

PLAN AND ZONING COMMISSION MEETING

CITY OF DAVENPORT, IOWA

TUESDAY, MARCH 6, 2018; 5:00 PM

COUNCIL CHAMBERS

COMBINED PUBLIC HEARING AND REGULAR MEETING AGENDAS

PUBLIC HEARING AGENDA

A.

I. Next Public Hearing

- A. Tuesday, March 20, 2018 at 5:00 P.M. in the Council Chambers of Davenport City Hall - 226 West 4th Street.

REGULAR MEETING AGENDA

I. Roll Call

II. Report of the City Council Authority

- A. Summary of Commission cases approved by City Council.

III. Secretary's Report

- A. Minutes of February 20, 2018 Commission meeting for approval.

IV. Report of the Comprehensive Plan Committee

V. Zoning Activity

A. Old Business

B. New Business

- i. Case No. REZ18-01: Request of Dave Meyer on behalf of Tom Swanwick dba Forest View LLC for a rezoning (map amendment) on 1.19 acres of property known as Lot 2 of Ryde High 2nd Addition located on the south side of East 46th Street between Grand and Tremont Avenues. The rezoning is from "C-2" General Commercial District to R-6M Planned Unit Development to construct attached townhouses on individual lots. Ten units are proposed. [Ward 7]

VI. Subdivision Activity

A. Old Business

B. New Business

- i. Case No. F18-02: Final plat of Bett's Corporation 2nd Addition on 1.21 acres, more or less, located abutting to the east of 741 West 53rd Street containing

one (1) lots. The property is zoned "R-1" Low Density Dwelling District. [Ward 7]

VII. Future Business

- A. Case No. CP18-01: Adoption of the Elmore Corners Area Plan as an Element of the City of Davenport Comprehensive Plan [6th Ward].
- B. Case No. ORD18-01: Proposed Amendment to Title 17.41 of Davenport Municipal Code (Zoning) creating an Elmore Corners Overlay District and establishing design standards for development within the district [6th Ward]
- C. Case No. P18-02: Preliminary plat Seng Meadows located west of Northwest Boulevard and north of 46th Street containing 58 residential lots and one outlot. [Ward 7]

VIII. Communications

- A. Time open for citizens wishing to address the Commission on matters not on the established agenda

IX. Other Business

X. Adjourn

- A. Note: Pursuant to §17.60.030 and §2.64.120 of the Davenport City Code the Commission is required to act on this item within 30 days unless the petitioner waives this requirement. Pursuant to the city code if the Commission does not act and report on this item within 30 days' time this agenda item is to be construed as approved by the Commission.

Note: The Plan and Zoning Commission meeting is not a public hearing. It is time for the commission to discuss the issue(s) with City staff and if questions rise, with the developer.

A rezoning or ordinance text amendment has a second public hearing before the City Council at its Committee of the Whole meeting. Notification of that meeting will be sent to surrounding owners following the Plan and Zoning Commission meeting.

Next Public Hearing/Regular Plan & Zoning Meeting:

Tuesday, March 20, 2017 at 5:00 P.M. in the Council Chambers of Davenport City Hall 226 West 4th St

City of Davenport
Plan and Zoning Commission

Department: Community Planning & Economic Development
Contact Info: Matt Flynn 888-2286

Date
3/6/2018

Subject:

Staff Workflow Reviewers

REVIEWERS:

Department	Reviewer	Action	Date
City Clerk	Wille, Wayne	Approved	3/2/2018 - 11:15 AM

City of Davenport
Plan and Zoning Commission

Department: Community Planning & Economic Development
Contact Info: Matt Flynn 888-2286

Date
3/6/2018

Subject:

Tuesday, March 20, 2018 at 5:00 P.M. in the Council Chambers of Davenport City Hall - 226 West 4th Street.

Staff Workflow Reviewers

REVIEWERS:

Department	Reviewer	Action	Date
City Clerk	Wille, Wayne	Approved	3/2/2018 - 11:06 AM

City of Davenport
Plan and Zoning Commission

Department: Community Planning & Economic Development
Contact Info: Matt Flynn 888-2286

Date
3/6/2018

Subject:
Summary of Commission cases approved by City Council.

