural design criteria, or elements of design as they relate to the standards for review prescribed in Section 14.01.040C.

1. Height. The height of any proposed addition, construction or reconstruction should be compatible with the designated property and the surrounding structures, if located within a designated historic district; and
2. Proportions. The proportions (width versus height relationship) between doors and windows should be compatible, if not replicated, with the architectural design and character of the designated property; and
3. Scale. A proposed alteration, construction, reconstruction or addition should not negatively impact the scale of the designated property or district; and
4. Materials. Historic or original architectural features, or replacement elements which in all ways replicated the original, should be repaired whenever possible; and
5. Relationship of building masses and spaces. The relationship of a structure within a designated historic district to the rear, side and front yards between it and surrounding structures should be compatible; and
6. Roof shape. The roof design and shape should remain consistent with its original configuration and character; and
7. Site improvements. Landscaping and other site improvements, including off-street parking, should have as minimal of an impact as possible to the designated property's original plan/layout and its visual character.



City of Davenport  
Historic Preservation Commission

Department: Development & Neighborhood Services  
Contact Info: Matt Werderitch 563.888.2221

**Date**  
**1/10/2023**

**Subject:**

Case COA23-04: Request to install new garage doors at 625 West 7th Street. The Hans Goos House is located within the Hamburg Local Landmark Historic District. Brett Lutz, petitioner. [Ward 3]

**Recommendation:**

A recommendation is made to approve the Certificate of Appropriateness to install new garage doors at 625 West 7th Street in accordance with the submitted material.

The project was reviewed for conformance with the Standards for Review, Chapter 14.01.060C of the Davenport Municipal Code. The project meets the following standards:

1. New additions and related new construction shall not be discouraged when such improvements do not destroy historic material and such design is compatible with the size, massing, scale, color, materials and character of the property, neighborhood and district, if applicable.

**Background:**

The Hans Goos House is listed as a contributing structure in the Hamburg Historic District. The simple four room structure was built in 1872. The architectural style, if viewed from West 7th Street, resembles a shotgun or New Orleans French Quarter cottage. The residence exemplifies the natural charm of smaller wood frame homes scattered throughout the Gold Coast.

The property has a 400 square foot garage, which takes access from the brick alley south of the home. Since the two stall garage was built in 1992, the building does not contribute to the historic character of the neighborhood.

**Request:**

The applicant is proposing to remove the two existing aluminum and fiberglass electrically operated roll up doors. The intention is to replace with two new manually operated wood garage doors. As opposed to electrically operated roll-up doors, the replacement wood doors will swing outward.

The new doors will incorporate materials, hardware, and designs that better match the original construction of the historic home. The wood doors will be painted in beige/gray/white to match the exterior of the garage and home and feature hinges and hardware that appear antique.

**ATTACHMENTS:**

Type	Description
▣ Backup Material	Application
▣ Backup Material	Background Materials & Photos
▣ Backup Material	Hans Goos Residence Brochure

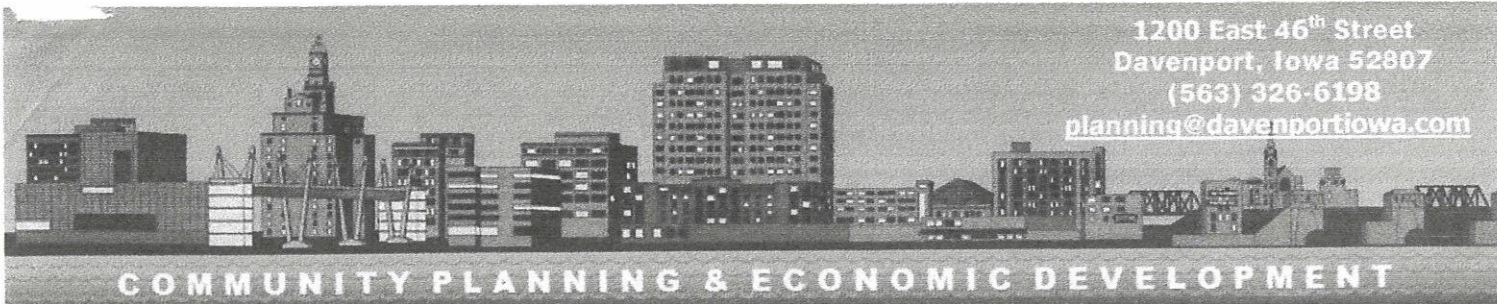
▣ Backup Material

## Historic Preservation Ordinance-Review Standards

### REVIEWERS:

Department	Reviewer	Action	Date
Community Planning & Economic Development	Werderitch, Matt	Approved	1/6/2023 - 10:26 AM





1200 East 46<sup>th</sup> Street  
Davenport, Iowa 52807  
(563) 326-6198  
[planning@davenportiowa.com](mailto:planning@davenportiowa.com)

Complete application can be emailed to [planning@davenportiowa.com](mailto:planning@davenportiowa.com)

Property Address\* 625 W 7th St

\*If no property address, please submit a legal description of the property.

12/25/22

**Applicant (Primary Contact)**

Name: Brett Lutz  
Company:   
Address: 625 West 7th Street  
City/State/Zip: Davenport, IA 52803  
Phone: 515-860 4469  
Email: brett.lutz1@gmail.com

**Owner (if different from Applicant)**

Name: Same  
Company:   
Address:   
City/State/Zip:   
Phone:   
Email:

**Engineer (if applicable)**

Name:   
Company:   
Address:   
City/State/Zip:   
Phone:   
Email:

**Architect (if applicable)**

Name:   
Company:   
Address:   
City/State/Zip:   
Phone:   
Email:

**Attorney (if applicable)**

Name:   
Company:   
Address:   
City/State/Zip:   
Phone:   
Email:

**Application Form Type:**

**Plan and Zoning Commission**

- Zoning Map Amendment (Rezoning) ☐  
Planned Unit Development ☐  
Zoning Ordinance Text Amendment ☐  
Right-of-way or Easement Vacation ☐  
Voluntary Annexation ☐

**Zoning Board of Adjustment**

- Zoning Appeal ☐  
Special Use ☐  
Hardship Variance ☐

**Design Review Board**

- Design Approval ☐  
Demolition Request in the Downtown ☐  
Demolition Request in the Village of East Davenport ☐

**Historic Preservation Commission**

- Certificate of Appropriateness ☒  
Local Landmark Nomination ☐  
Demolition Request ☐

**Administrative**

- Administrative Exception ☐  
Health Services and Congregate Living Permit ☐

### Historic Resource:

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Applicant: Brett Lutz Date: 12/25/22

By typing your name, you acknowledge and agree to the aforementioned submittal requirements and formal procedure and that you must be present at scheduled meetings.

Received by:  Date:   
Planning staff

Date of the Public Meeting: 

Meetings are held in City Hall Council Chambers located at 226 West 4<sup>th</sup> Street, Davenport, Iowa.

## Work Plan

Please describe the work being performed. Please note that only work described in the application may be approved by the Board.

1. Creation of barn-door like garage doors that swing out on hinges, manually operated.
2. Removal of existing aluminum and fiberglass electrically operated roll up doors.
3. Garage was built onsite in 1992, and only has minor "historic look or appearance" to it with the exterior side walls. I would like to not only make it "look" more historic for the 1870s home, but have garage doors operate manually, without electricity, as they didn't have electricity in the 1870s.
4. I will do the construction of the new doors and removal of existing doors myself.
5. Jon Shinbori of the building department already said such a project didn't need a building permit.
6. I would paint the new doors to match the existing garage, which is beige/gray/white.
7. There are some other garages in the Hamburg neighborhood that have old garages with these types of doors (swingout barn or folding carriage doors) some of which are very old and dilapidated, some are new and only appear historic, but operate electrically and are made out of metal and fiberglass. I want my doors to be closer to what they were like in the 1870s.
8. I would use regular lumber, plywood, and hardware found at any big box hardware store like

Add additional pages in needed.

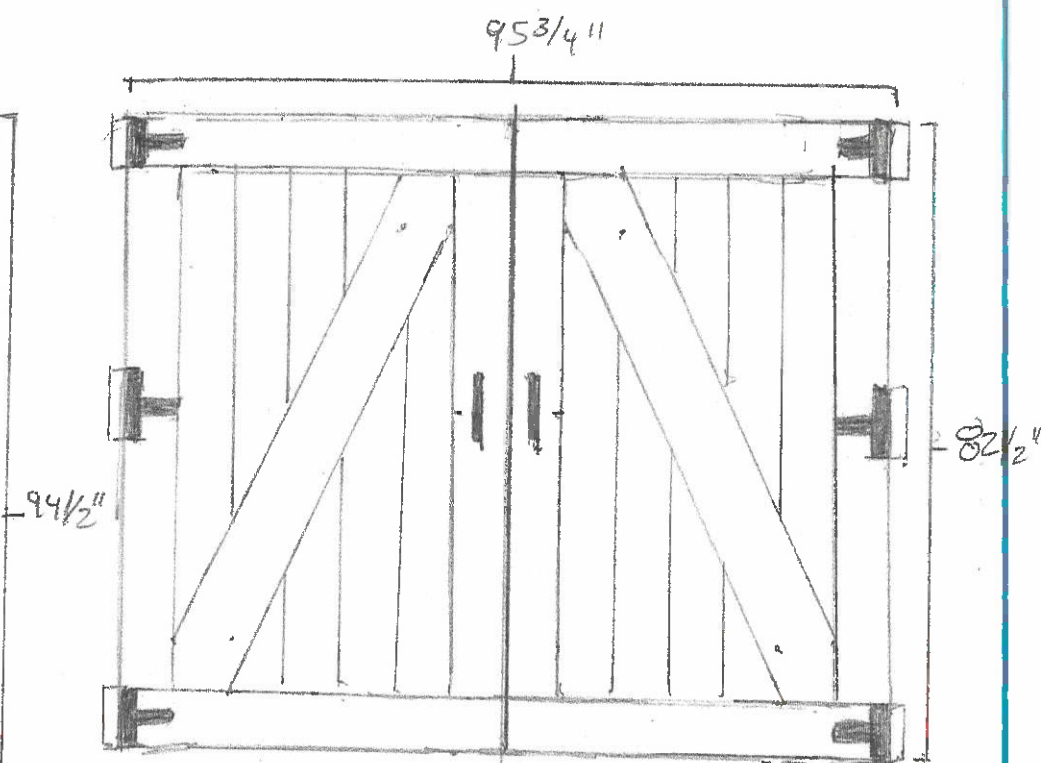
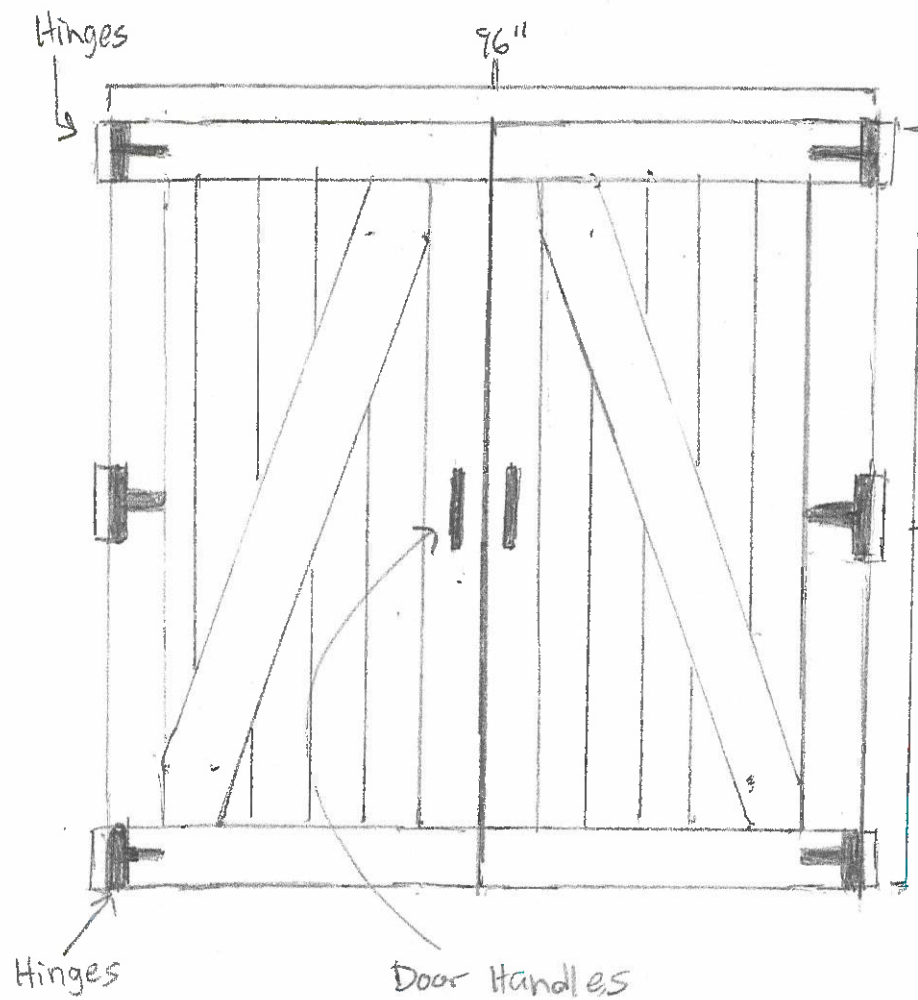
Menards or Home Depot.

please see next page →



9. I will use hinges from the hardware store that appear "historic" and "antique."
10. Though the garage was built in 1992 and some of its exterior appears "historic", none of it really is. My goal is to replace the garage doors to bring the garage closer to authentic historicity, or at least replicate it, to better conform the property as a whole to the goals of historic preservation or replication.
11. There are other modifications to the garage I would like to make in the future to reach those goals, But I thought I'd start with the doors. One step at a time.
12. Please see the attached photos of the garage and the attached building diagram of the garage doors
13. Please let me know if you need more information.

Thank You - Brett Lutz







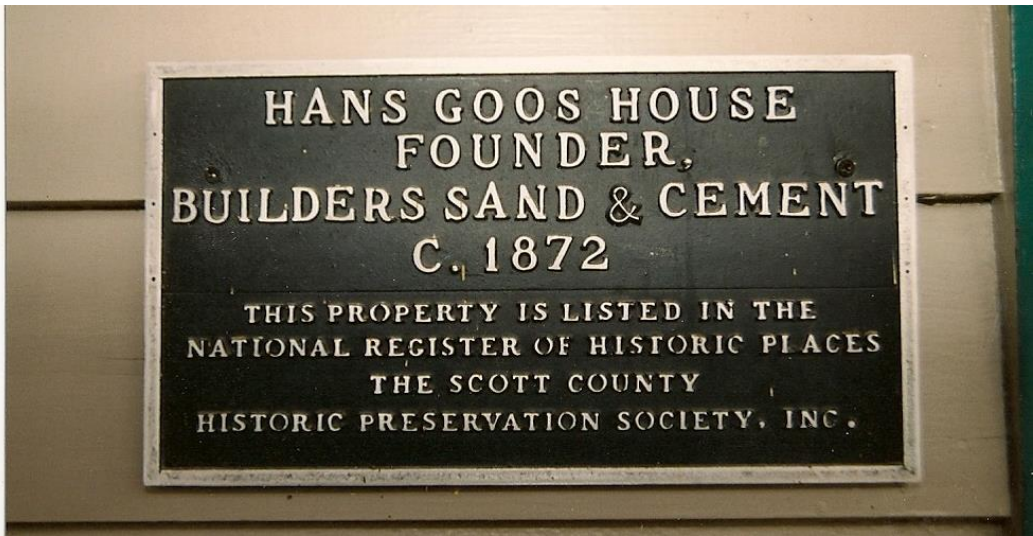














# Hans Goos House

DAVENPORT  
IOWA | USA

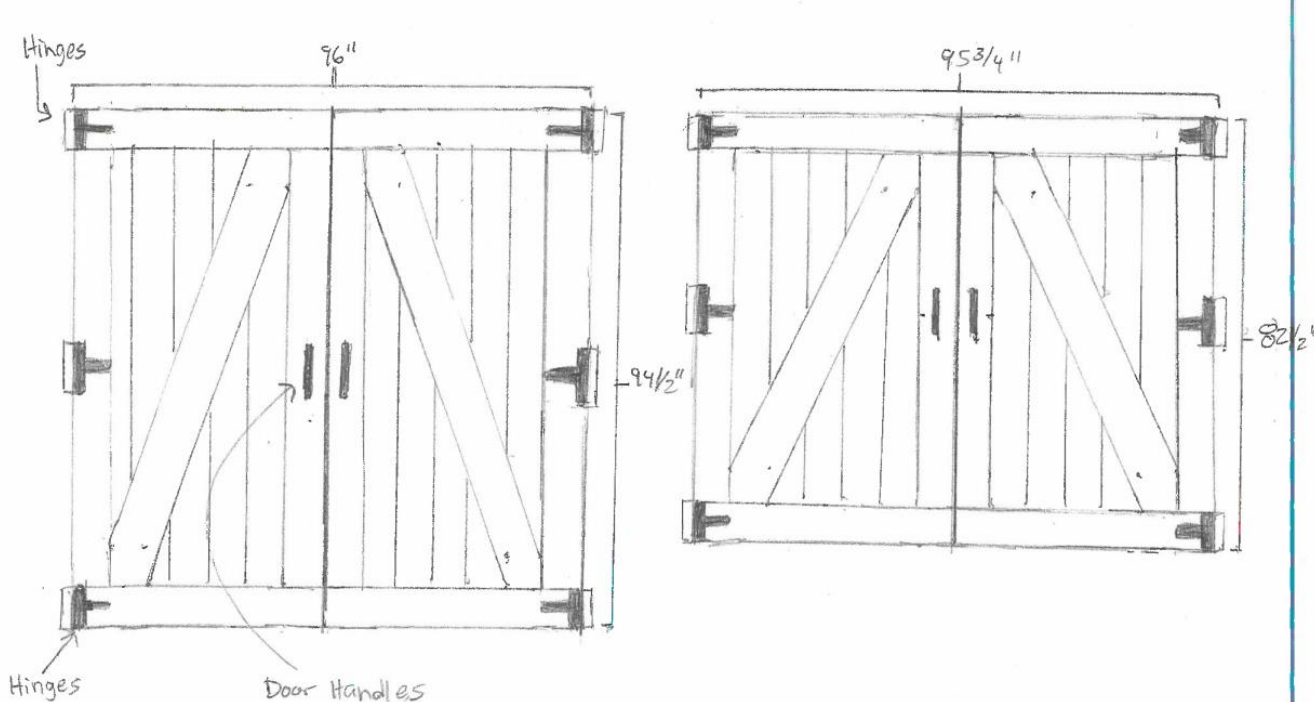




# New Garage Doors

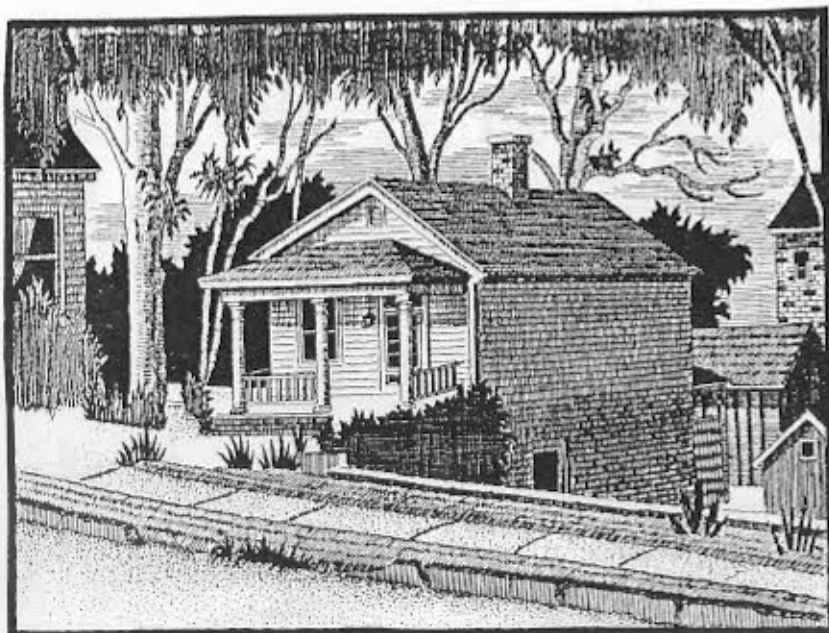
DAVENPORT  
IOWA | USA

- Install new manually operated wood doors.
- Doors to swing outwards
- Incorporate antique style hinges & hardware.
- Paint doors beige/gray/white to match garage exterior.



Existing Garage – Accessed From Alley  
Built in 1992





The Hans Goos Residence, ca 1872.  
625 West Seventh Street

This home, believed to have been built by Hans Goos, an immigrant from Schleswig-Holstein in Germany, was originally a simple four room structure containing two bedrooms, a kitchen and a parlor. The style of the house might best be compared to the shotgun or New Orleans French Quarter cottage if viewed from the south side. In 1921 a sunporch above and bathroom below were added on to the east side of the house.

Mr. Goos was an early Davenport entrepreneur, dealing in cordwood and sand. In 1862 he founded the Builders Lime & Cement Company which in 1891 was passed on to his only son, Hans Goos Jr. The Builders Company still exists today in Davenport. Mr. Goos Sr. was also the owner of the steamer, North Star, which was a local regular on the Mississippi. Both the elder Hans Goos and his son resided and passed away in this house.

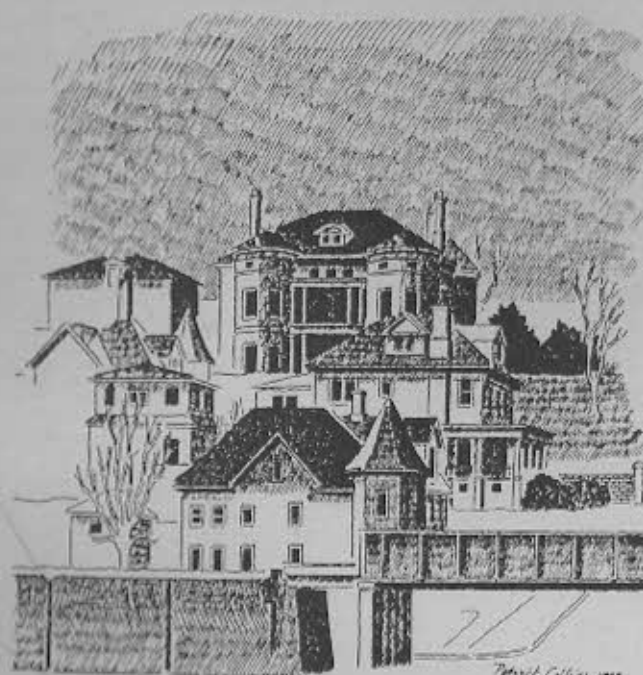
Restoration of the home back to its 1870s simplicity (with the exception of the necessity of the 1921 bathroom addition) began in April of 1992. Like many properties the house had fallen into a state of extreme disrepair and was completely sided with asbestos siding. The front porch has been restored and one original outbuilding remains. The Hans Goos Sr. and Jr. residence is a prime example of the natural charm and grace of many of the smaller woodframe and brick structures found throughout the Gold Coast.

625 W. 7th



## THE GOLD COAST THIRD ANNUAL OPEN HOUSE & WALKING TOUR

"In Commemoration of Davenport's Founders"  
12 September 1993



View Looking North From Ripley and West 4th Sts, Davenport, Iowa

625 W. 7th

## **Historic Preservation Ordinance**

### **Section 14.01.060 Certificate of Appropriateness Review Process**

**C. Commission review process** - Standards for review. In considering an application for a certificate of appropriateness, the commission shall be guided by the following general standards in addition to any other standards or guidelines established by ordinance for a local landmark or historic district. In all cases, these standards are to be applied in a reasonable manner, taking into full consideration the issue of economic feasibility and other technical considerations.

1. Every reasonable effort shall be made to make the minimal number of changes necessary to maintain a designated property in a good state of repair, thereby minimizing the impact of the proposed alteration; and
2. The removal, alteration or concealing of distinguishing exterior architectural features and historic material of a designated property should be avoided when possible; and
3. All designated property shall be recognized as a product and physical record of its time, place and use. Changes that create a false sense of historical development, such as adding conjectural architectural features shall be discouraged; and
4. Most properties change over time, and those changes that have acquired architectural and/or historical significance in their own right shall be recognized, respected and retained; and
5. Distinctive architectural features, construction techniques and/or examples of craftsmanship that characterize a designated property shall be treated with due consideration; and
6. Deteriorated architectural features should, where possible, be repaired rather than replaced. Where the severity of deterioration requires replacement, the new feature shall match the old in design, color, texture and other visual qualities and, where possible, materials. Replacement of missing features shall be substantiated by documentary, physical or pictorial evidence; and
7. Activities that cause deterioration of a designated property and its architectural features shall be discouraged. In those cases where the damage would be irreversible, such as sand-blasting and wet blasting fire-hardened bricks, the activities shall be prohibited. If cleaning is to be done, the gentlest means possible shall be encouraged; and
8. Known significant archeological resources possibly affected by a proposed activity shall be protected and preserved. If such resources must be disturbed, mitigation measures shall be undertaken; and
9. New additions and related new construction shall not be discouraged when such improvements do not destroy historic material and such design is compatible with the size, massing, scale, color, materials and character of the property, neighborhood and district, if applicable.



**D. Design criteria to implement review standards.** When the commission is considering an application for a certificate of appropriateness, it shall consider the following architectural design criteria, or elements of design as they relate to the standards for review prescribed in Section 14.01.040C.

1. Height. The height of any proposed addition, construction or reconstruction should be compatible with the designated property and the surrounding structures, if located within a designated historic district; and
2. Proportions. The proportions (width versus height relationship) between doors and windows should be compatible, if not replicated, with the architectural design and character of the designated property; and
3. Scale. A proposed alteration, construction, reconstruction or addition should not negatively impact the scale of the designated property or district; and
4. Materials. Historic or original architectural features, or replacement elements which in all ways replicated the original, should be repaired whenever possible; and
5. Relationship of building masses and spaces. The relationship of a structure within a designated historic district to the rear, side and front yards between it and surrounding structures should be compatible; and
6. Roof shape. The roof design and shape should remain consistent with its original configuration and character; and
7. Site improvements. Landscaping and other site improvements, including off-street parking, should have as minimal of an impact as possible to the designated property's original plan/layout and its visual character.

City of Davenport  
Historic Preservation Commission

Department: Development & Neighborhood Services  
Contact Info: Matt Werderitch 563.888.2221

**Date**  
**1/10/2023**

**Subject:**

Case NRHP23-01: Review and provide a recommendation for the International Harvester Truck Sales and Service Station at 601 West 2nd Street to be listed on the National Register of Historic Places. [Ward 3]

**Recommendation:**

Staff recommend the International Harvester Truck Sales and Service Station at 601 West 2nd Street be listed on the National Register of Historic Places. The Commission shall authorize the Chairperson to sign the Certified Local Government National Register Nomination Evaluation Report Form.

The nomination achieves consistency with Criterion A (Historical Events) and Criterion C (Architecture).

1. The International Harvester Truck Sales and Service Station is associated with events that have made a significant contribution to the broad patterns of our history. (Criterion A)
2. The International Harvester Truck Sales and Service Station embodies the distinctive characteristics of a type, period, or method of construction, or that represent the work of a master, or that possess high artistic values, or that represent a significant and distinguishable entity whose components may lack individual distinction. (Criterion C)

-

**Background:**

As a participant in the Certified Local Government Program, the Historic Preservation Commission is required to review and comment on proposed National Register nominations of properties within its jurisdiction.

The question for the Historic Preservation Commission to answer when reviewing the nomination is whether the nominated property meets the National Register of Historic Places criteria for significance and integrity.

**Criteria for Evaluation:**

1. **Criteria A (Historical Events):** That are associated with events that have made a significant contribution to the broad patterns of our history; or
2. **Criteria B (Important Person):** That are associated with the lives of persons significant in our past; or
3. **Criteria C (Architecture):** That embody the distinctive characteristics of a type, period, or method of construction, or that represent the work of a master, or that possess high artistic values, or that represent a significant and distinguishable entity whose components may lack individual distinction; or
4. **Criteria D (Archaeological):** That have yielded, or may be likely to yield, information important in prehistory or history.



**Nominated Property: The International Harvester Truck Sales and Service Station**

"The International Harvester Truck Sales and Service Station is located in downtown Davenport, Iowa, near the Mississippi River. Placed in service in 1928 as a vehicular retail and service center and occupying about one quarter of a city block, this one-story building features steel columns and brick walls for support, large storefront windows, and Romanesque and Gothic Revival ornament showing the influence of the Commercial Style of architecture on its design. The building's roof employs innovative steel bowstring trusses to clear span a vast interior space for unimpeded vehicular operations. The low height and placement of a 1979-built one story addition at the rear of the building does not negatively impact the property's historic integrity because the addition's visual impact is minimal. Overall, the International Harvester Truck Sales and Service Station possesses good historic integrity." -National Register of Historic Places Registration Form.

Please see the attached documentation for background information.

**ATTACHMENTS:**

Type	Description
▢ Backup Material	Certified Local Government Notification Letter
▢ Backup Material	CLG Review Form
▢ Backup Material	NRHP Registration Form
▢ Backup Material	NRHP Registration Form Photos
▢ Backup Material	Letter to Mayor Matson
▢ Backup Material	State National Register Nomination Review Committee Agenda

**REVIEWERS:**

Department	Reviewer	Action	Date
Community Planning & Economic Development	Werderitch, Matt	Approved	1/6/2023 - 11:53 AM

December 12, 2022

Matt Werderitch, Bob McGivern  
Davenport Historic Preservation Commission  
[matt.werderitch@davenportiowa.com](mailto:matt.werderitch@davenportiowa.com); [bob@mcgivernappraisal.com](mailto:bob@mcgivernappraisal.com)  
Via Email

RE: **International Harvester Truck Sales and Service Station**  
**601 West Second Street, Davenport**  
[https://drive.google.com/drive/folders/1YztKSs-OcL0pmSMqqV4ZPYbN12QSY2OP?usp=share\\_link](https://drive.google.com/drive/folders/1YztKSs-OcL0pmSMqqV4ZPYbN12QSY2OP?usp=share_link)

Dear Friends:

The State Nominations Review Committee (SNRC) plans to consider the property referenced above for nomination to the National Register of Historic Places during their February 10, 2023 meeting. To view or download the associated nomination and photographs, click on the link listed above.

As a participant in the Certified Local Government Program, the Historic Preservation Commission is required to review and comment on proposed National Register nominations of properties within its jurisdiction. The State is required to provide you with a 60-day period for the review, unless we mutually agree to expedite the process. I am contacting you to ask that you initiate the review process for the Historic Preservation Commission. As an attachment to this notification email is a copy of the review form.

The review process will require the following:

- ◆ The Historic Preservation Commission should review the nomination during one of their meetings. In advance of the meeting, please send a formal invitation to your chief local elected official (the mayor or chair of the Board of Supervisors) with a copy of the nomination. You also need to send a formal invitation to the property owner/owners. If they are not familiar with the National Register of Historic Places, information about listing and the benefits of nomination is available at <http://www.nps.gov/nr/> (see especially the Frequently Asked Questions and Owners sections): You also need to make available copies of the nomination for public review before the meeting. For example, leave a review copy at the courthouse and public library. Please indicate in your public meeting announcement that a review copy of the nomination is available and where the review copies can be found.
- ◆ The question for the Historic Preservation Commission to answer when reviewing the nomination is whether the nominated property meets the National Register of Historic Places criteria for significance and integrity (see Bulletin 15, How to Apply the National Register Criteria for Evaluation). If the Commission feels that the property and the nomination meets the criteria, the Commission should check the box recommending that the property be listed. If the Commission feels that the property does not meet the criteria, then check the box recommending that the property not be listed. Your chief local elected official should use the same approach when reviewing the nomination.



- ◆ You might want to invite the individual who prepared the nomination to attend the public meeting and present the nomination. Please keep a record of the meeting (copy of the public notice, agenda, minutes, list of attendees). At the conclusion of the meeting, the Commission should make a motion regarding their recommendation. The Chairman of the Commission will complete Item #1, the Commission's portion of the review form. Be sure to fill in the date of the public meeting, sign the signature line and record any comments made by commission members during the meeting. If your chief local elected official attended the public meeting, inquire if he/she is prepared to sign the review form.
- ◆ In the event that your chief local elected official was unable to attend the meeting, the Commission Chairman should forward the review form to him/her for review and comment. Have your chief local elected official sign the form and return it to the Historic Preservation Commission.
- ◆ Item #3 on the Review form asks for the review and signature of a preservation professional. If your commission does not have a professionally qualified historian or architectural historian who can complete this part of the form, you may leave Item #3 blank and I will arrange to have a SHPO staff member complete that part of the form.
- ◆ After you have completed Items #1 through #2 (through #3 if a preservation professional is available), please make a copy of the completed review forms for your file and send the original copies of the completed forms to me.
- ◆ The Commission should keep the nomination and photographs. File them in your inventory, as you will need the information for future reference.

If the Historic Preservation Commission and your chief local elected official disagree with one another on the property's National Register eligibility, both views will be presented to the SNRC for their consideration during review of the nomination. If both the Historic Preservation Commission (by Commission majority) and the Mayor do not consider the property eligible for National Register listing, we must halt the nomination. Be advised that when a nomination is halted, the property owner, the person who prepared the nomination or any interested party may appeal the decision. In addition, the nomination will still go forward to the National Park Service for an official "Determination of Eligibility."

Please contact Allison Archambo at [allison.archambo@iowa.gov](mailto:allison.archambo@iowa.gov) with any questions or concerns regarding the CLG program or the process for this review. You can find the answers to frequently asked questions on our website, such as the meaning of being listed on the National Register of Historic Places or the National Register process <https://iowaculture.gov/history/preservation/national-register-historic-places>.

Questions regarding the nomination and digital copies of completed CLG review forms should be directed to [nationalregister@iowa.gov](mailto:nationalregister@iowa.gov).

Sincerely,

**National Register of Historic Places**  
**State Historic Preservation Office of Iowa**  
[nationalregister@iowa.gov](mailto:nationalregister@iowa.gov) | 515.281.5111 | [iowaculture.gov](http://iowaculture.gov)

# CERTIFIED LOCAL GOVERNMENT NATIONAL REGISTER NOMINATION EVALUATION REPORT FORM

As a participant in the Certified Local Government Program (CLG), the Historic Preservation Commission is required to review and comment on proposed National Register nominations of properties within its jurisdiction. The State is required to provide the CLG with a 60-day period for the review prior to a State Nominations Review Committee (SNRC) meeting. This form must be received by the State Historic Preservation Office (SHPO) five days in advance of the State Nomination Review Committee (SNRC) meeting.

(Please print clearly)

**Historic Property Name:** \_\_\_\_\_

**Address:** \_\_\_\_\_

**Certified Local Government Name:** \_\_\_\_\_

**Date of public meeting for nomination review:** \_\_\_\_\_

**Applicable Criteria:** (Please Check the Appropriate Box)

☐ Criterion A (Historical Events)

☐ Criterion B (Important Person)

☐ Criterion C (Architecture)

☐ Criterion D (Archaeological)

**Please check the following box that is appropriate to the nomination** (Please print clearly).

☐ The Commission recommends that the property should be listed on the National Register of Historic Places.

☐ The Commission recommends that the property should not be listed in the National Register for the following reasons: \_\_\_\_\_

☐ The Commission chooses not to make a recommendation on this nomination for the following reasons: \_\_\_\_\_

☐ The Commission would like to make the following recommendations regarding the nomination: (use additional sheets if necessary) : \_\_\_\_\_

## Official Signatures Required Below

### Historic Review Board Chair or Representative

Print Name: \_\_\_\_\_

Approved ☐ Not Approved ☐

Signature: \_\_\_\_\_

### Chief Elected Official

Print Name: \_\_\_\_\_

Approved ☐ Not Approved ☐

Signature: \_\_\_\_\_

### Professional Evaluation

Print Name: \_\_\_\_\_

Approved ☐ Not Approved ☐

Signature: \_\_\_\_\_



United States Department of the Interior  
National Park Service

# National Register of Historic Places Registration Form

This form is for use in nominating or requesting determinations for individual properties and districts. See instructions in National Register Bulletin, *How to Complete the National Register of Historic Places Registration Form*. If any item does not apply to the property being documented, enter "N/A" for "not applicable." For functions, architectural classification, materials, and areas of significance, enter only categories and subcategories from the instructions. **Place additional certification comments, entries, and narrative items on continuation sheets if needed (NPS Form 10-900a).**

## 1. Name of Property

historic name International Harvester Truck Sales and Service Station

other names/site number \_\_\_\_\_

Name of Multiple Property Listing n/a

(Enter "N/A" if property is not part of a multiple property listing)

## 2. Location

street & number 601 West Second Street

☐

not for publication

city or town Davenport

☐

vicinity

state Iowa

county Scott

zip code 52801

## 3. State/Federal Agency Certification

As the designated authority under the National Historic Preservation Act, as amended,

I hereby certify that this X nomination     request for determination of eligibility meets the documentation standards for registering properties in the National Register of Historic Places and meets the procedural and professional requirements set forth in 36 CFR Part 60.

In my opinion, the property X meets     does not meet the National Register Criteria. I recommend that this property be considered significant at the following level(s) of significance:     national     statewide X local

Applicable National Register Criteria: X A     B X C     D

Signature of certifying official/Title: Deputy State Historic Preservation Officer

Date

State Historical Society of Iowa

State or Federal agency/bureau or Tribal Government

In my opinion, the property     meets     does not meet the National Register criteria.

Signature of commenting official

Date

Title

State or Federal agency/bureau or Tribal Government

## 4. National Park Service Certification

I hereby certify that this property is:

    entered in the National Register

    determined eligible for the National Register

    determined not eligible for the National Register

    removed from the National Register

    other (explain:)

Signature of the Keeper

Date of Action

International Harvester Truck Sales and  
Service Station

Scott County, Iowa

Name of Property

County and State

## 5. Classification

### Ownership of Property

(Check as many boxes as apply.)