ATTACHMENTS:

Type	Description
▢ Backup Material	Report of Council activity

Staff Workflow Reviewers

REVIEWERS:

Department	Reviewer	Action	Date
City Clerk	Wille, Wayne	Approved	3/2/2018 - 11:07 AM

Report of City Council Activity

February 28, 2018 Meeting –

City of Davenport
Plan and Zoning Commission

Department: Community Planning & Economic Development
Contact Info: Matt Flynn 888-2286

Date
3/6/2018

Subject:
Minutes of February 20, 2018 Commission meeting for approval.

ATTACHMENTS:

Type	Description
▯ Backup Material	02-20-18 Minutes

Staff Workflow Reviewers

REVIEWERS:

Department	Reviewer	Action	Date
City Clerk	Wille, Wayne	Approved	3/2/2018 - 11:07 AM

**CITY PLAN AND ZONING COMMISSION
CITY OF DAVENPORT, IOWA**

**TUESDAY FEBRUARY 20, 2018 • 5:00 PM
COUNCIL CHAMBERS – DAVENPORT CITY HALL
226 W 4TH STREET DAVENPORT, IA**

MINUTES

PUBLIC HEARING AGENDA

The public hearing was opened at 5:00 p.m. and the following hearing was held and closed at 5:05 p.m.

OLD BUSINESS –

NEW BUSINESS –

- Case No. REZ18-01: Request of Dave Meyer on behalf of Tom Swanwick dba Forest View LLC for a rezoning (map amendment) on 1.19 acres of property known as Lot 2 of Ryde High 2nd Addition located on the south side of East 46th Street between Grand and Tremont Avenues. The rezoning is from “C-2” General Commercial District to R-6M Planned Unit Development to construct attached townhouses on individual lots. Ten units are proposed. [7th Ward]

Next Public Hearing:

Tuesday, March 06, 2018 at 5:00 P.M. in the Council Chambers of Davenport City Hall – 226 West 4th Street.

REGULAR MEETING AGENDA

The regular meeting was called to order at 5:06 p.m.

I. Roll Call of the Membership

Present: Connell, Inghram, Kelling, Lammers, Martinez, Medd, and Quinn

Excused: Hepner, Maness, Reinartz and Tallman

Staff: Flynn, Rusnak, Leabhart, Wille and Heyer

II. Report of the City Council Activity

III. Secretary’s Report The minutes for the February 06, 2018 meeting were approved

IV. Report of the Comprehensive Plan Committee

Flynn indicated that staff is reviewing the first draft of the zoning ordinance and should be sent to the advisory committee in 2-3 weeks and then a committee meeting will be scheduled.

V. Zoning Activity

A. Old Business –

B. New Business -

1. Case No. FDP18-01: Request of John Crump dba Woodspring LLC for a PDD Final Development Plan located north of East 53rd Street and west of Elmore Avenue (Lot 1 of Lakehurst Commercial Park Third Addition) on 2.28 acres, more or less to develop a Woodspring Suites Hotel. [6th Ward]

Findings:

- The proposed Final Development Plan would achieve consistency with adopted ORD1994-625; and
- The proposal would facilitate infill development within an area containing existing public utilities.

Staff recommends the Plan and Zoning Commission forward case FDP18-01 to the City Council with a recommendation for approval subject to the following condition:

1. That the PDD Zoning Requirements table be updated to the following:
Front Yard: 25 Ft. Minimum Building Setback.
Side Yard: 10 Ft. Minimum Building Setback.
Year Yard: 0 Ft. Minimum Building Setback.

A motion by Connell, seconded by Quinn, to forward case FDP18-01 to the City Council with a recommendation for approval subject to the above stated condition was unanimously approved: 6-yes, 0-no and no-abstentions.

VI. Subdivision Activity

A. Old Business –

B. New Business –

1. Case No. P18-01: Preliminary plat of Reed's Retreat located west of Wisconsin Avenue and north of Telegraph Road, containing seven (7) lots on 62.86 acres, more or less, The property is zoned both "R-1" Low Density Dwelling District and "A-1" Agricultural District.[1st Ward]

Findings

- The plat conforms to Davenport+2035.
- With the below corrections and additions the preliminary plat will conform to the minimum standards of Chapter 16.12 entitle 'Preliminary Plats'.
- The plat proposes development for the southwest area.