- |                                     |                  |
|-------------------------------------|------------------|
| <input checked="" type="checkbox"/> | private          |
| <input type="checkbox"/>            | public - Local   |
| <input type="checkbox"/>            | public - State   |
| <input type="checkbox"/>            | public - Federal |

### Category of Property

(Check only **one** box.)

- |                                     |             |
|-------------------------------------|-------------|
| <input checked="" type="checkbox"/> | building(s) |
| <input type="checkbox"/>            | district    |
| <input type="checkbox"/>            | site        |
| <input type="checkbox"/>            | structure   |
| <input type="checkbox"/>            | object      |

### Number of Resources within Property

(Do not include previously listed resources in the count.)

Contributing	Noncontributing	
1		Buildings
		Site
		Structure
		Object
1	0	<b>Total</b>

Number of contributing resources previously listed in the National Register: 0

## 6. Function or Use

### Historic Functions

(Enter categories from instructions.)

Commerce/Trade/specialty store

### Current Functions

(Enter categories from instructions.)

Vacant/Not in Use

## 7. Description

### Architectural Classification

(Enter categories from instructions.)

Late 19<sup>th</sup> & Early 20<sup>th</sup> Century American Movements/  
Commercial Style

### Materials

(Enter categories from instructions.)

foundation: Concrete

walls: Brick

roof: Synthetics

other:



International Harvester Sales and Service  
Station

Scott County, Iowa

Name of Property

County and State

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### Narrative Description

**Summary Paragraph** (Briefly describe the current, general characteristics of the property, such as its location, type, style, method of construction, setting, size, and significant features. Indicate whether the property has historic integrity.)

The International Harvester Truck Sales and Service Station is located in downtown Davenport, Iowa, near the Mississippi River. Placed in service in 1928 as a vehicular retail and service center and occupying about one quarter of a city block, this one-story building features steel columns and brick walls for support, large storefront windows, and Romanesque and Gothic Revival ornament showing the influence of the Commercial Style of architecture on its design. The building's roof employs innovative steel bowstring trusses to clear span a vast interior space for unimpeded vehicular operations. The low height and placement of a 1979-built one-story addition at the rear of the building does not negatively impact the property's historic integrity because the addition's visual impact is minimal. Overall, the International Harvester Truck Sales and Service Station possesses good historic integrity.

---

**Narrative Description** (Describe the historic and current physical appearance and condition of the property. Describe contributing and noncontributing resources if applicable.)

(**Iowa SHPO Additional Instructions:** After the main **Narrative Description**, discuss any physical alterations since the period of significance under the subheading **Alterations**, and the seven aspects of integrity as it applies to the resource in a **Statement of Integrity** with each aspect discussed in its own paragraph.)

### Setting

The International Harvester Truck Sales and Service Station is a building placed in service in 1928 with an addition constructed in 1979. The building is situated in an urban area about three blocks to the west of downtown Davenport, Iowa, about three blocks north of the Mississippi River and two blocks north of the Modern Woodmen Park baseball stadium. U.S. Highway 61 runs one-half block to the south of the sales and service station. That highway intersects North Gaines Street one block to the west. In 1940, this street became the entrance and exit to the Rock Island Centennial Bridge, a span over the Mississippi River linking Iowa with Illinois. (Figure 3) Retail businesses in Davenport, historically centered along West Second Street, and industry stayed nearer the riverfront to the east and west of the downtown core.

### Site

Original Town, Lots 5 & 6, Block 4, City of Davenport, Iowa, is the legal description of the International Harvester Truck Sales and Service Station site.

The site occupies the full northeast quarter of Block 4 on the southwest corner at the intersection of West Second Street and Western Avenue. The main block occupies about 63% of this site. The 1979 addition, located at the south end of its west side, occupies 15% of the site. The building is situated directly adjacent to the public right-of-way on the southwest corner of West 2nd Street and Western Avenue. On street parking is allowed on both the south side of West 2nd Street and the west side of Western Avenue.

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Concrete public pedestrian walks are situated between the adjacent streets and the building. Those portions of the walks nearest the streets are edged in brick pavers installed in the late 20th or early 21st century.

A series of curb cuts provides vehicular access to the property. One curb cut on West 2nd Street is situated near the west property line and provides access to a parking lot standing to the west of the building. Two curb cuts on Western Avenue provide access to overhead vehicular doors on the east elevation of the building. The drives from these curb cuts to the property flare outward by the street. An alley running east and west stands directly adjacent to the building's south elevation. Originally this alley was brick surfaced but is now extensively patched with concrete and asphalt. This alley provided access to a dock door on the south elevation of the 1979 addition.

The parking lot associated with this property was laid out in 1990. (Scott County Assessor's Office) It features an asphalt surface.

At the time International leased this site from the Merchants Transfer & Storage Company in 1927, three older buildings occupied the property. They included a building on the corner, a one-story barn, and an eight-family apartment building. (*Quad-City Times*, May 22, 1927) A multi-story commercial building also occupied the western portion of this site. It was razed to build the 1990 parking lot. (William V. Nielsen)

### Exterior of Main Block

Placed in service in 1928, the International Harvester Truck Sales and Service Station is a one-story, masonry edifice covered with a roof clad with rubber membrane. The building measures 100' x 150' with the narrower dimension facing West Second Street. This elevation also serves as its front facade.

The building has no basement but rests on a poured concrete slab. This slab is situated at grade, providing a level surface for vehicular entrance to the building. Where the topography of the site slopes to the south, the depth of this slab increases to keep the building's floor level. A cast stone water table surmounts the concrete slab.

Bearing walls employ brick and clay tile for all four elevations. Face brick is laid in a common bond with one course of header brick alternating with six courses of stretcher brick. The brick is polychrome in shades of green, yellow, blue, and red with a roughly textured surface and a slightly glazed finish. Mortar joints are raked. The north and east elevations of the building feature pink-colored mortar. The south and west elevations feature gray-colored mortar. This difference in mortar color changes the appearance of the building. Those elevations with the pink-colored mortar appear darker and denser than those elevations pointed with gray-colored mortar. (It should be noted that mortar deterioration is more evident on the walls laid in pink mortar than those laid in standard gray.) A coping of cast stone surmounts all four elevations of the building. When under construction, this 15,000 square foot edifice was advertised as fireproof because of its masonry construction. (*Quad-City Times*, May 22, 1927)

The north elevation of the building serves as one its two principal facades. This elevation features a symmetrical configuration of two bays with the building's main entrance centering them. Storefront windows occupy the bays. All of these original window openings remain intact, but the original windows were removed in the late 20th century and replaced with a row of glass lights in the center of each opening with greenish-blue



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metal panels set in metal frames occupying the lower third and upper third of each opening. The predominance of metal over glass in these replacement windows lends an opaque feeling to these storefronts, which originally possessed a transparent quality. (Figure 13)

The north entrance of the building is its most detailed architectural feature. Brick buttresses surmounted by cast stone dripstones and geometric detailing, flank this entrance. The door opening is edged with cast stone columns and a Romanesque arched lintel centering a keystone. Late 20th century double doors have replaced the original single wood door and sidelights as pictured in a 1964 photograph. (Figure 13) The brick wall above this entrance rises one tall and one short step higher than the rest of this elevation, giving a clear and distinct signal to the visitor where to enter the building. This design also nicely accents the symmetrical order of this elevation. The cast stone parapet, which surmounts the entire building, also caps this stepped entrance with a geometric keystone centering it.

The east elevation serves along with the north elevation as the building's other principal facade. The east elevation repeats the water table and matches the brick pattern, roofline, and parapet of the north elevation but differs in other aspects. The north section of the east elevation features one bay with six windows in like design with those on the north elevation. The south section of the east elevation features two vehicular doors, three large windows, and two passage doors. The window adjacent to the south of the large vehicular door was altered sometime during the mid-20th century to include one passage door. An historic photograph (Figure 5) pictures this window before the conversion. The conversion itself was carefully crafted, matching existing materials. The vehicular door on the north of the west elevation originally was a window, converted sometime during the 20th century into a door. This is evident by the opening's brickwork, which does not match the rest of the elevation. All of the windows feature cast stone sills. The water table on the east elevation steps down slightly in concert with the sloping grade of the site between the vehicular door on the south of the west elevation and the adjacent man door.

The south elevation of the building is clad with the same brick as its other elevations, but the mortar is gray in color instead of pink. As noted above, this renders the south elevation lighter in color than those pointed in pink. Because of a slope in the grade, the concrete slab on which the building rests is exposed on the south elevation and rises higher than on the water tables elsewhere. This gives a finished look at the southeast corner of the building where its water table meets the concrete slab. The south elevation shows noticeable mortar deterioration. There are six window openings in this elevation. A coal chute is situated near the east end of the south elevation. The middle window on this elevation is surrounded by infill brick of a different color than the rest of the elevation. Two windows are situated, one above the other, near its west end. Electrical conduit is attached to the south elevation to provide power to the building. At the top of the south elevation, its brick wall rises above the building's parapet to reflect the arched profile of the building's vaulted roof. A brick chimney is situated on the southeast corner of the building. The 1979 addition stands adjacent to the building in the same plane on its southwest corner.

The west elevation of the building is clad with the same brick as its other elevations, and the mortar is gray like the south elevation. As noted above, this renders the west elevation lighter in color than those pointed in pink. The west elevation is a solid brick wall. A steel door is centered in the original portion of the building. At one time, it served as a fire door to an adjacent, nonextant building to the west. The 1979 addition stands to the south of this door.

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A complex roof system covers the building. (Figure 4) A flat roof covers the north and west portions of the building with a series of four skylights situated on its west side. A barrel-vaulted roof covers the balance of the building, extending all the way to its east and south walls. This vault is supported internally by four steel bowstring arches. Four steel columns support the bowstring arches on the west. Four brick piers and the building's east wall support them on the east. Originally, a portion of this barrel vault included a large, 60 x 12 foot skylight. The base of this skylight remains extant, situated on a north-south axis centered on the building's footprint. The skylight's superstructure featured a nonextant front gable roof of glass and metal, removed circa 1979 because of water leakage. (William V. Nielsen) All the exterior surfaces of this building's roof are covered today with synthetic membrane, including the skylight base. This membrane also extends over the parapet of the building and down its west wall to the roof of the 1979 addition. A large HVAC chiller is situated near the southwest corner of this roof.

### Exterior of Addition

In 1979, Florist Supply, Inc., of Des Moines, Iowa, built a new warehouse for its business on the southwest corner of the International building at its rear. This is a 1-story, concrete block addition covered with a flat roof clad with synthetic membrane. The addition measures 60 x 60 feet.

The addition's exterior walls feature 8-inch concrete block now painted white. These walls were laid on the foundation of a previous building on this site. Mortar is gray in color where flaking paint has exposed it and is raked. The north elevation includes one vehicular entrance with an over-head door and one passage door. The vehicular door features 12-panels. The west elevation is a solid wall. The south elevation features one dock door with an over-head matching that on the north elevation. The east elevation abuts the main block. The 1979 addition is in good condition, having been well maintained over the years.

For convenience, this report sometimes will abbreviate the name of this building with the acronym IHC for International Harvester Corporation, its owner.

### Interior of Main Block

Although the interior of the main block has evolved somewhat over the years, most of its interior remains as originally designed with an open space unimpeded by structural obstructions. (Figure 6) Entrances to the 1979 addition are situated unobtrusively in its southwest corner.

A vestibule is situated inside the pedestrian entrance to the main block on West Second Street. The height of this vestibule has been raised and now partially blocks the transom window above this entrance. The vestibule leads into an open space. A large office flanks this space on the west. A smaller office and a large display room flank this space on the east. All of these spaces feature vinyl tile floors, concrete block partitions, and dropped acoustical tile ceilings. All of these spaces date from the late 1960s, when Eastern Iowa Community College occupied the building.

When originally built, the eastern section of these front rooms served as the IHC Sales Room. It measured 50 x 80 feet and faced West Second Street and Western Avenue. This room could display about ten trucks and was illuminated by thirty 200-watt lamps.



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The rest of the main block's main floor remains today, as in the past, a vast open space with a concrete floor. This space served as the IHC service and parts department. Walls are of brick now painted white. Originally, a series of wire partitions on the west side of this space separated the service department from the parts department. These partitions are nonextant. Another space, partitioned off on the south from the parts department, provided an area where vehicles could be washed and painted. (Figure 7) Although these walls are nonextant, a floor drain exists in front of the erstwhile entrance to this space. Contemporary advertisements called these spaces "Sales Room," "Receiving Room," "Waiting Room," "Store Room," and "Reconditioning Room." (*The Daily Times*, February 2, 1928)

In addition to the storefront windows on the north and east elevations, the main block includes three windows on the east elevation and six windows on the south elevation. All feature steel, industrial windows with single-glazing and wire safety glass.

Men and women's restrooms are situated on the east side of the service department. They date to the late 20th century. The rear of the service department originally included, at its southeast corner, a boiler room and a coal room. A coal chute remains extant in its south wall for delivery service from the adjacent alley. A jerry-built set of stairs, situated adjacent to the coal room, leads to a similarly constructed mezzanine built in the 1960s by Eastern Iowa Community College. The mezzanine includes a short hall with two small offices. Early in the 21st century, a drywall partition was constructed near the south end of the service department to convert this space into an electrical breaker-box room.

The barrel-vaulted roof, whose exterior was described above, is hidden by a dropped ceiling with acoustical tile, installed sometime during the mid-20th century. A series of metal rafters, supported by the bowstring arches and running perpendicular to them, supports the original plaster surface of the ceiling. The open space above this ceiling includes loose insulation and HVAC ductwork running between the bowstring arch framework.

### Interior of Addition

The interior of the 1979 addition contains only one large room occupying its entire 3,600 square foot space. The floor is of poured concrete with a natural finish. Two 6-inch metal poles, aligned to the east and west of each other and placed off-centered to the south of the room, support a steel I-beam, which in turn and in conjunction with the exterior walls, supports 13 metal trusses. These trusses run north and south to support a sheet metal ceiling and the roof.

A raised, concrete platform is situated in the southwest corner of the room. A metal railing edges its east side. A poured concrete ramp on the north end of the platform runs down to the main floor level facilitating deliveries. The platform and ramp together serve the dock door situated in the south wall. A mechanical door opener activates the dock door.

The addition's east wall includes two large openings connecting it to the main block. The opening near the south wall is smaller than the opening farther to the north. These openings are without doors. A smaller, passage door now infilled with brick is situated on the north end of the east wall. At one time, this door provided access to a nonextant 1-story building at the rear of 601 West Second Street. (Figure 12) This was likely a metal fire door similar to the extant fire door situated on the west elevation of the 601 property.

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The 1979 addition is illuminated by neon lights, original to the addition, and affixed to the metal trusses. A gas space heater is suspended from the ceiling near the east wall.

## STATEMENT OF INTEGRITY

The exterior and interior of the International Harvester Truck Sales and Service Station retains historic integrity in its seven aspects. Historic images of the building picture its exterior and interior at several points of time. A comparison of these with the present-day building reveals that the building's original design remains largely intact except for the replacement of storefront windows.

While the construction of the 1979 Addition is beyond the period of significance of International Harvester Truck Sales and Service Station, the Addition is situated on the foundation of an earlier commercial building occupying the exact same footprint. As such, the Addition lends the feeling of an urban site commensurate with its historic use. At the same time, the Addition's deep setback from the street and its lower roofline than that of the main block reduce its visual impact as a more recent building.

Because this resource remains on its original site, its level of integrity as related to *location* is excellent. As noted by an architectural historian:

Although little evidence remains today of Davenport's earliest large commercial and light industrial sectors, concentrations of early twentieth century manufacturing and commercial businesses are found in the east and west ends of the Downtown Davenport, including the International Harvester Truck Sales and Service Station, evaluated as individually National Register-eligible in 2005 within this historic context. (AKAY Consulting 2005: F-18)

The integrity of the building's exterior and interior *design* is fair. When the 1979 Addition to the building was constructed, its design used materials and massing sympathetic to the Main Block. Although the building's original storefront windows have been replaced, the window openings remain intact, and, as a multiple property documentation for downtown Davenport notes, this building "relies on other elements (i.e. entrance, construction material and overall form) to define its character." (AKAY Consulting 2004: 1). The steel bowstring trusses remain intact over a dropped ceiling, which can be removed without negatively impacting them.

The integrity of the building's *setting* is fair. Although situated on its original site, the building's surroundings adjacent to the west have changed over the years. There, several buildings from the late 19<sup>th</sup> or early 20<sup>th</sup> century have been razed and parking lots substituted in their place. On the other hand, the remaining 19<sup>th</sup> and 20<sup>th</sup> century improvements on this city block remain extant, lending the urban feeling to the property that has characterized its setting and historic context. (AKAY Consulting 2005: F-18)

The integrity of the building as it relates to *materials* is fair. Although the building's original storefront windows have been replaced, the building's exterior brick, cast stone detailing, commercial style windows, and roofing materials remain extant. The integrity of interior materials remains good with most original floors and interior brick walls intact. Although now covered by a dropped ceiling, original steel bowstring trusses and rafters supporting the main roof remain in excellent condition.



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The integrity of the building's exterior and interior *workmanship* is excellent, evident in the skilled erection of its exterior masonry walls and the construction of the main roof with its bowstring trusses, steel rafters, and plaster ceiling, having served the building trouble-free since 1928.

The building retains a good *feeling* of its historic function. Viewers today perceive the function of this building as a commercial building and, with its overhead doors at the rear, as a facility associated with vehicles.

The building retains excellent integrity as it relates to *association*. Visitors from the building's period of significance would readily recognize it today.