Staff recommends the City Plan and Zoning Commission forward Case No. P18-01 the preliminary plat of Reed's Retreat to the City Council for approval subject to the following conditions (note these are ordinance requirements):

1. Existing buildings be shown.
2. Existing water courses are shown.
3. Existing tree masses are shown.
4. The size and location of existing sanitary sewers are shown.
5. The size and location of all existing culverts and storm sewers are shown.
6. The size and location of the nearest existing water mains are shown.
7. All proposed and existing easements are shown.
8. All known underground structures are shown.
9. Existing contours are shown at intervals of 5' or less.
10. Names and boundaries of existing adjacent subdivisions are shown.
11. A vicinity sketch is shown.

12. Zoning is shown.
13. The surface types of existing streets are shown.
14. That the proposed ROW widths for Wisconsin Avenue and Telegraph Road are shown as 45' from existing centerline.

A motion by Connell, seconded by Quinn, to forward case P18-01 to the City Council with a recommendation for approval subject to the above stated condition was unanimously approved: 6-yes, 0-no and no-abstentions.

2. Case No. F18-01: Final plat of Reed's Retreat located west of Wisconsin Avenue and north of Telegraph Road, containing seven (7) lots on 62.86 acres, more or less, The property is zoned both "R-1" Low Density Dwelling District and "A-1" Agricultural District.[1st Ward]

Findings

- The plat conforms to Davenport+2035.
- With the below corrections and additions the preliminary plat will conform to the minimum standards of Chapter 16.12 entitle 'Preliminary Plats'.
- The plat proposes development for the southwest area.

Staff recommends the City Plan and Zoning Commission forward Case No. F18-01 the final plat of Reed's Retreat to the City Council for approval subject to the following conditions:

(These are ordinance conditions)

1. The surveyor signs the plat.
2. The utility companies sign the plat when their easement needs have been met and all easement are shown and tied to the plat by bearing/dimension.
3. The Owner/Developer's name and address are shown.

(These are conditions specific to this plat)

4. A 15-foot wide utility easement is dedicated along all street frontages including Wisconsin Avenue.
5. Dimensions are shown for the existing sanitary sewer easement and that the easement is tied to the plat in a manner that it can be located in reference to property corners.
6. An additional 12-feet of right-of-way is dedicated along Wisconsin Avenue bringing the total to 45-feet from existing right-of-way/easement centerline.
7. Additional right-of-way is dedicated along Telegraph Road as needed to bring the total width to 45-feet from existing right-of-way/easement centerline.
8. That Lot 5 be redesigned to accommodate a sufficient buildable area.
9. Future platting of Lot 1 will require the submission of a new preliminary plat and the possible dedication of future street(s) right-of-way.

A motion by Connell, seconded by Quinn, to forward case F18-01 to the City Council with a recommendation for approval subject to the above stated condition was unanimously approved: 6-yes, 0-no and no-abstentions.

VII. Other Business –

VIII. Future Business – Preview of items for the March 06th public hearing and/or regular meeting (*note-not all items to be heard may be listed*):

- Case No. F18-02: Final plat of Bett's Corporation 2nd Addition located along the south side of West 53rd Street east of Brown Street containing one residential lot on 1.21 acres, more or less. [7th Ward]

IX. Communications (Time open for citizens wishing to address the Commission on matters not on the established agenda)

X. Adjourn – the meeting was adjourned at approximately 5:22 p.m.

- *Note: Pursuant to §17.60.030 and §2.64.120 of the Davenport City Code the Commission is required to act on this item within 30 days unless the petitioner waives this requirement. Pursuant to the city code if the Commission does not act and report on this item within 30 days' time this agenda item is to be construed as approved by the Commission.*
- *Note: The Plan and Zoning Commission meeting is not a public hearing. It is time for the commission to discuss the issue(s) with City staff and if questions rise, with the developer.*
- *A rezoning or ordinance text amendment has a second public hearing before the City Council at its Committee of the Whole meeting. Notification of that meeting will be sent to surrounding owners following the Plan and Zoning Commission meeting.*

Next Public Hearing/Regular Plan & Zoning Meeting:

Tuesday, March 06, 2017 at 5:00 P.M. in the Council Chambers of Davenport City Hall 226 West 4th Street.

Report of City Council Activity

February 14, 2018 Meeting – No Commission activity was considered by Council.