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## AERIAL MAP WITH PROPERTY BOUNDARIES

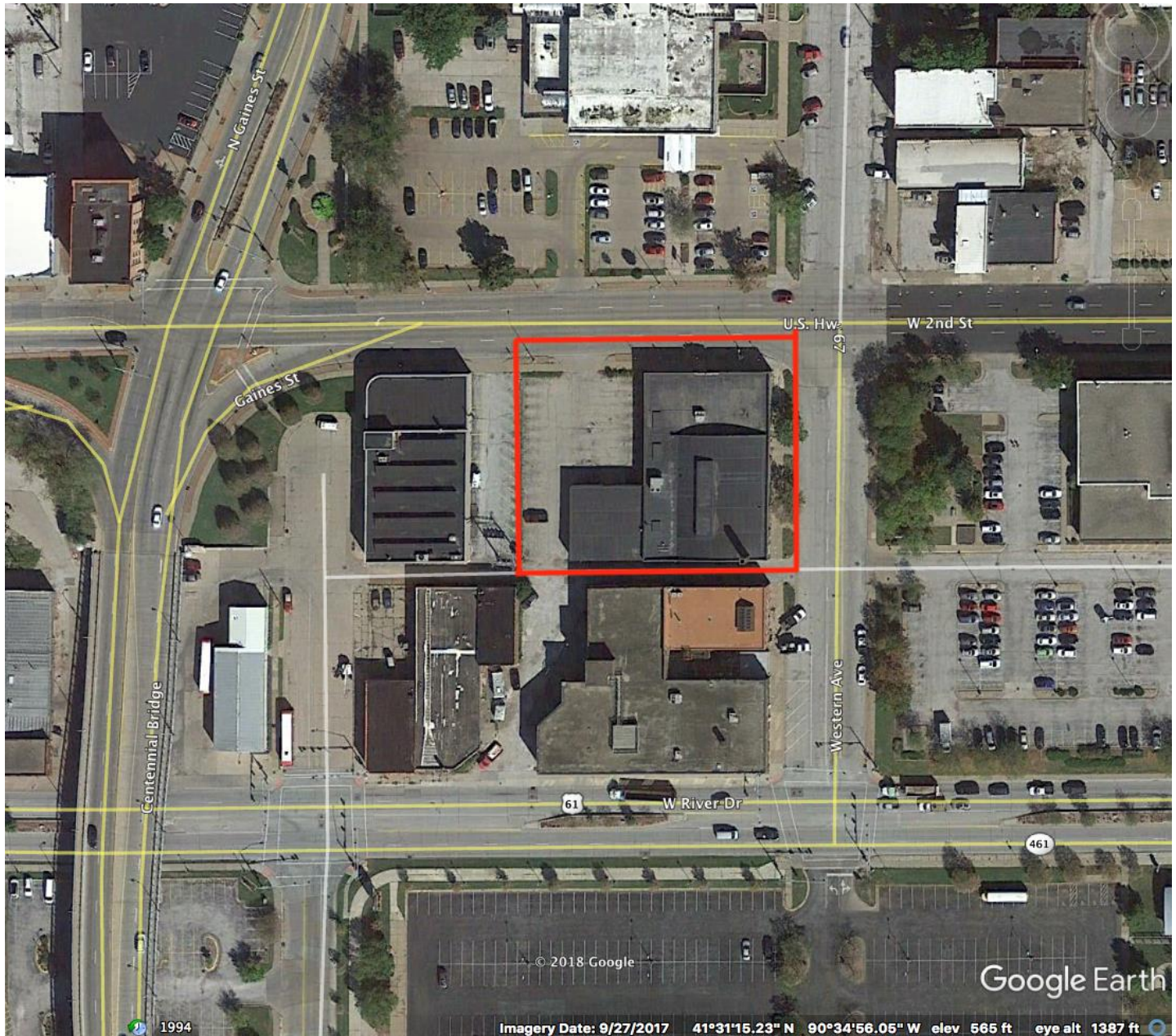


Figure 1

The red lines locate the boundaries of the International Harvester Truck Sales and Service Station.

Source: Google Earth, viewed January 8, 2019.



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## MAP WITH BAR SCALE

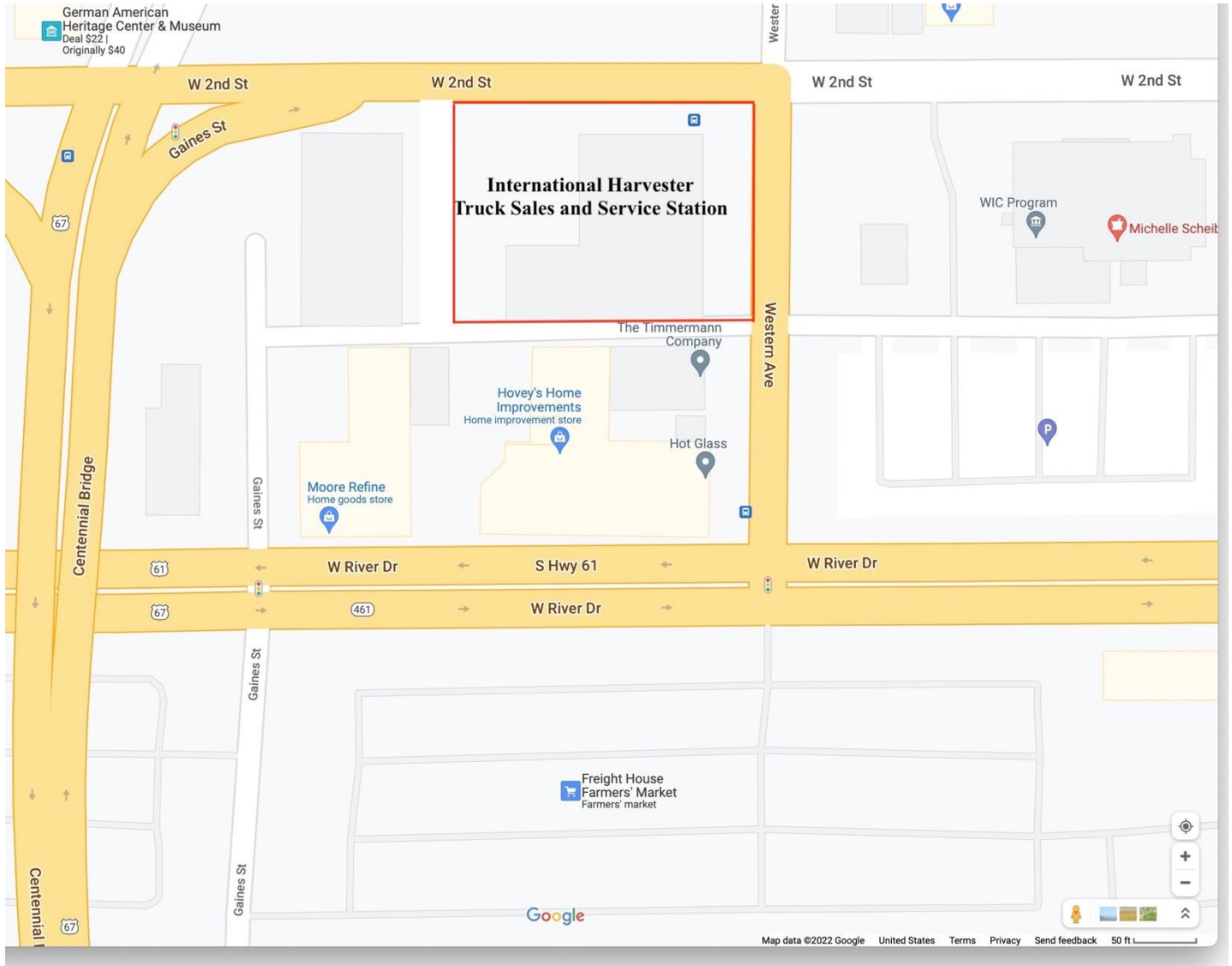


Figure 2

The red lines locate the International Harvester Truck Sales and Service Station. A bar scale is located in the lower right corner.

Source: Google Maps, viewed January 8, 2019.

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## TOPOGRAPHICAL MAP LOCATES PROPERTY



Figure 3

The arrow locates the International Harvester Truck Sales and Service Station within context.

Source: USGS US Topo 7.5 Minute Map, Davenport East, IA-IL, 2015.



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## ROOF SCHEME

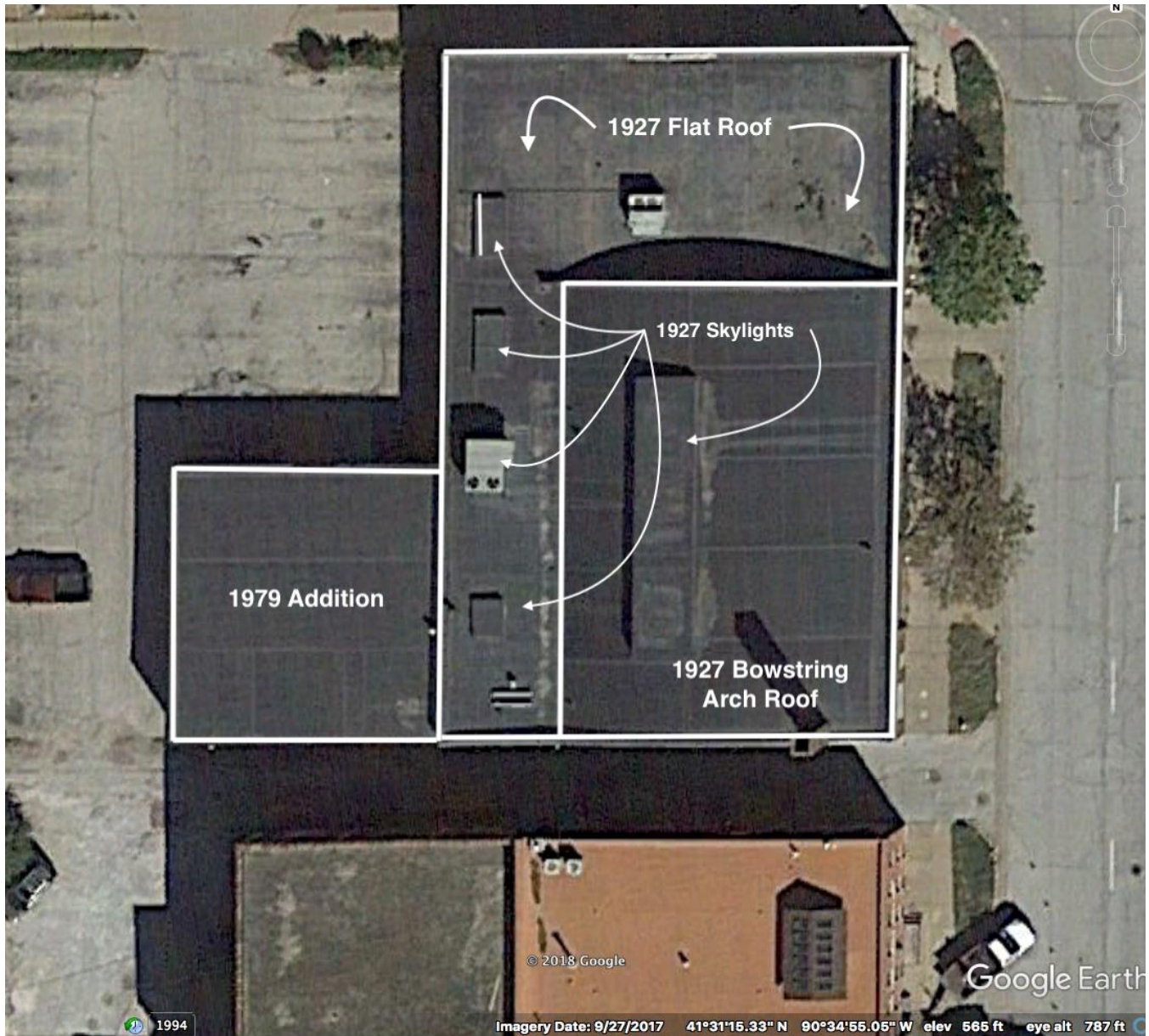


Figure 4

This aerial photograph clearly pictures the two components of the International Harvester Truck Sales and Service Station along with their roofs and skylights. One of the skylights is covered now with a HVAC chiller.

Source: Google Earth, January 8, 2019.

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## EXTERIOR IN 1928

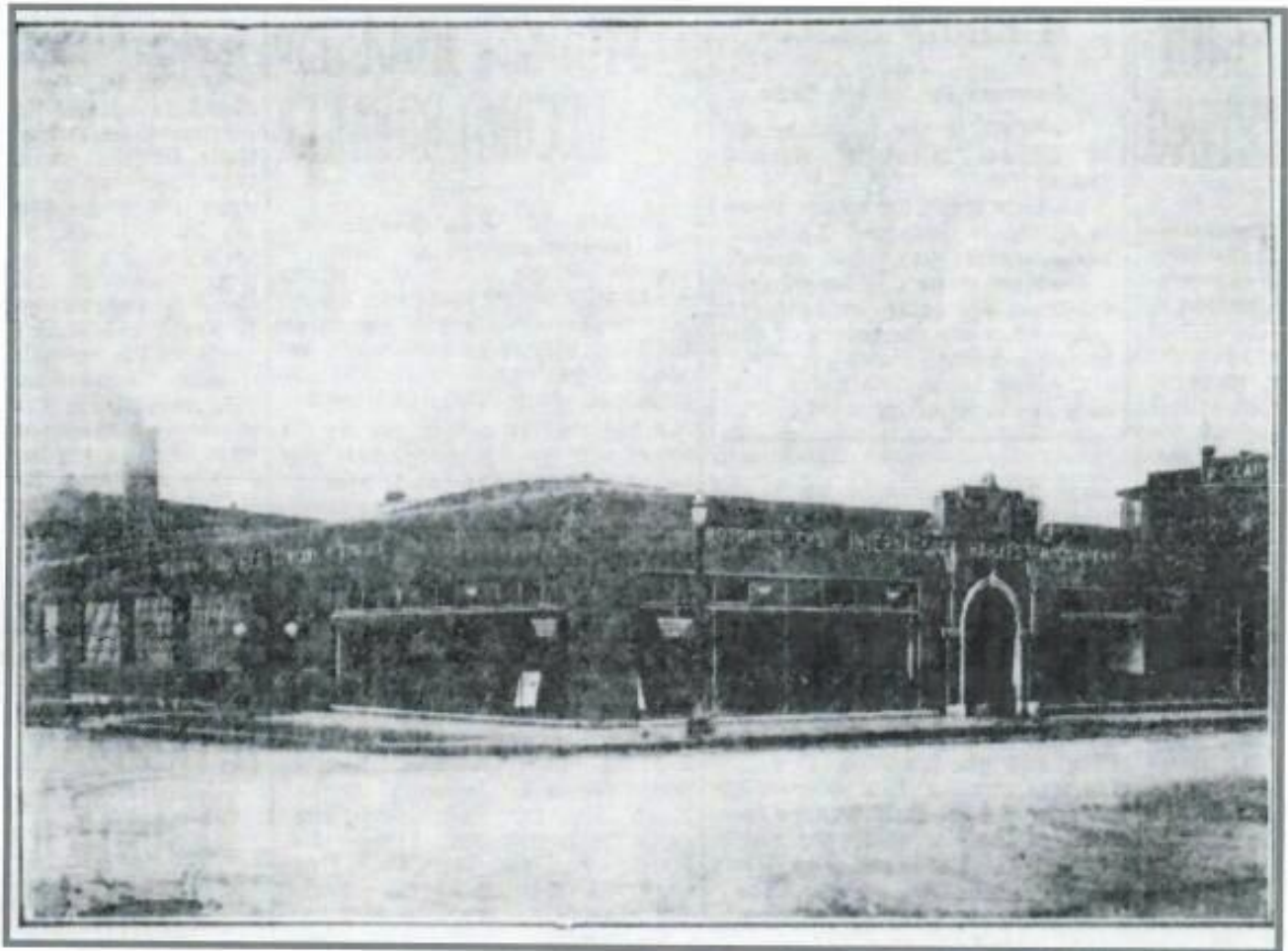


Figure 5

Looking to the southwest, this historic photograph pictures the exterior of the International Harvester Truck Sales and Service Station when newly completed. Storefront windows occupy almost all of the front façade and wrap around some of the east facade of the building. This location at the corner of West Second Street and Western Avenue offers excellent opportunity to display International products. The window at the far left appears slightly ajar, showing an awning style commercial window in operation. A smoke stack stands at the rear of the building; the arched roof over part of the building is also just visible. Although this photograph is fuzzy, it offers convincing evidence that the integrity of the building's design remains intact today.

This and the next two figures are taken from a newspaper advertisement announcing the formal opening of the International Harvester Truck Sales and Service Station on Friday and Saturday, February 3 and 4, 1928.

Source: *The Daily Times*, February 2, 1928.



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## INTERIOR IN 1928



Figure 6

Looking to the northeast, this historic photograph pictures the soaring expanse of the bowstring truss ceiling and the orderly arrangement of workstations in the newly opened International Harvester Truck Sales and Service Station. The main skylight in the ceiling is visible in the upper left-hand corner, its lighter color and gabled configuration differentiated from the rest of the darker plaster-clad ceiling. Some of the building's thirty, 200-watt light fixtures hang from the trusses. Abundant natural light pours in through the steel, industrial windows facing the east and Western Avenue. Accordion-style doors with geometric-paned transoms lead to the sales and display area on the north end of the building.

Source: *The Daily Times*, February 2, 1928.

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## INTERIOR IN 1928

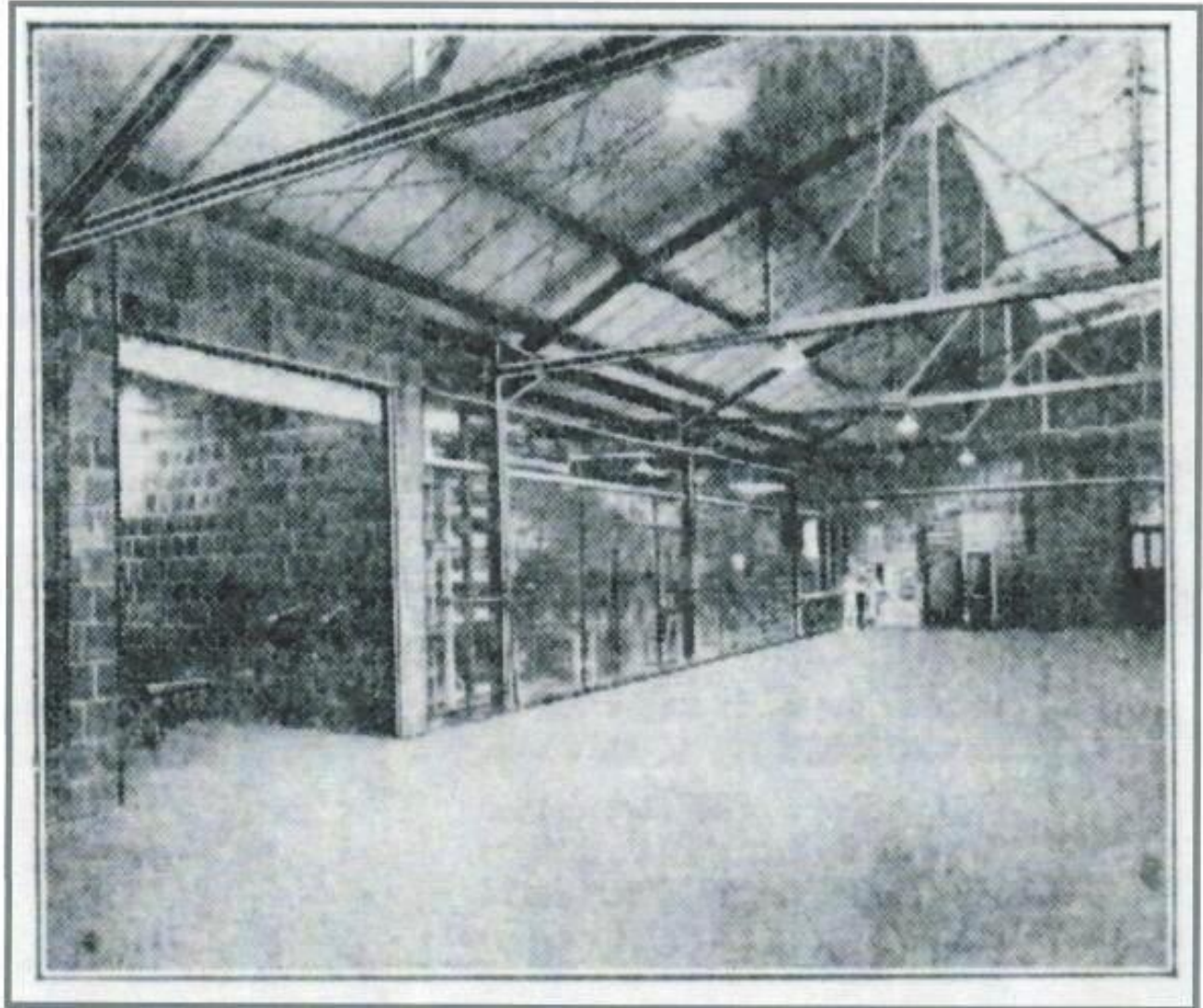


Figure 7

Looking to the northwest, this historic photograph pictures the Reconditioning Room, an alcove at the left, where vehicles were washed or painted using 500-pound pressure spray equipment. A drain in the floor in front of the doorway remains as evidence. The spaces enclosed with wire next to the right served as the Store Room, where an extensive inventory of International Harvester factory-made parts were kept at the ready for repairs.