City of Davenport
Plan and Zoning Commission

Department: Community Planning & Economic Development
Contact Info: Matt Flynn 888-2286

Date
3/6/2018

Subject:

Case No. REZ18-01: Request of Dave Meyer on behalf of Tom Swanwick dba Forest View LLC for a rezoning (map amendment) on 1.19 acres of property known as Lot 2 of Ryde High 2nd Addition located on the south side of East 46th Street between Grand and Tremont Avenues. The rezoning is from "C-2" General Commercial District to R-6M Planned Unit Development to construct attached townhouses on individual lots. Ten units are proposed. [Ward 7]

Recommendation:

ATTACHMENTS:

Type	Description
▣ Backup Material	REZ18-01 Staff report

Staff Workflow Reviewers

REVIEWERS:

Department	Reviewer	Action	Date
City Clerk	Wille, Wayne	Approved	3/2/2018 - 11:07 AM



City of Davenport
Community Planning & Economic Development Department
FINAL STAFF REPORT

Meeting Date: March 06, 2018
Request: C-2 General Commercial to R-6M Planned Unit Development
Address: Lot 3 Ryde High 2nd Add'n – S of E 46th St between Grand & Tremont Aves
Case No.: REZ18-01
Applicant: David Meyer on behalf of Tom Swanwick dba Forest View LLC

Recommendation:

Staff recommends the City Plan and Zoning Commission accept the findings and forward Case No. REZ18-01 to the City Council for approval subject to the listed condition.

Introduction:

Request of Tom Swanwick dba Forest View LLC for a rezoning (map amendment) on 1.19 acres of property known as Lot 3 of Ryde High 2nd Addition located on the south side of East 46th Street between Grand and Tremont Avenues. The rezoning is from "C-2" General Commercial District to R-6M Planned Unit Development to construct attached townhouses on individual lots. Ten units are proposed. [Ward 7]

AREA CHARACTERISTICS:

Aerial

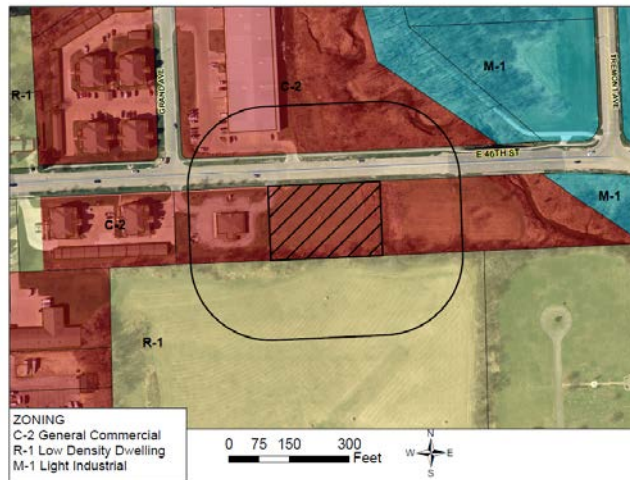


2014 Aerial Photo

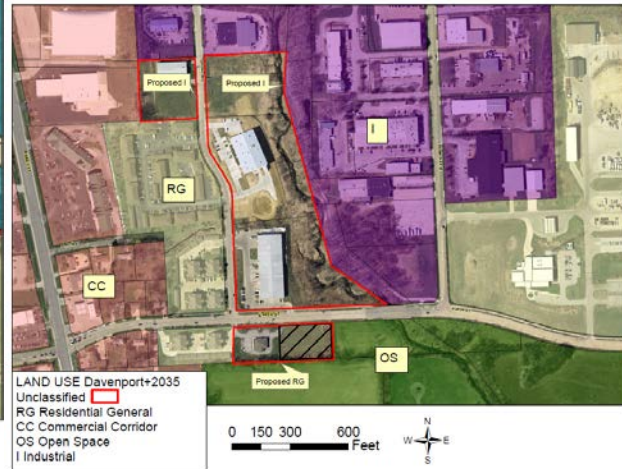
0 100 200 400
Feet



Zoning Map



Land Use



Background: **Comprehensive Plan:**

Within Urban Service Boundary +2035: Yes

Future Land Use Designation: The property being rezoned, the abutting property to the west and the area north of 46th Street (outlined in red) was missed during the development of the Davenport+2035 Land Use map. Staff would suggest that this be rectified as part of this rezoning.

Staff proposes the area south of 46th Street be classified as Residential General (RG) and the area north of 46th Street be classified as Industrial (I).