Source: *The Daily Times*, February 2, 1928.



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## INTERIOR IN 1928

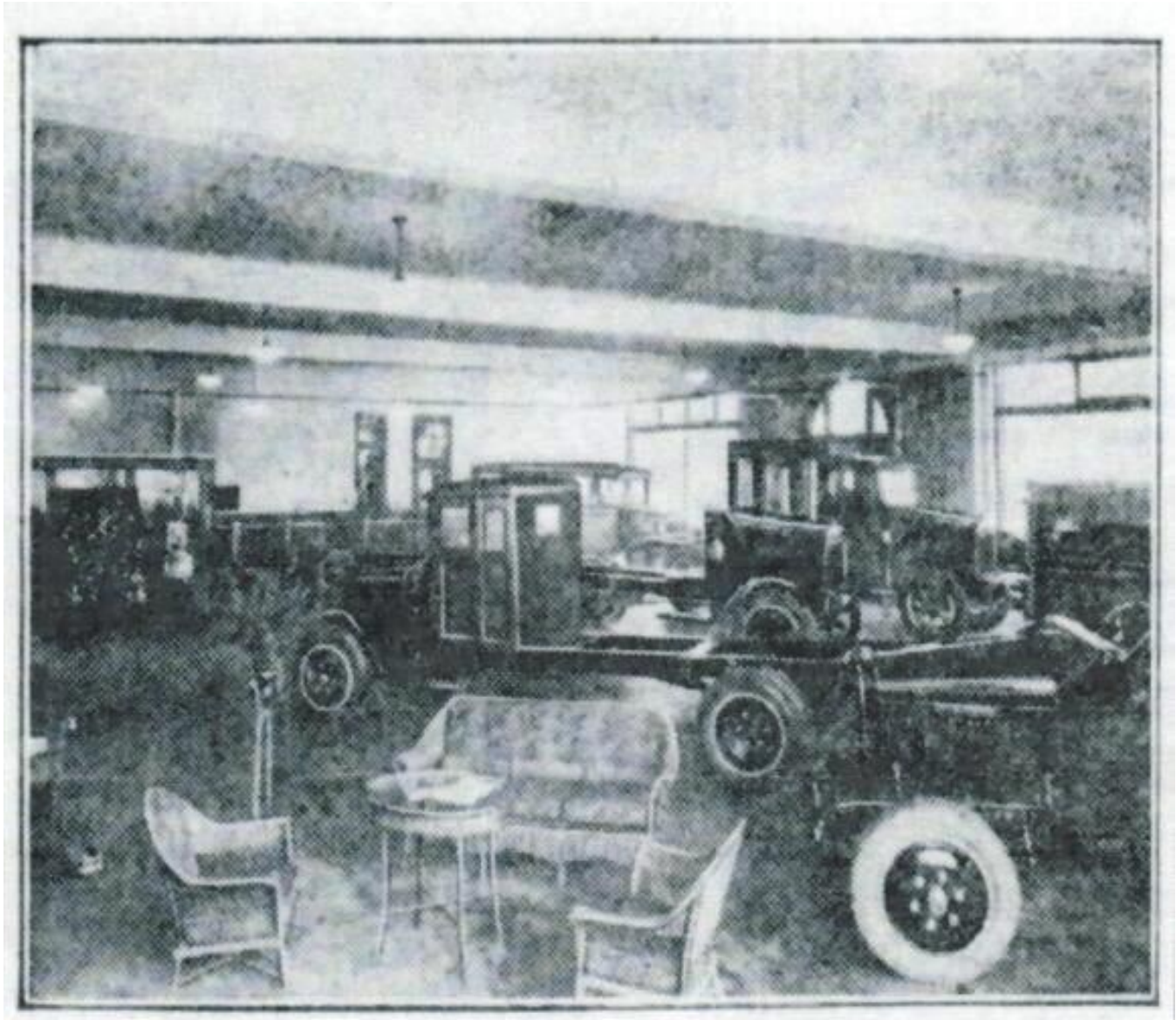


Figure 8

Looking to the northwest, this historic photograph of the Display Room (also called the Sales Room) pictures various IHC products, the customer waiting area, main entrance with transom, and vestibule. In the IHC tradition of trucks "built for the job," the beds of trucks on display remain unfinished, waiting for customized finishing.

Source: *The Daily Times*, February 2, 1928.

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## SETTING IN 1910

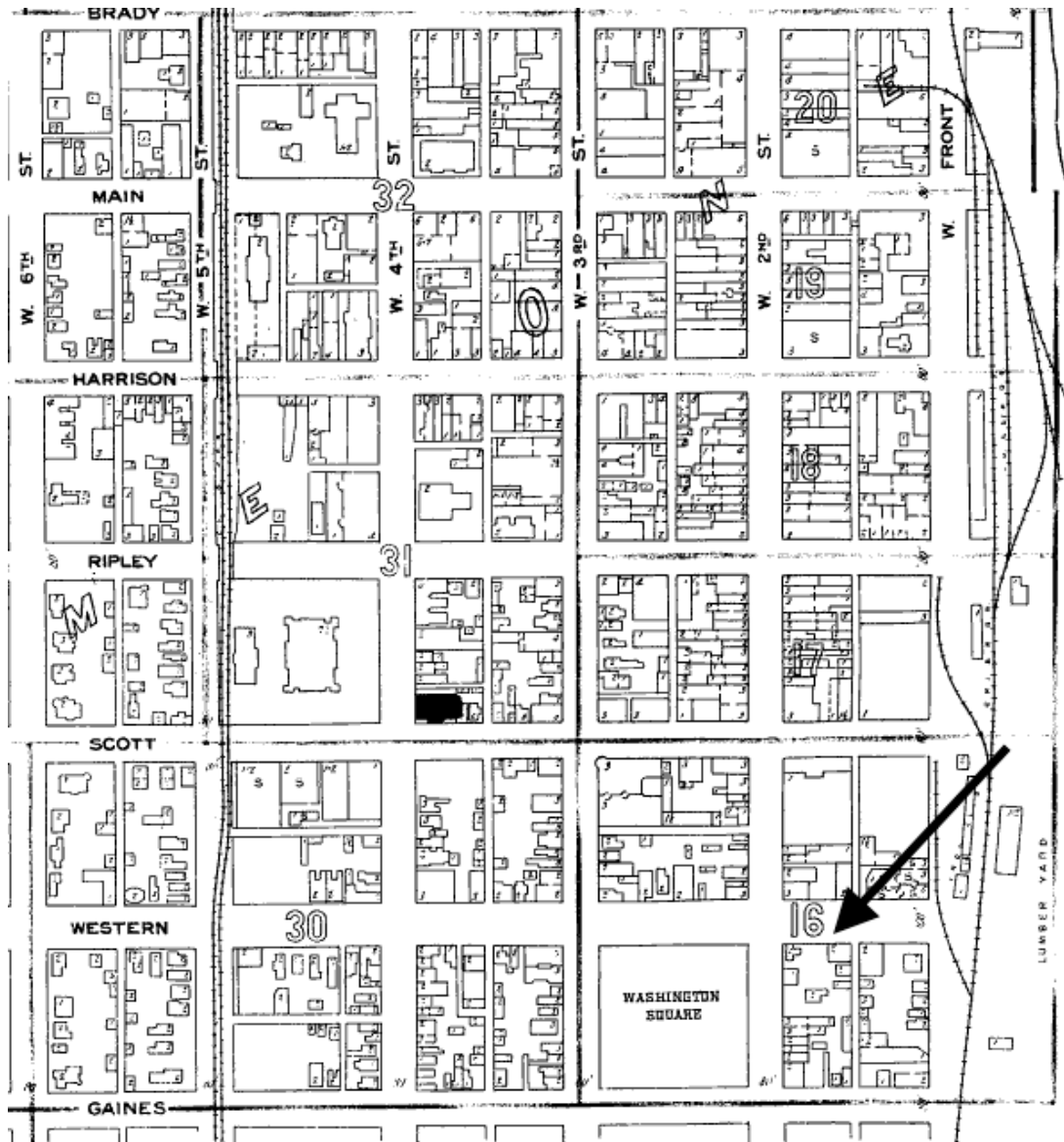


Figure 9

The arrow indicates improvements as of 1910 in the quarter block prior to where the International Harvester Truck Serve Station was placed in service in 1928. The scale of this new building far exceeded most of those in its surroundings, indicating an evolution of land use from residential to commercial taking place on the western edge of downtown Davenport during the prosperous 1920s. This evolution continued as a windshield survey of the area today attests.

Source: Sanborn Fire Insurance Map, Davenport, Iowa, 1910.



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## SETTING IN 1950

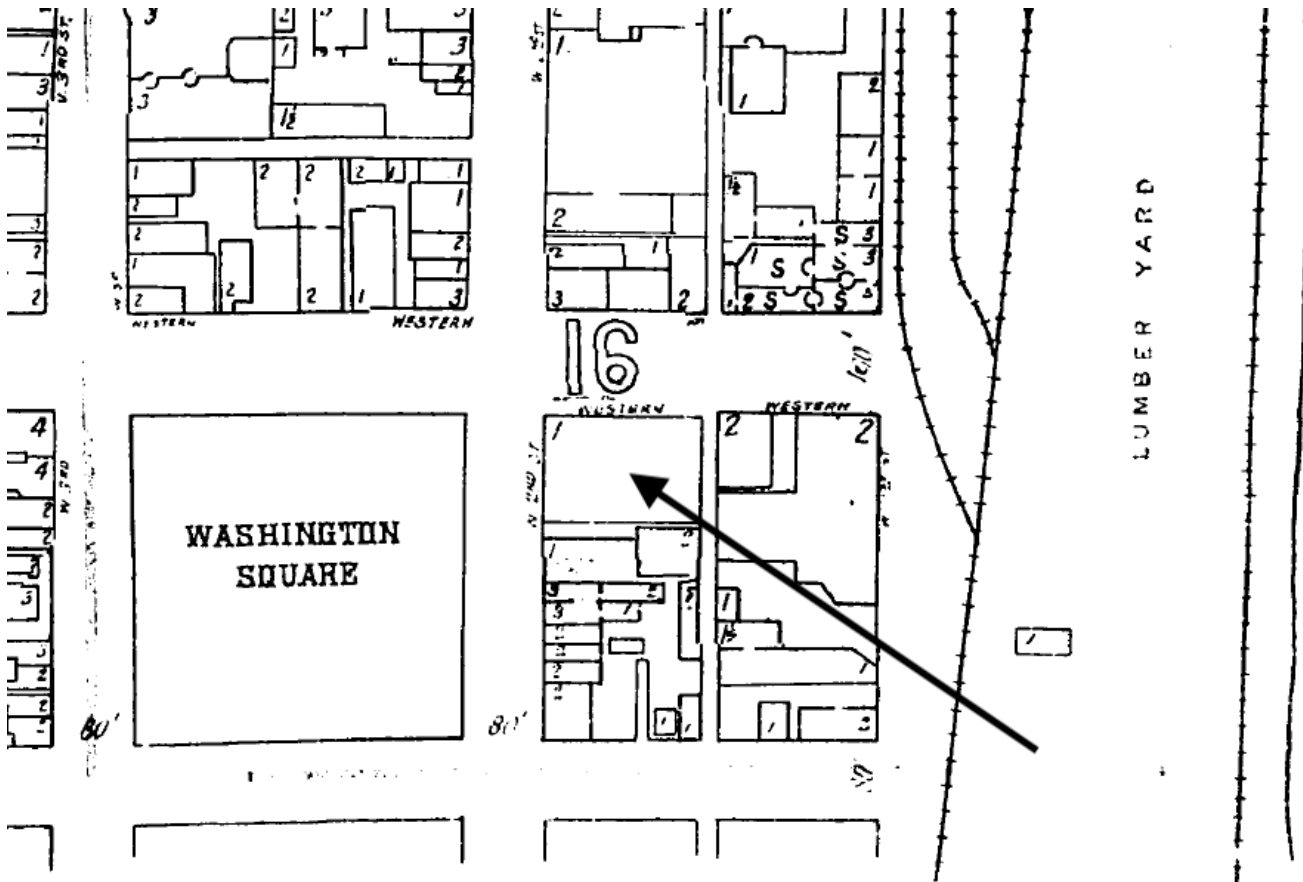


Figure 10

The arrow shows the footprint of the International Harvester Truck Sales and Service Station.

The building's prominent location adjacent to Washington Square, a public park, lent it prestige. Although this map reveals little about the service station, it shows that this building is replacing many of the smaller buildings in the neighborhood. The Mississippi River flows through Davenport from the east to the west at this point, and its north bank is visible at the far right.

Source: Sanborn Fire Insurance Map, Davenport, Iowa, 1910-May 1950.

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## SETTING IN 1956

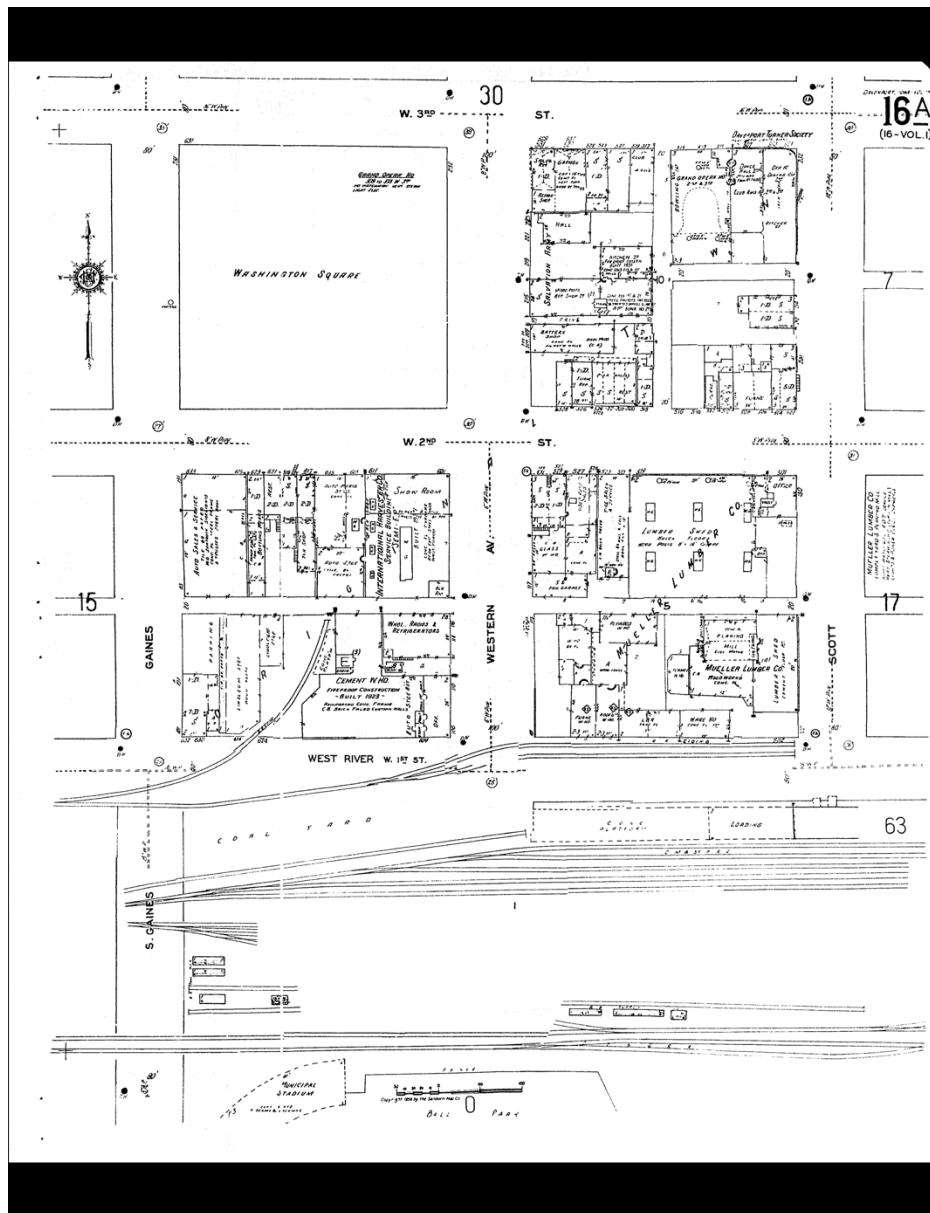


Figure 11

The dashed arrow locates the International Harvester Truck Sales and Service Station immediately south of Washington Square. This public park and its prominent location lent prestige and visibility to the firm's business.

Source: Sanborn Fire Insurance Map, Davenport, Iowa, 1956.



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## MAIN BLOCK IN 1956

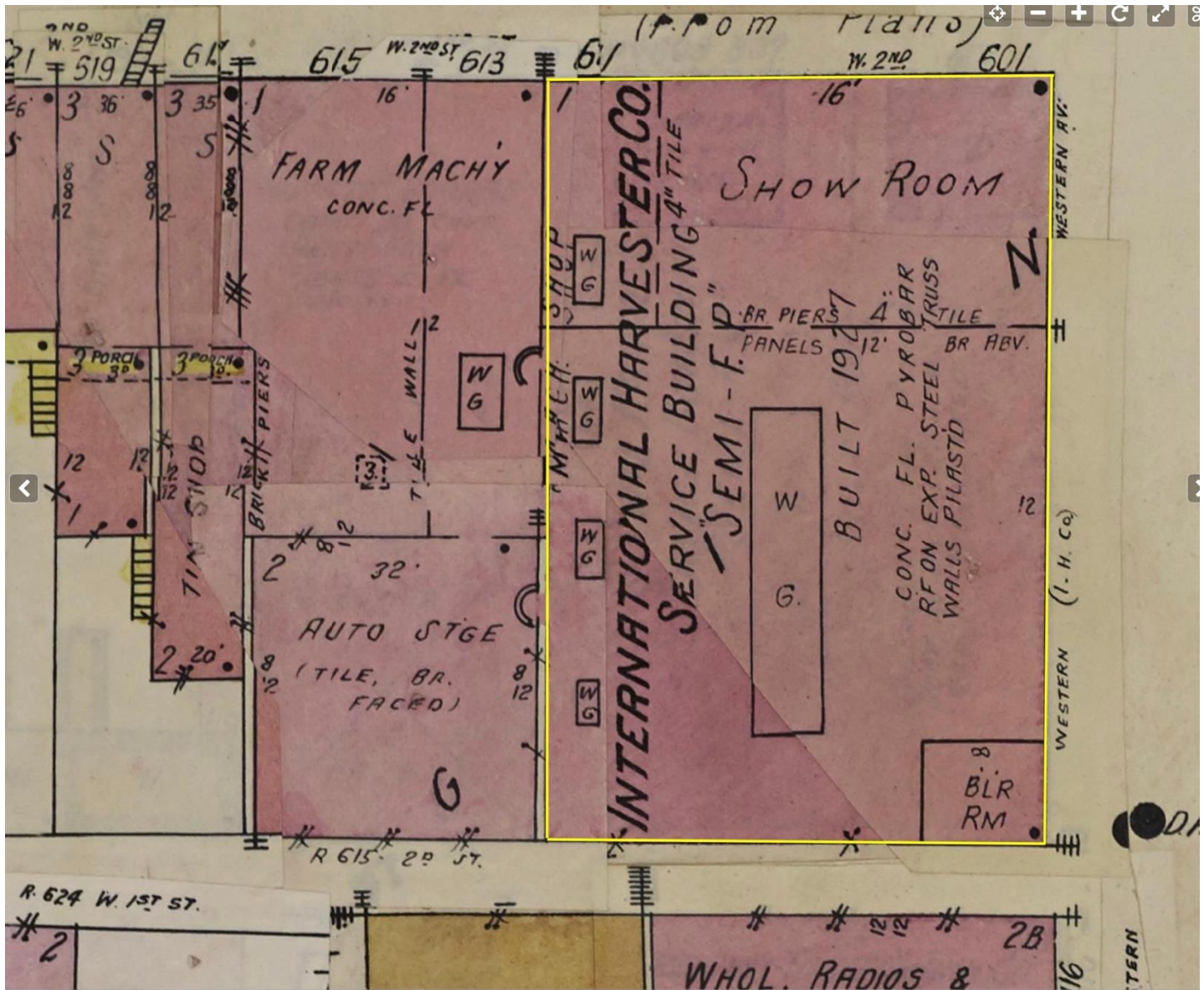


Figure 12

The yellow lines locate the International Harvester Truck Sales and Service Station and show its original floor plan.

Source: Sanborn Fire Insurance Map, Davenport, Iowa, 1956.



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## NORTH FACADE IN 1964

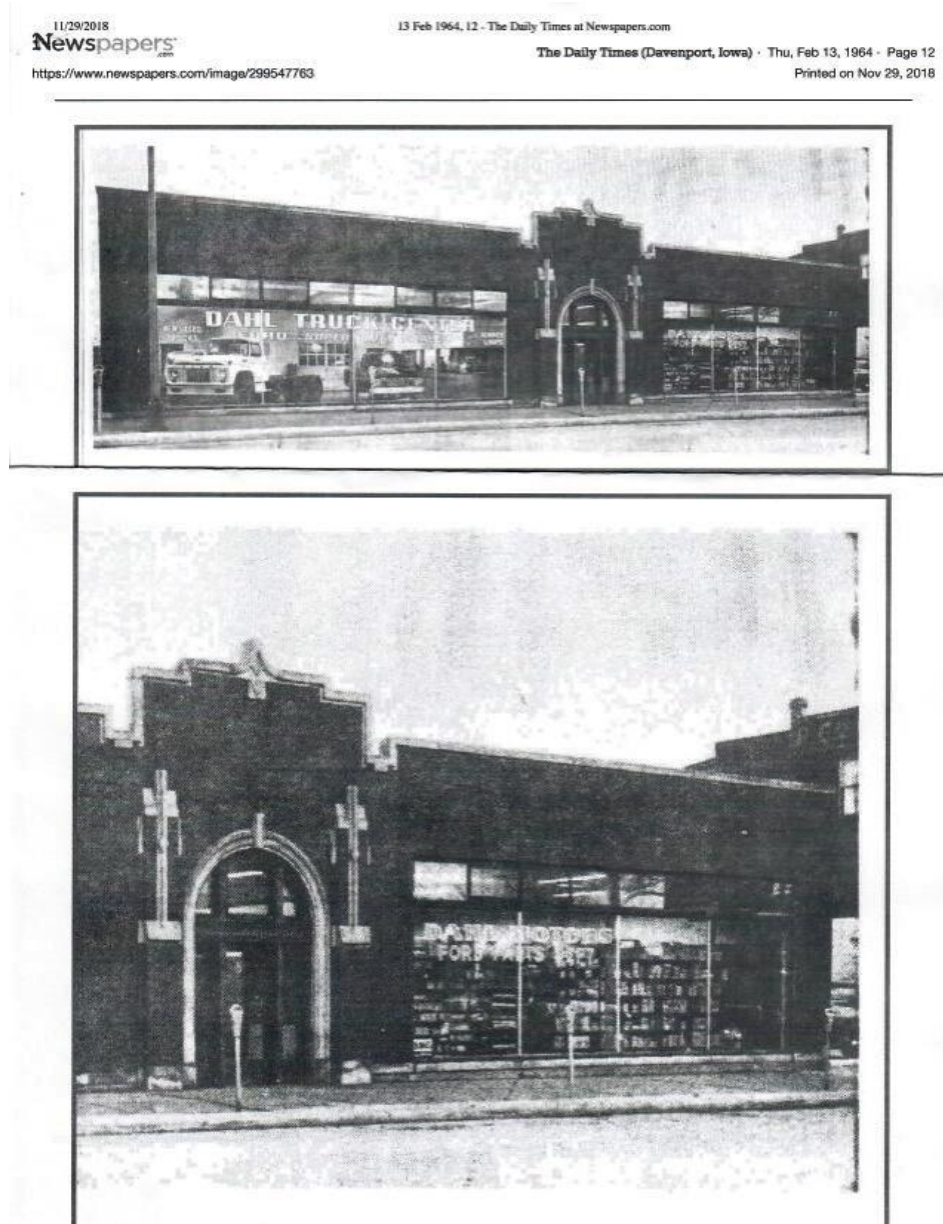


Figure 13

These two photographs picture the original storefront windows of the International Harvester Truck Sales and Service Station.

Source: *The Daily Times*, February 13, 1964.



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## BOWSTRING TRUSS USED AT THIS PROPERTY

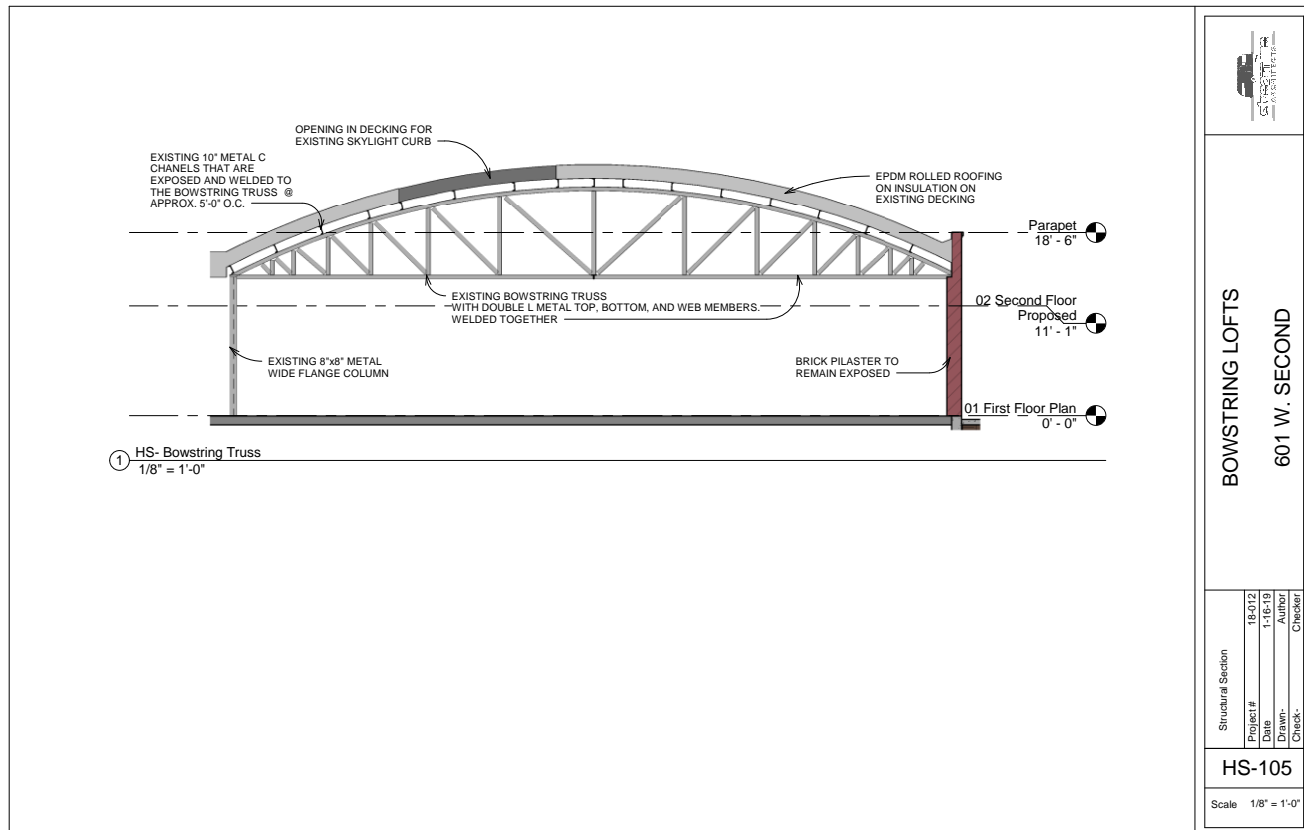


Figure 14

Source: Streamline Architects, Davenport, Iowa, 2019.

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## HISTORIC FLOOR PLAN

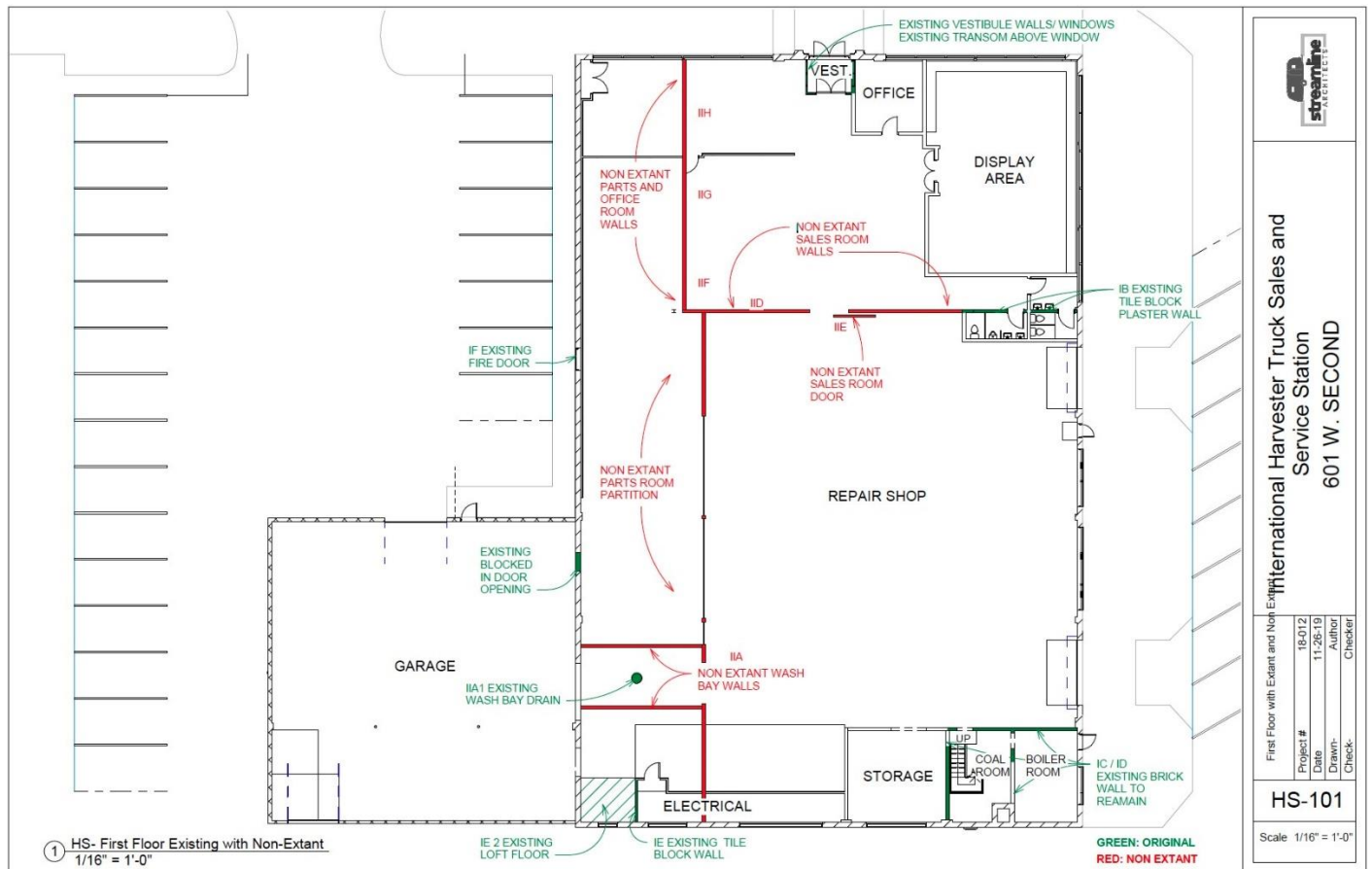


Figure 15

This drawing pictures the building's original and existing floor plan. Red lines indicate the building's original floor plan, including its nonextant Sales Room, Parts and Office Room, Repair Shop, Parts Room, and Wash Bay walls. If the viewer disregards the red lines, the building's existing floor plan becomes apparent. Figure 12 provides another drawing of the original floor plan.

Source: Drawing HS-101R, Streamline Architects, Davenport, Iowa, 2019.



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8. Statement of Significance

**Applicable National Register Criteria**

(Mark "x" in one or more boxes for the criteria qualifying the property for National Register listing.)

- ☒ A Property is associated with events that have made a significant contribution to the broad patterns of our history.
- ☐ B Property is associated with the lives of persons significant in our past.
- ☒ C Property embodies the distinctive characteristics of a type, period, or method of construction or represents the work of a master, or possesses high artistic values, or represents a significant and distinguishable entity whose components lack individual distinction.
- ☐ D Property has yielded, or is likely to yield, information important in prehistory or history.

**Criteria Considerations**

(Mark "x" in all the boxes that apply.)

Property is:

- ☐ A owned by a religious institution or used for religious purposes.
- ☐ B removed from its original location.
- ☐ C a birthplace or grave.
- ☐ D a cemetery.
- ☐ E a reconstructed building, object, or structure.
- ☐ F a commemorative property.
- ☐ G less than 50 years old or achieving significance within the past 50 years.

**Areas of Significance**

(Enter categories from instructions.)

Commerce

Architecture

**Period of Significance**

1928-1960

**Significant Dates**

1928

**Significant Person**

(Complete only if Criterion B is marked above.)

**Cultural Affiliation** (if applicable)

**Architect/Builder**

Hunzinger, J. H.

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### Statement of Significance

**Statement of Significance Summary Paragraph** (Provide a summary paragraph that includes level of significance, applicable criteria, justification for the period of significance, and any applicable criteria considerations).

The International Harvester Truck Sales and Service Station is National Register eligible, locally and under Criterion A, because it calls attention to the rise of trucking as a new mode of transportation during the early 20th century in Iowa and the commercial opportunities created by it. Erected to retail and service lightweight motorized trucks, this facility and its new product line capitalized on Iowa's emerging hard-surfaced highway system and promoted trucks as an efficient and convenient means of transport while initiating competition with railroads for long-haul freight carriage, obtaining to the present day.

The International Harvester Truck Sales and Service Station is National Register eligible, locally and under Criterion C, because its architectural design. The building transformed the existing garage type building in Davenport by employing steel bowstring trusses to support a vaulted roof over a vast interior space unencumbered by structural impediments. This innovative technology provided motorized vehicles greater freedom than hitherto to move about on the sales and service floor. The building also calls attention to other safety and efficiency features and the influence of the Commercial style of architecture with Romanesque and Gothic Revival ornament.

"Davenport Central Business District, Davenport, Iowa," a multiple property documentation (MPD) prepared in 2005 for resources in downtown Davenport, corroborates these determinations. According to this report, the International Harvester Truck Sales and Service Station is individually eligible, under Criterion A, within the context "The Impact of Commerce & Industry (c. 1836-1960)." (AKAY Consulting 2005: F-23) The building is also individually eligible, under Criterion C, within the context "The Impact of Architecture (c. 1880-c.1960)." (*Ibid.*: F-34) As to integrity, the MPD holds this building to a higher standard when considering its historic integrity because it is being nominated to the National Register as an individual rather than as a contributing resource. (*Ibid.*: F-17) Further, the report states that this building meets all registration requirements. (*Ibid.*: H-40)

The International Harvester Truck Sales and Service Station's period of significance, under Criterion C, is 1928, the time when the building was placed in service. The year 1928 is significant for this reason. The property's period of significance, under Criterion A, is 1928-1960, the time IHC occupied the site. The year 1960 is significant because IHC ceased operations at the site at that time.



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**Narrative Statement of Significance** (Provide at least one paragraph for each area of significance.)

(Iowa SHPO Additional Instructions: For properties not nominated under Criterion D, include a statement about whether any archaeological remains within or beyond the footprint of the property were assessed as part of this nomination under the subheading **Archaeological Assessment**.)

COMMERCE

International Harvester Trucks Emerge

The manufacturing of motorized trucks by International Harvester began in 1907. As this industrial giant developed this product line, IHC substantially contributed to the rise of trucking, now so common in the United States but distinct from river and rail shipping that had dominated shipping nationally in the 19th and early 20th centuries. It also foreshadowed the demise of the horse as a means for the American farmer to transport his agricultural products to market. Although prior to World War I trucks had not made inroads on the railroad carriage of these and other heavy products, by the 1920s trucks had begun competing with the railroads for that custom. (Thompson: 133)

The origins of IHC date to 1835 and Cyrus Hall McCormick's founding of McCormick Reaper and this company's manufacturing of farm equipment. The firm grew throughout the 19th century helping America to mechanize agriculture. In 1903, McCormick Reaper incorporated as International Harvester Company of America and subsequently launched research and development to produce motorized equipment, including trucks. The "Auto Wagon," the first of the firm's motorized vehicles, appeared in 1907. This air-cooled vehicle with a 2-cylinder engine featured high wheels designed to carry farm-to-market products over rough country roads. The name "motor truck" first appeared in 1910, and the trade name "International" was bestowed on these vehicles in 1914. At first, the auto-wagons were manufactured in Chicago, but production soon was relocated to Akron, Ohio. (International Harvester Company: 1-2)

By 1915, the utility of motorized trucks had become apparent across the nation, and IHC launched a new series of vehicles to meet the emerging market for them. IHC trucks were manufactured in Akron, Ohio, between 1915 and 1925, producing 56,685 trucks in ten different models during that time. This included the firm's first "heavy duty" truck with a capacity of five tons.