Residential General (RG) - Designates neighborhoods that are mostly residential but include, or are within one-half mile (walking distance) of scattered neighborhood-compatible commercial services, as well as other neighborhood uses like schools, churches, corner stores, etc. generally oriented along Urban Corridors (UC). Neighborhoods are typically designated as a whole. Existing neighborhoods are anticipated to maintain their existing characteristics in terms of land use mix and density, with the exception along edges and transition areas, where higher intensity may be considered.

Relevant Goals to be considered in this Case:

Identify and reserve land for current and future development – *encourage a full range of development.*

Zoning:

The property is currently zoned "C-2" General Commercial District. The C-2 designation would allow for multiple family (apartments) similar to those developed to the west and northwest. The Planned Unit Development would allow for the attached town homes on individual lots (fee simple title).

Technical Review:

Streets. The property is located along East 46th Street east of Brady Street / Welcome Way couplet. East 46th St dead-ends east at the railroad to the east. There is a separation requirement for the driveways of 203 feet. The topography may not allow a full 230 feet but discussion with the City's Development and Traffic Engineers should be held earlier than later.

Storm Water. Stormwater infrastructure is located in East 46th Street.

Sanitary Sewer. Sanitary sewer service is located along the south side of East 46th Street (8-inch line).

Other Utilities. This is an urban area and normal utility services are available.

Emergency Services. The property is located approximately 1-1/2 miles from Fire Stations No. 3 at 3506 Harrison Street and 2-miles from Fire Station No. 8 at 2820 E 53rd Street.

Parks/Open Space. The proposed rezoning does not impact any existing or planned parks or public open spaces.

Public Input:

A neighborhood meeting was held February 13th at the Public Works Center. No one from the notice area attended. The developer presented staff with the proposed building elevations (not complete) and floor plans.

Discussion:

Zoning

The existing zoning is currently C-2 General Commercial. The Planned Unit Development (PUD) is a residential enhancement, therefore the residential downzoning. With regard to multiple family development the C-2 and R-6M zoning allow the same density.

The rezoning to the R-6M Planned Unit Development is to facilitate town home development with each unit on its own lot (fee simple title of land with each unit). The existing commercial zoning allows for multiple family (apartments) but not attached town home units on individual lots.

A final development plan and subdivision will also be required prior to the property being developed.

Staff Recommendation:

Findings:

That the proposed development fits with the surrounding developments

Recommendation:

Staff recommends the City Plan and Zoning Commission accepts the findings and forward Case No. REZ18-01 to the City Council for approval subject to the following condition:

1. That the Proposed Land Use Map for Davenport+2035 be corrected to include this area as Residential General as shown in Exhibit "A".
2. That the center access point for 46th Street be moved eastward as far as practical (separation should be 230 feet) though may be less if approved by the Traffic Engineer.

Prepared by:

Wayne Wille, CFM - Planner II
Community Planning Division

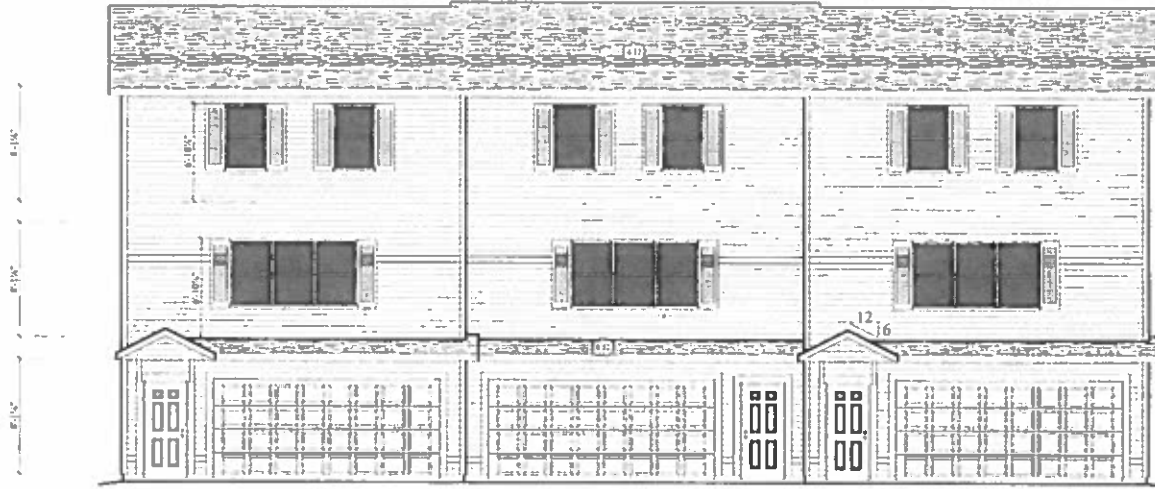
Age Group	Total (%)	Male (%)	Female (%)	Unknown (%)
18-24	18	10	8	2
25-34	15	8	7	0
35-44	12	6	6	0
45-54	10	5	5	0
55-64	8	4	4	0
65+	5	2	3	0