World War I proved to be a turning point for IHC and for truck manufacturers across the nation. The U.S. Government demand for war-related vehicles resulted in an industry-wide rise in truck production from 92,000 units in 1915 to 227,250 units in 1918. Only 49,000 units, however, were shipped overseas, and a vast surplus of vehicles became available for sale and civilian use when the war ended in 1918. This surplus stimulated the adoption of trucks for shipping across the nation and foreshadowed the decline of horsepower for farm-to-market transportation. The phrase "Ship by Truck" became commonplace. (*Ibid.*: 3)

This was an era of great expansion for the IHC line of trucks. In 1921, IHC's plant in Springfield, Ohio, was converted to truck production, and the company also introduced pneumatic tires for them. This innovation increased the performance of these so-called "speed" trucks at the same time that agitation by the Good Roads Movement promoted hard-surfaced highways across the nation.

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Speed trucks gained so much popularity and the truck industry expanded so rapidly that International production leaped from 7,183 motor trucks in 1920 to a new record of 39,008 trucks in 1928. Introduction of a new line in 1929 boosted production to 49,797 units. (*Ibid.*)

IHC also offered “built to the job” trucks—customizing them for specific needs. (International Harvester Company: 1-2) An historic photograph of the International Harvester Truck Sales and Service Station in Davenport pictures truck cabs and chaises without beds, possibly awaiting customer directives. (Figure 8)

IHC continued to expand and develop its truck line, introducing heavy-duty trucks in the 1930s. During World War II, IHC contracted with the U.S. Government to build all-wheel-drive military vehicles and other war materiel. Following the war, IHC introduced new product lines as the nation’s economy and over-the-road truck transportation boomed. Then, as the 20<sup>th</sup> century moved on, IHC lost its position in truck production to Detroit’s giant truck manufacturers and its leadership in agricultural machinery to competitors like Deere & Company. Then, during a mid-1980s national agricultural recession, International shocked many Americans by merging in 1984 with J. I Case Company to be known as Case IH.

International Harvester at 601 West Second Street

The opening of the International Harvester Truck Sales and Service Station in Davenport in 1928 and the subsequent success of its business calls attention to the emergence of trucking industry as a new mode of transportation in Iowa. By 1929, Davenport had emerged as the third largest city in Iowa and the fifth in terms of manufactured goods. (AKAY Consulting 2005: E-5) Trucks, as exemplified by the IHC facility in Davenport, played an important role in this development; and, following World War I, began competition with the railroads for the delivery of long-haul carload freight, which remains to the present day.

As hard-surfaced roads had improved overland transportation in the 1920s and 1930s, Davenport’s strategic location bridging the Mississippi River favored economic development. Already in 1899, 50,000 vehicles were crossing the Government Bridge across the Mississippi River between Davenport and Moline, Illinois. By 1923, that traffic had increased to 5,000,000. (Svendsen: 3-9) These statistics call attention to the massive increase in vehicular traffic locally during the period and the commercial opportunity for retailing and servicing cars and trucks.

A second bridge across the Mississippi River—the Centennial Bridge—opened in 1939, linking Rock Island and Davenport and serving U.S. Highway 6 between Greater Chicago and Omaha, Nebraska. This route--favoring Davenport when the rise of trucking in the 1930s sought highways of convenience--became a popular national route to the western agricultural areas of the Upper Midwest.

The development of a paved system of roadways and the 1939 construction of the four-lane Rock Island Centennial Bridge opened the city [of Davenport] to a greater influence from the surrounding areas and signaled a significant increase in heavy truck traffic, an indication of the health of city’s manufacturing base. (*The Palimpsest* quoted in AKAY Consulting 2005: E-5)

In 1928, IHC had located its new truck center in Davenport exactly because of this growing volume of traffic. All in all, this new facility stimulated the use of trucks for local and regional purposes, while promoting “heavy



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truck traffic,” now known as over-the road transportation. The Davenport facility stood among the earliest of IHC’s branch operations in Iowa. Already by this time, IHC had developed a widespread network of authorized, independent dealerships across the nation to retail and service the company’s agricultural machinery. An extensive, online inventory of these dealers has identified more than 1,200 of these IHC dealers working during the life of the company in Iowa alone. (Michael F. M. Haynes) While some dealers served as agents for IHC’s trucks, “many of them were small outfits that sold trucks as a sideline to their tractor and farm implement business.” (Foster: 46)

In Davenport, IHC’s new building and the speed of its construction captured public attention. Scheduled for completion within five months of start (*Quad-City Times*, May 2, 1927), construction began on August 1, 1927 and was rushed to completion by December 19, 1927. (*Quad City Times*, December 30, 1927) The building opened to the public on February 2 and 3, 1928. In addition to this speed of construction—boasted as a sign of efficiency—the International Harvester Corporation also touted its service department’s modern equipment and vast on-site inventory of factory-made replacement parts—another signal of efficiency.

The new building and the stock and equipment to be carried here will be valued at well over the \$200,000 mark. So expensive is the stock and equipment, it was stated, that it will represent almost two-thirds of that figure. (*Ibid.*, December 30, 1927)

The service department occupied an area 100’ x 70’ in size. The parts department occupied an area 20’ x 110’ in size. These facilities, complimented by a showroom 50’ x 80’ in size with a lounging area for customers to wait while repairs were made, rendered the new building a modern innovation in Davenport, Iowa.

Actually, Merchants Transfer & Storage Co., the titleholder of this site, had contracted for the building’s construction, leasing the edifice to the International Harvester Company in 1927 for ten years. J. H. Hunzinger and Company served as the general contractor for its construction at a published cost of \$45,490. (*The Daily Times*, August 13, 1927) This new International Harvester Company sales and service branch served an 18 county area, including four counties in Illinois and 14 in Iowa. (*Ibid.*)

During the early 20<sup>th</sup> century, IHC also established branch operations directly associated with the home office in Chicago. In addition to Cedar Rapids (1924) and Davenport (1928), these branches included Fort Dodge (1930), Council Bluffs (1931), Sioux City (1931), Cedar Falls (1931), and Dubuque (1933). (*Ibid.*) A branch office had been established in Des Moines in 1916. These branch operations carried the IHC name “International Harvester Company.” By way of contrast, IHC’s independent dealerships usually carried the dealer’s name or the place name of its location: “McGuire Farm Equipment” in Albia, for example, or “Burlington Farm Machinery Company” in Burlington.

As IHC expanded its truck production during this era, the opening of these branch centers allowed customers like Iowa farmers to view IHC products without relying on catalogs as at independent dealerships. (Ford: 8-5) The inclusion of “International Harvester” in the name of these corporate branches, as at Davenport, distinguished them from their independent peers and lent status to their operations. Customers could purchase over-the-counter spare parts at branch operations, for example, rather than waiting for a delivery order.

During World War II, IHC concentrated its industrial might to win the war. Although the government allowed the production of a restricted number of motor trucks for essential home front industries, the firm focused on

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manufacturing military trucks and other war materiel. (Foster: 68) By the end of the war in 1945, the nation's fleet of civilian trucks had taken a beating. (*Ibid.*: 74)

Following the war, the demand for new truck sales soared. (*Ibid.*) To accommodate local demand, in 1958 International Harvester built a new facility for truck sales at 2160 West River Drive in Davenport, thus ending more than three decades at its original location on West Second Street. (*The Daily Times*, June 3, 1960) That site shifted to selling used equipment. Later in 1960, IHC ceased operations at this site altogether.

Other businesses subsequently occupied the West Second Street property. In 1963, the building was serving as a venue for auction sales. By 1964, Dahl Motors, a Ford Motor Company franchise, occupied the building. In 1968, Eastern Iowa Community College purchased the building and remodeled the front office. Some demising walls from that remodeling still exist. Florist Supply, Inc., leased the building in 1978 from Builders Lime and Cement. Then, in 1982, O.V. Nielsen and Eileen C. Nielsen purchased the property from the Alter Company. The O.V. Nielsen Family Trust holds the title presently.

### Progressive Ideals

The International Harvester Truck Sales and Service Station reflects many of the ideals associated with the Progressive era in which it was built. These ideals include cleanliness, convenience, health, natural light, innovative technology, and fire protection, as noted elsewhere in this nomination.

The building also calls attention to the Progressive ideals of efficiency, standardization, and education. According to one local newspaper report:

Designated and executed with an eye to the greatest possible amount of efficiency is the new \$200,000 sales and service building which has just been completed by the International Harvester Co. for trucks at 601 West Second street. Every feature that has ever been used in many innovations have been incorporated in arrangement of the service station proper which occupies a large part of the 100 by 150 foot building. (*The Daily Times*, February 28, 1928)

International Harvester's public relations emphasized standardization and the ready availability of its factory-made inventory—"repair parts will be instantly available from an immense stock, systematically stored." (*Ibid.*)

Speed was another merited characteristic of the era associated with efficiency. The "instantly available" note in the above quote reflected this quality. Reporting on the construction of the new International Harvester Truck Sales and Service Station in 1927, one local newspaper reported: "Work is being rushed day and night to install the service station equipment. . ." (*Quad-City Times*, December 30, 1927)

As to education, IHC emphasized that its repairmen were "highly trained factory service men" (*Ibid.*) appealing to the respect for education, which characterized the Progressive era.

Through publicity like this, IHC signaled to customers the quality of its products and services in unspoken contrast with many of the era's smaller independent vehicle repair shops.



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## ARCHITECTURE

The International Harvester Truck Sales and Service Station is National Register eligible, locally and under Criterion C, because of its architectural design. The building's use of steel bowstring arch trusses to support its barrel ceiling calls attention to a new architectural form emerging in the early 20th century—the motorized vehicle sales and service facility. Enabled by this innovative structural engineering then emanating from Chicago, the interior of this building features a vast interior space, allowing free vehicular movement within it unencumbered by internal structural impediments. (Figures 6, 7) Further, the building calls attention to the influence of the Commercial style of architecture on its design, blending Romanesque and Gothic Revival ornament with structural functionalism.

### Structural Engineering

New facilities for motor vehicle indoor sales and service required a roofing system capable of spanning large, unimpeded interior spaces for display, repair, and movement. The employment of bowstring trusses to support such roofs admirably served this purpose. During the early 20th century, new industrial technology emanating from Chicago popularized the engineering capability of the bowstring truss for roofing. Throughout the rest of the century, bowstring trussed roofs served many uses, including bowling alleys, warehouses, armories, supermarkets and drugstores, indoor sports facilities such as skating rinks, and airplane hangars, among others.

The bowstring truss has the capability to span wide spaces. This engineering solution provided a structurally efficient method to support the International Harvester Truck Sales and Service Station's expansive roof. The bowstring truss uses an arrangement of triangles to transfer tension from its linear bottom and the compression from its curved top chord. The two chords meet at the ends of the arch and transfer the compression forces within the top chord to force the load bearing walls outward as well as downward. The roof of the International Harvester Truck Sales and Service Station possesses four bowstring trusses. Each truss features a double metal top chord, bottom chord, and web members with 28 triangular units between the chords. (Figure 14) The east ends of these bowstring trusses rest on and are supported by load-bearing brick pilasters, which form part of the east wall of the building; the west ends of the bowstring trusses are supported by steel columns within the building.

It should be noted that while the use of steel trusses for the roof was innovative at the time, the building's solid masonry walls providing the rest of its structural support remained traditional in design. As such, the International Harvester Truck Sales and Service Station calls attention to the transition from solid masonry load-bearing walls to what later emerged in the construction industry as steel frame and curtain walls.

During an era when fire safety was ever-present in the public's consciousness, the use of steel trusses for the International Harvester Truck Sales and Service Station offered greater protection from fire than standard wood trusses. Further, the building's rafters above the trusses also were constructed of steel rather than wood. These fireproofing measures added an important quality to this building in an era of raised public consciousness of the danger of gasoline and fire.

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Other practical concerns for this type of building included provisions for abundant natural and artificial light to aide repairmen. The skylight in the barrel-vaulted roof and the smaller skylights over other spaces in the building served this purpose along with the series of commercial windows on the building's east elevation. The barrel vaulted ceiling drew gasoline fumes up from the floor for ventilation through operable skylight windows.

By way of comparison, other early 20<sup>th</sup> century garages in Davenport exhibit barrel roof design. They include the Joehnke & Lage Garage (built circa 1925 at 210-214 Iowa Street) and the Rehder Motor Company (built circa 1930 at 207 Western Avenue). While these buildings employ bowstring arched trusses to support their roofs, these trusses are constructed of wood with limited span. The Rehder roof, for example, spans 40-feet. The International Harvester Truck Sales and Service Station roof spans 84-feet.

Other early 20<sup>th</sup> century garages in Davenport also employed steel roof trusses. Examples include the Black Hawk Chevrolet Co., Inc., at 221-223 East 2<sup>nd</sup> Street; Mack International Motor Truck Corp. at 728-732 West 1<sup>st</sup> Street; and Schwind Motor Car Co. at 420-422 Harrison Street. But these garages featured linear rather than arched designs, and the distances they spanned were modest by comparison with the International Harvester building. The Black Hawk Chevrolet trusses, for example, spanned 44-feet. Again, the steel arched trusses at the Internantional Harvester Truck Sales and Service Station offered greater unencumbered interior space than its peers in Davenport at the time.

Other Up-to-Date Features

The building offered many innovations for efficient service with an eye to safety, efficiency, and customer comfort. The building's masonry construction contributed its claim as fire-proof, furthered by the use of plaster rather than wood to clad its roof deck. This floor plan reflects a careful regard for the division of labor, spatial efficiency, and safety. The spare parts department was located in a separate section of the building. The showroom included an area for customers waiting while their vehicles were repaired, a convenience to the public. These and other specialized spaces provided an efficient layout for mechanical repairs and for customer comfort, all of which IHC touted with pride when the building first opened. (*Ibid.*, February 2, 1928)

According to one local newspaper:

The display room, 80 feet on Second street by 50 feet on Western avenue, is exposed to outside view from both thorofares, and the interior decoration has been worked out to give the best lighting effect. At night, the show room is flooded with 30 concealed 200 watt reflectors, an innovation creating everything desired from a lighting standpoint. (*Quad-City Times*, December 30, 1927)

The light fixtures (actually electrical sockets) fronting Western Avenue remain extant above the showroom's dropped ceiling. (William V. Nielsen)

This emphasis on specialization affected other aspects of the business. "Every tool and machine known to modern repair service is included. Men who are experts in their line are assigned to repair certain parts of the trucks." (*The Daily Times*, February 2, 1928)



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When International leased this site in 1927, the firm had envisioned a \$47,500 facility to be erected within five months. As planning for it continued, this radically changed; and, although it took about eight months to build, its final cost of \$200,000 far exceeded that initially publicized. While this 6-figure number included equipment and furnishings, the difference still indicates a substantial commitment by International to upgrade its new facility.

In addition to mechanical repairs, this service station also offered facilities for bodywork. The Reconditioning Room included spray equipment with 500-pound pressure for washing vehicles and for spray-painting them.

Commercial Style

Influenced by the Commercial Style of architecture, the exterior of the International Harvester Truck Sales and Service Station nicely blends the utilitarian look of its fenestration with architectural ornament to render a commercial building attractive to early 20th century eyes.

The building's exterior walls and parapets frame the storefront windows, anchor them in space, and provide a counterpart to their functionalism. These brick surfaces feature a panoply of polychrome and textures. This rich array of brick coloration includes green, yellow, blue, and red. The use of green and blue is uncommon. Additionally, the brick features a rough and slightly glazed surface. These characteristics point to a quality product purchased at a premium over typical face brick common to the period. The fact that this face brick is used throughout the exterior—not just on the primary facades—further testifies to a commitment to quality. The use of pink mortar to bind this brick serves as another notable design feature. Pink mortar is used on the north and east primary elevations, while natural gray color is used on the south and west secondary elevations. This pink color lends a deeper, richer, and more substantial feeling to the north and east elevations than the gray-colored mortar on the south and west elevations. There, the lighter-colored mortar renders a checkerboard effect to the walls, breaking up the visual strength affected by the darker colored mortar on the primary facades.

The International Harvester Truck Sales and Service Station exhibits other stylistic features associated with the Commercial Style. As the Pennsylvania Historical & Museum Commission notes:

Some examples of this style employ decorative elements of other popular styles of the era, such as Romanesque or Gothic Revival ornament. (Pennsylvania Architectural Field Guide)

This description fits the exterior design of the International Harvester Truck Sales and Service Station. Its north facade is conceived in a symmetrical fashion with its main entrance centering the facade. This entrance is flanked by Gothic-like buttresses and surmounted by a prominent Romanesque arch.

These exterior design elements conveyed a stylish and contemporary feeling to the building, assuring customers that the occupant-business was both trustworthy and up-to-date. By the same token, the over-arching volume of the barrel vaulted service department must have impressed any customer with its industrial technology.

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Architect

To date, the architect or architectural firm responsible for the design of the International Harvester Truck Sales and Service Station remains unidentified. During the 1920s, a number of well-regarded and experienced architects practiced in Davenport. Local newspapers frequently published stories about new construction in the city. Numerous stories appeared in the local press about the construction of the IHC building, as well as identifying its general contractor. All of this suggests that the building's architectural design emanated internally from IHC headquarters in Chicago.

General Contractor

J. H. Hunzinger and Company served as the general contractor for the International Harvester Truck Sales and Service Station. A local newspaper account published this information obtained from a building permit for the building. At this time, the cost for this construction was said to be \$45,490. (*The Daily Times*, August 13, 1927)

Three brothers founded J. H. Hunzinger and Company in 1907. Frank, Fred, and Charles Hunzinger wished to leave their family farming background in Iowa so they established the firm to engage in construction. Their contract for the construction of the International Harvester Truck Sales and Service Station in Davenport in 1927 might have served as a turning point for the firm. In any case, Frank and Fred relocated their business to Milwaukee, Wisconsin, the following year, specializing in schools, churches, and, later, office buildings and manufacturing plants. The Hunzinger Construction Company remains in business to the present day.

(<https://www.hunzinger.com/who-we-are/>)<sup>1</sup>

SELECTION OF HISTORIC NAME

When originally opened to the public in 1928, IHC named this building the "International Harvester Truck Sales and Service Station." When IHC relocated this branch dealership in Davenport in 1960 to a new site, IHC employed nearly the same name: "International Harvester Truck Sales and Service Center" (*The Daily Times*, June 3, 1960), showing the longevity of this rather complicated but highly descriptive name. The earlier name was chosen for this nomination because it served as the building's original name.

<sup>1</sup> For further information, the Richardson-Sloane Special Collections Center of the Davenport Public Library archives a collection of photographs and information regarding specific projects associated with J. H. Hunzinger and Company while still located in Iowa. (<https://archives.davenportlibrary.com/repositories/3/resources/66>)



International Harvester Truck Sales and  
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## 9. Major Bibliographical References

**Bibliography** (Cite the books, articles, and other sources used in preparing this form.)

AKAY Consulting

2004 "International Harvester." Iowa Site Inventory Form, prepared for City of Davenport, Iowa, and State Historical Society of Iowa. On file, State Historical Society of Iowa, Des Moines, IA.

AKAY Consulting

2005 "Davenport Central Business District, Davenport, Iowa" Multiple Property Documentation prepared for City of Davenport, Iowa, and State Historical Society of Iowa. On file, State Historical Society of Iowa, Des Moines, IA.

Bowers, Martha H.

n.d. "The Historic Resources of Davenport, Iowa." Multiple Property Documentation Form. State Historical Society of Iowa, Des Moines, IA.

*Davenport City Directory*

1929 R. L. Polk & Co., Publishers, Davenport, Iowa.

Ford, Susan Jezak

2002 "International Harvester Building, Wichita, Kansas." National Register of Historic Places nomination prepared by Citysearch Preservation. U.S. Department of Interior, National Park Service, Washington, D.C.

Foster, Patrick

2015 *International Harvester Trucks: The Complete Story*. Motorbooks, Quarto Publishing Group USA, Inc., Minneapolis, Minnesota

Gowans, Alan

1992 *Styles and Types of North American Architecture: Social Function and Cultural Expression*. Icon Editions, Harper Collins, New York, NY.

Hunzinger Construction Company

2019 Website <https://www.hunzinger.com/who-we-are/>. Viewed January 15, 2019.

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1961 "A History of International Trucks." News bulletin of the Consumer Relations Department. Chicago, Illinois.

Iowa Secretary of State

2018 Iowa State corporation renewal records.

"Pennsylvania Architectural Field Guide"

2019 Pennsylvania Historical & Museum Commission, website viewed January 22, 2019.  
<<http://www.phmc.state.pa.us/portal/communities/architecture/styles/commercial.htm>>

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Sanborn Fire Insurance Co.

1929 "Davenport, Iowa." Sanborn Fire Insurance Co., New York, NY.

Scott County Assessor's Office

2018 Property Record Card, 601 West Second Street, Davenport, IA.

Svendsen, Marlys; John Pfiffner; Martha H. Bowers

n.d. "Davenport, Where the River Runs West: A Survey of Davenport History & Architecture." Report prepared for City of Davenport, Iowa, and State Historical Society of Iowa. On file, State Historical Society of Iowa, Des Moines, IA.

Svendsen, Marlys

1987 *Davenport, A Pictorial History*. Bradley Publishing, Inc., New York, NY.

Thompson, William H.

1989 *Transportation in Iowa: A Historical Survey*. Iowa Department of Transportation, Ames, IA.

Newspapers

*Daily Times, The*

1927 Building Permit for International Harvester Truck Sales and Service Station, August 13, 1927.

*Daily Times, The*

1928 "Many Innovations At Service Plant Of Harvester Co." February 2, 1928.

*Daily Times, The*

1928 "Formal Opening of A New International Harvester Truck Service Station." Advertisement w/photographs, February 2, 1928.

*Daily Times, The*

1960 "You're Invited to the grand opening of our new International Truck Sales and Service Center." June 3, 1960.

*Daily Times, The*

1963 "Extraordinary Estate and Closing Out Sale." June 18, 1963.

*Quad-City Times*

1927 "\$7,500 Building Will Be Erected on Second St. For International Harvester Co." May 22, 1927.

*Quad-City Times*

1927 "Increases Size Of Structure." July 13, 1927. This concerns the IHC Moline-East Moline plant, another contemporary structure in the Quad Cities constructed by the firm and indication of the company's expansion during the 1920s.



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Internet

Haynes, Michael F. M.

2019 "International Harvester Dealers of the Past." <[http://www.ihdealerspast.net/states/ia\\_id.pdf](http://www.ihdealerspast.net/states/ia_id.pdf)>  
Comprehensively listing of IHC dealerships, last updated on March 2, 2019 showing more than  
1,200 Iowa dealerships and viewed on March 16, 2022. Author died in 2003 and status of on-going  
project unknown.

Oral History

Nielsen, William V.

2019 Oral informant interview with William C. Page, January 15, 2019. Nielsen is the son of O. V.  
Nielsen, president of the former Florist Distributing Co. William was actively involved in various  
rehabilitation projects for the IHC building, including the construction of the mezzanine offices.

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**Previous documentation on file (NPS):**

☐ preliminary determination of individual listing (36 CFR 67) has been requested

☐ previously listed in the National Register

☐ previously determined eligible by the National Register

☐ designated a National Historic Landmark

☐ recorded by Historic American Buildings Survey

#

☐ recorded by Historic American Engineering Record

#

☐ recorded by Historic American Landscape Survey

#

**Primary location of additional data:**

☒ State Historic Preservation Office

☐ Other State Agency

☐ Federal Agency

☐ Local Government

☐ University

☐ Other

☐ Name of repository:

**Historic Resources Survey Number (if assigned):**

International Harvester Truck Sales and  
Service Station

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## 10. Geographical Data

**Acreage of Property** Less than one acre.

(Do not include previously listed resource acreage; enter "Less than one" if the acreage is .99 or less)

### Latitude/Longitude Coordinates

Datum if other than WGS84: \_\_\_\_\_

(enter coordinates to 6 decimal places)

1	<u>41.311533</u>	<u>90.345605</u>	3	_____	_____
	Latitude	Longitude		Latitude	Longitude
2	_____	_____	4	_____	_____
	Latitude	Longitude		Latitude	Longitude

### Verbal Boundary Description (Describe the boundaries of the property.)

West Second Street at corner of Western Avenue, south along Western Avenue to first alley, west along alley to first alley, north along alley to West Second Street, and east along West Second Street to point of beginning, in Davenport, Iowa. This boundary embraces Lots 5 and 6 in Block 4 in the City of Davenport's original plat.

### Boundary Justification (Explain why the boundaries were selected.)

Contains the site historically associated with the building known as the International Harvester Truck Sales and Service Station.

## 11. Form Prepared By

name/title	<u>William C. Page, Public Historian</u>	date	<u>July 1, 2022</u>
organization	<u>William Colgan Page, Inc.</u>	telephone	<u>515-243-5740</u>
street & number	<u>520 East Sheridan Avenue</u>	email	<u>page@dwx.com</u>
city or town	<u>Des Moines</u>	state	<u>IA</u> zip code <u>50313</u>

### Additional Documentation

Submit the following items with the completed form:

- **GIS Location Map (Google Earth or BING)**
- **Local Location Map**
- **Site Plan**
- **Floor Plans (As Applicable)**
- **Photo Location Map** (Key all photographs to this map and insert immediately after the photo log and before the list of figures).



, Iowa

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Name of Property County and State

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**Photographs:**

Submit clear and descriptive photographs under separate cover. The size of each image must be 3000x2000 pixels, at 300 ppi (pixels per inch) or larger. Key all photographs to the sketch map. Each photograph must be numbered and that number must correspond to the photograph number on the photo log. For simplicity, the name of the photographer, photo date, etc. may be listed once on the photograph log and does not need to be labeled on every photograph.

**Photo Log**

**Name of Property:** International Harvester Truck Sales and Service Station

**City or Vicinity:** Davenport, Iowa

**County:** Scott **State:** IA

**Photographer:** William C. Page; William V. Nielsen (2-9, 20)

**Date Photographed:** January 8, 2019; April 14, 2020, ; May 15, 2022

Description of Photograph(s) and number, include description of view indicating direction of camera:

**Photo 1 of \_25\_:**

1. North elevation looking SW
2. North elevation looking SE
3. Main entrance, north elevation looking SW
4. East elevation (north side) looking SW
5. East elevation (south side) looking SW
6. South elevation (left) & east elevation (right) looking NW
7. South elevation looking NW
8. Addition west elevation looking NE
9. Addition west and south elevations looking SE
10. Main block and addition looking SE
11. Exterior brick looking W
12. Lobby with entrance vestibule looking NW
13. Main office with entrance vestibule looking NE
14. Main lobby looking NW
15. Showroom looking NW
16. Main floor looking SSE
17. Main floor looking SE
18. Main floor looking NE
19. Main floor looking NW
20. Typical commercial window looking E
21. Roof trusses exposed looking N
22. Skylight exposed looking N
23. Bowstring trusses exposed looking N
24. Addition interior looking NE
25. Addition interior looking SW

International Harvester Truck Sales and Service Center, Davenport, Iowa



Photo #1. North elevation looking SW, shot 01-08-21.



Photo #2. North elevation looking SE, shot 05-15-22.



International Harvester Truck Sales and Service Center, Davenport, Iowa



Photo #3. Main entrance, north elevation looking SW, shot 05-15-22.

International Harvester Truck Sales and Service Center, Davenport, Iowa



Photo #4. East elevation (north side) looking SW, shot 05-15-22.



International Harvester Truck Sales and Service Center, Davenport, Iowa



Photo #5. East elevation (south side) looking SW, shot 05-15-22.



Photo #6. South elevation (left) & east elevation (right) looking NW, shot 05-15-22.



International Harvester Truck Sales and Service Center, Davenport, Iowa



Photo #7. South elevation looking NW, shot 05-15-22.



Photo #8. Addition, west elevation (left) & south elevation (right) looking NE, shot 05-15-22.

International Harvester Truck Sales and Service Center, Davenport, Iowa



Photo #9. West (left) and south (right) elevations of Addition looking SE, shot 05-15-22.



Photo #10. Main block (left) & Addition (left) looking SE, shot 05-15-22.



International Harvester Truck Sales and Service Center, Davenport, Iowa



Photo #11. Exterior brick (sample) looking W, shot 01-08-19.



Photo #12. Lobby w/entrance vestibule (center left) looking NW, shot 01-08-19.

International Harvester Truck Sales and Service Center, Davenport, Iowa



Photo #13. Main office w/entrance vestibule (center) looking NE, shot 01-08-19.



Photo #14. Main lobby looking NW, shot 01-08-19.



International Harvester Truck Sales and Service Center, Davenport, Iowa



Photo #15. Showroom looking NW, shot 01-08-19.



Photo #16. Main floor looking SSE, shot 01-08-19.

International Harvester Truck Sales and Service Center, Davenport, Iowa



Photo #17. Main floor looking SE, shot 01-08-19.



Photo #18. Main floor looking NE, shot 01-08-19.



International Harvester Truck Sales and Service Center, Davenport, Iowa



Photo #19. Main floor looking NW, shot 01-08-19.



Photo #20. Typical commercial style window looking E, shot 04-14-20.

International Harvester Truck Sales and Service Center, Davenport, Iowa



Photo #21. Roof trusses exposed looking N, shot 01-08-19.



International Harvester Truck Sales and Service Center, Davenport, Iowa



Photo #22. Skylight exposed looking N, shot 01-08-19.



Photo #23. Bowstring trusses exposed looking N, shot 01-08-19.



International Harvester Truck Sales and Service Center, Davenport, Iowa



Photo #24. Addition looking NE, shot 01-08-19.



Photo #25. Addition looking SW, shot 01-08-19.



# IOWA DEPARTMENT OF CULTURAL AFFAIRS

IOWA ARTS COUNCIL    PRODUCE IOWA    STATE HISTORICAL SOCIETY OF IOWA

KIM REYNOLDS, GOVERNOR  
ADAM GREGG, LT. GOVERNOR

CHRIS KRAMER, DIRECTOR

December 10, 2022

Mayor Mike Matson  
Mayor of Davenport  
226 West 4th Street  
Davenport, IA 52801

RE:    **International Harvester Truck Sales and Service Station**  
      **601 West Second Street**  
      **Davenport, IA**

Dear Mayor:

We are pleased to inform you that the above-named property will be considered by the State National Register Nominations Review Committee for nomination to the National Register of Historic Places on February 10, 2023 via Zoom. The National Register is the Federal Government's official list of historic properties worthy of preservation. Listing in the National Register provides recognition and assists in preserving our Nation's heritage. Listing of the property provides recognition of the community's historic importance and assures protective review of Federal projects that might adversely affect the character of the historic property. Listing in the National Register does not mean that limitations will be placed on the properties by the Federal government. Public visitation rights are not required of owners. The Federal government will not attach restrictive covenants to the properties or seek to acquire them.

Listing in the National Register results in:

- Consideration in the planning for Federal, federally licensed, and federally assisted projects. Section 106 of the National Historic Preservation Act of 1966 requires Federal agencies allow the Advisory Council on Historic Preservation an opportunity to comment on projects affecting historic properties listed in the National Register. For further information please refer to 36 CFR 800.
- Eligibility for Federal tax provisions. If a property is listed in the National Register, certain Federal tax provisions may apply. The Tax Reform Act of 1986 revised the historic preservation tax incentives authorized by Congress in the Tax Reform Act of 1976, the Revenue Act of 1978, the Tax Treatment Extension Act of 1980, the Economic Recovery Tax Act of 1981, and Tax Reform Act of 1984, and as of January 1, 1987, provides for a 20 percent investment tax credit with a full adjustment to basis for rehabilitating historic commercial, industrial, and rental residential buildings. The former 15 percent and 20 percent Investment Tax Credits (ITCs) for rehabilitation of older commercial buildings are combined into a single 10 percent ITC for commercial or industrial buildings built before 1936. The Tax Treatment Extension Act of 1980 provided Federal tax deductions for charitable contributions for conservation purposes of partial interests in historically important land areas or structures. Whether these provisions are advantageous to a property owner is dependent upon the particular circumstances of the property and the owner. Because tax aspects outlined above are complex, individuals should consult legal counsel or the appropriate local Internal Revenue Service office for assistance in determining the tax consequences of the above provisions. For further information please refer to 36 CFR 67 and Treasury Regulation Sections 1.48-12 (ITCs) and 1.170A-14 (charitable contributions).
- Consideration of historic values in the decision to issue a surface coal mining permit where coal is located, in accord with the Surface Mining and Control Act of 1977. For further information please refer to 30 CFR 700 et seq.

- Qualification for Federal and State grants for historic preservation when funds are available.
- Eligibility for State Tax Credits for rehabilitation. Properties listed on the National Register, eligible for listing on the National Register or Barns constructed before 1937 are eligible to apply for a 25 percent state tax credit for rehabilitation. The State Historic Preservation office must approve the rehabilitation work before an amount of tax credits will be reserved for your project. There are limited credits available each year, so let us know if you want the application information.

Owners of private properties nominated to the National Register of Historic Places have an opportunity to concur in or object to listing in accord with the National Historic Preservation Act and 36 CFR 60. Any owner or partial owner of private property who chooses to object to listing is required to submit to the State Historic Preservation Officer a statement of objection certifying that the party is the sole or partial owner of the private property and objects to the listing. Each owner or partial owner of private property has one vote regardless of what part of the property that party owns. If a majority of private property owners object, a property will not be listed; however, the State Historic Preservation Officer shall submit the nomination to the Keeper of the National Register for a determination of the eligibility of the property for listing in the National Register. If the property is then determined eligible for listing, although not formally listed, Federal agencies will be required to allow the Advisory Council on Historic Preservation an opportunity to comment before the agency may fund, license, or assist a project which will affect the property. If you choose to object to the listing of your property, the notarized objection must be submitted to the State Historical Society of Iowa, 600 E. Locust, Des Moines, IA 50313 not more than 75 days from the date of this notice.

If you wish to comment on the nomination of the property to the National Register, please send your comments to the State Historical Society of Iowa before the State Nomination Review Committee meets on February 10, 2023. A copy of information on the National Register and the Federal tax provisions are available from the above address upon request.

You are invited to attend the State Nomination Review Committee meeting at which the nomination will be considered. The meeting will take place via Zoom. A meeting agenda is enclosed. If you would like to review the nomination, please visit the following link to download a copy of the nomination:  
[https://drive.google.com/drive/folders/1YztKSs-OcL0pmSMqqV4ZPYbN12QSY2OP?usp=share\\_link](https://drive.google.com/drive/folders/1YztKSs-OcL0pmSMqqV4ZPYbN12QSY2OP?usp=share_link)

Questions can be directed to [nationalregister@iowa.gov](mailto:nationalregister@iowa.gov).

Sincerely,

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**National Register of Historic Places**  
**State Historic Preservation Office of Iowa**  
[nationalregister@iowa.gov](mailto:nationalregister@iowa.gov) | 515.281.5111 | [iowaculture.gov](http://iowaculture.gov)



**STATE NATIONAL REGISTER NOMINATION REVIEW COMMITTEE**  
**February 10, 2023**

The meeting of the State Nomination Review Committee (SNRC) will be held virtually over Zoom. If you wish to attend, please request an invite no later than 4:00 pm Wednesday, February 8, 2023, here:  
<https://forms.gle/u5HWTQZeBj3WhHTC9>.

If you are representing a particular nominated property, please **arrive at least 30 minutes before** your nomination is scheduled on the agenda. Due to the unpredictable length of time it takes to review each nomination the times on the agenda may fluctuate.

- Presenter time is limited to **5 minutes**. This time is provided so preparers may describe the PowerPoint slides as they are shown. Following that brief slide presentation, the committee chair will invite the committee members to discuss the nomination. The preparer should be prepared to answer any questions regarding the nomination.
- A copy of the minutes from the meeting will be emailed to each preparer after the meeting notes and ballots are compiled. The minutes will outline any revisions the SNRC members requested for each nomination. Revised nominations can be submitted after the minutes have been distributed and all revisions completed by the preparer following the instructions that will be provided.

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**AGENDA**

**OLD BUSINESS**

9:00 Minutes and Announcements

**NOMINATIONS**

9:15 Council Bluffs Federal Building and Post Office, Council Bluffs – Catlin Benton  
9:30 Cranford Apartment Building, Ames – Alexa McDowell  
9:45 Decorah Hospital, Decorah – Rebecca Conard & Jan Olive Full  
10:00 Break (15 minutes)  
10:15 First National Bank of Pomeroy, Pomeroy – Libby Wielenga  
10:30 Friedl Bakery, Waterloo – Jennifer James  
10:45 International Harvester Truck Sales and Service Station, Davenport – William Page  
11:00 Adjourn SNRC





**STATE NATIONAL REGISTER NOMINATION REVIEW COMMITTEE**  
**February 10, 2023**

The meeting of the State Nomination Review Committee (SNRC) will be held virtually over Zoom. If you wish to attend, please request an invite no later than 4:00 pm Wednesday, February 8, 2023, here:  
<https://forms.gle/u5HWTQZeBj3WhHTC9>.

If you are representing a particular nominated property, please **arrive at least 30 minutes before** your nomination is scheduled on the agenda. Due to the unpredictable length of time it takes to review each nomination the times on the agenda may fluctuate.

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City of Davenport  
Historic Preservation Commission

Department: Development & Neighborhood Services  
Contact Info: Matt Werderitch 563.888.2221

**Date**  
**1/10/2023**

Subject:

Please Note: The February 14th and March 14th Historic Preservation Commission meetings will be held in the Davenport Police Department Community Room, 416 N. Harrison Street.

REVIEWERS:

Department	Reviewer	Action	Date
Community Planning & Economic Development	Werderitch, Matt	Approved	1/6/2023 - 2:26 PM