1 SCALE: P = 257



4111 EAST 90th STREET
DAVENPORT, IOWA 52807
PHONE NUMBER: (563) 399 - 1348

VINCE 17203 - 82

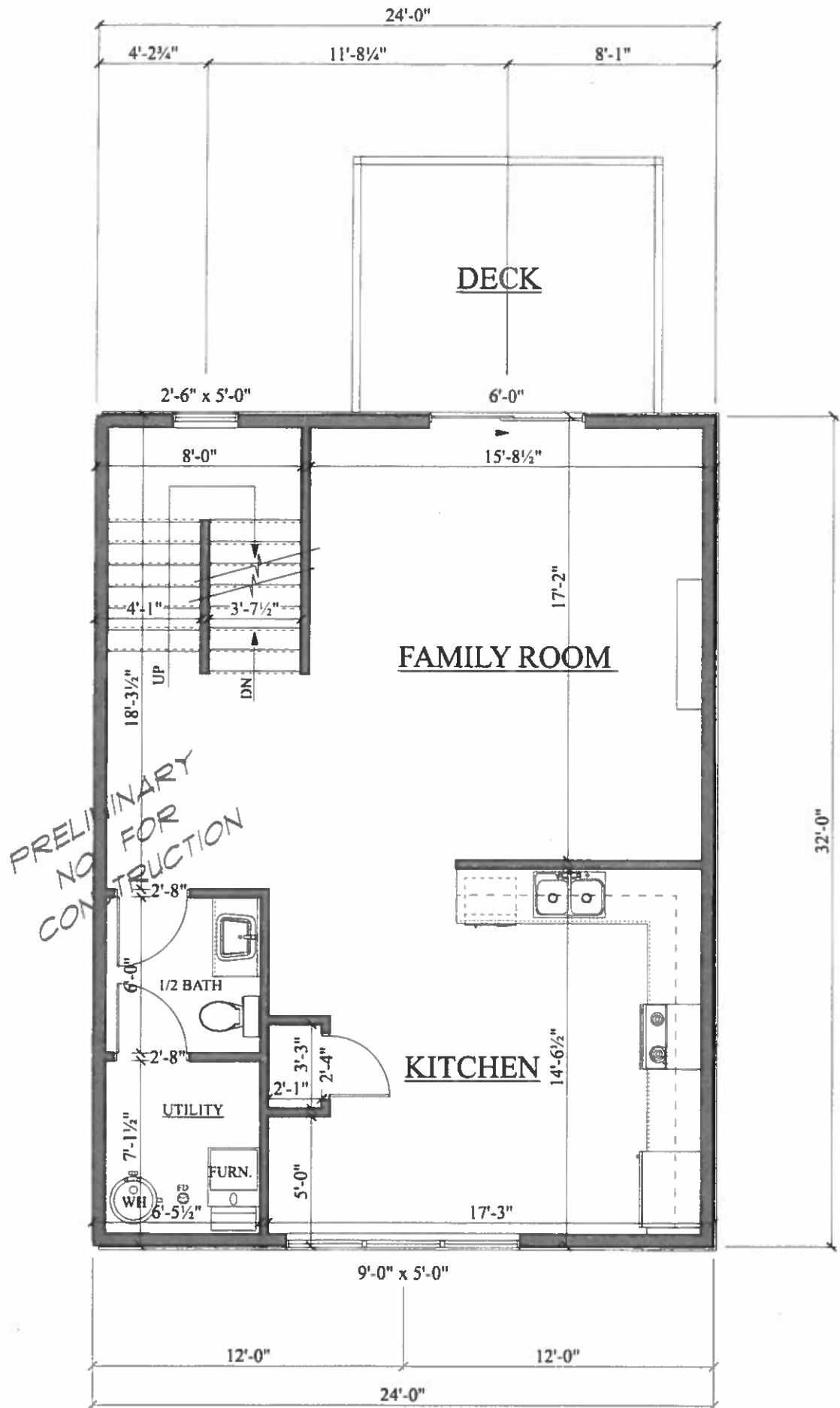


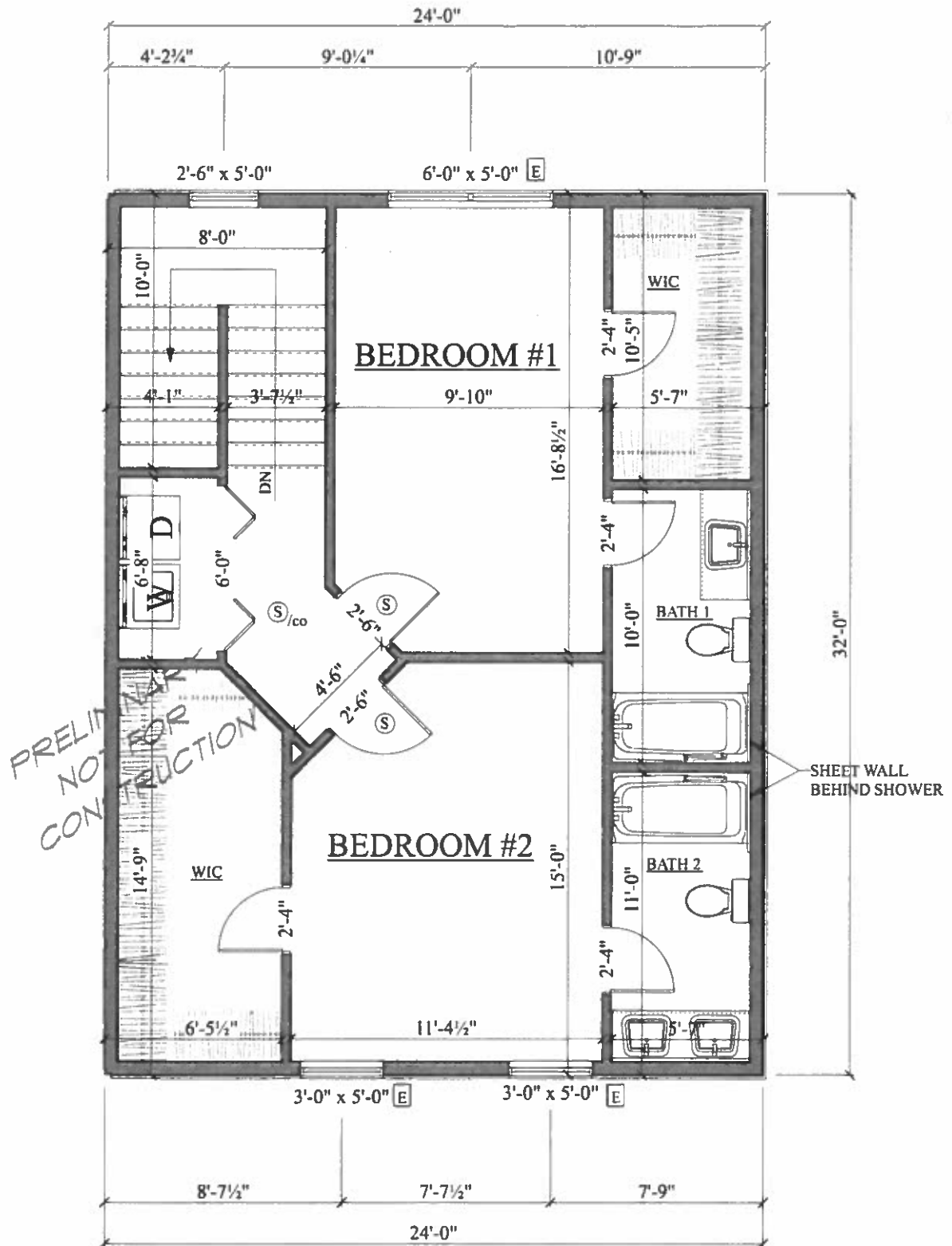
FRONT ELEVATION

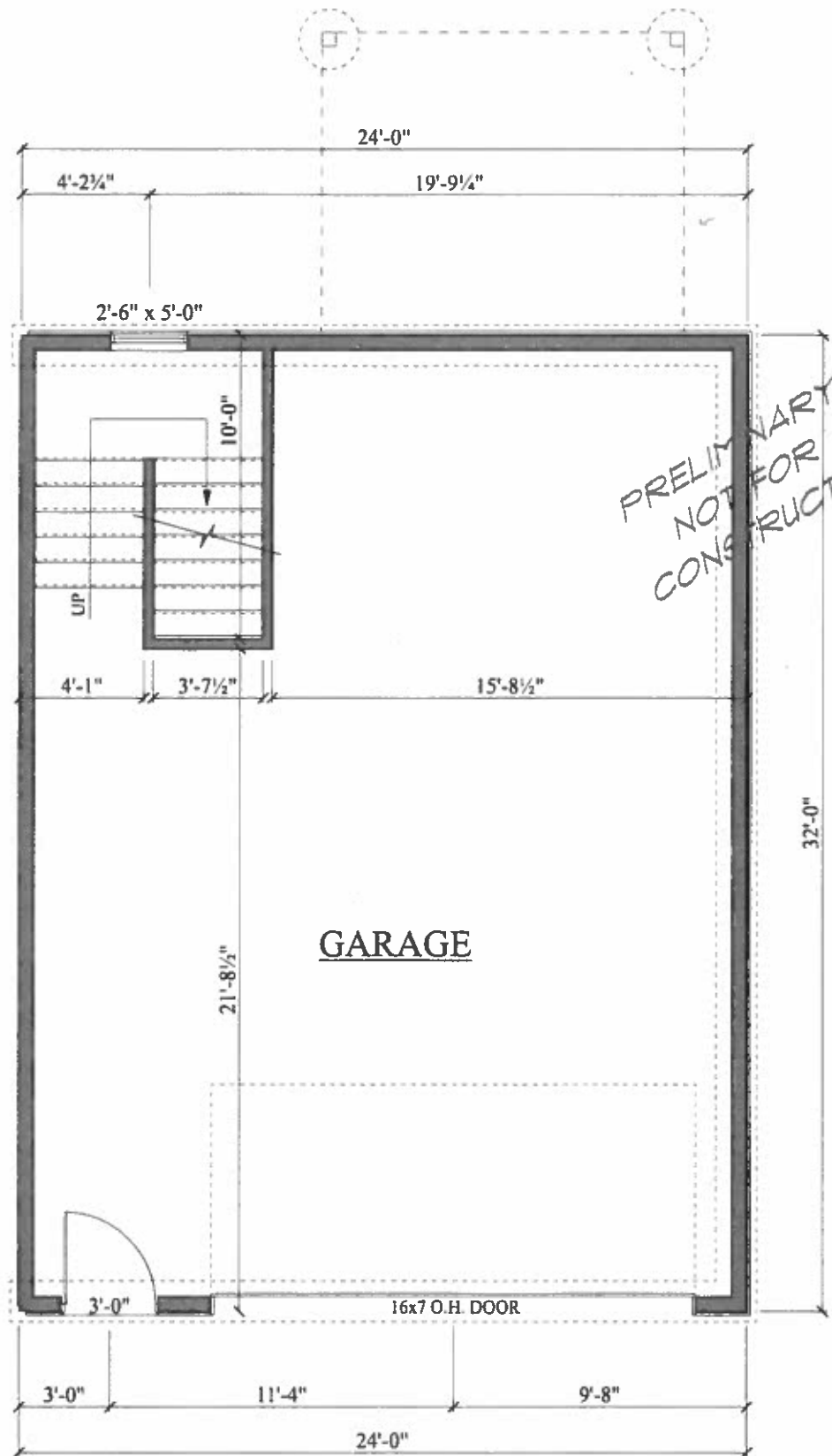


REAR ELEVATION

SHEET # 1	JOB # 1972-1017 SCALE 1/4" = 1'-0" February 28, 2015	SUBJECT NAME SWANEY WEST 46TH CONDOS	SHEET DESCRIPTION Front & Rear Elevations
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City of Davenport
Plan and Zoning Commission

Department: Community Planning & Economic Development
Contact Info: Matt Flynn 888-2286

Date
3/6/2018

Subject:

Case No. F18-02: Final plat of Bett's Corporation 2nd Addition on 1.21 acres, more or less, located abutting to the east of 741 West 53rd Street containing one (1) lots. The property is zoned "R-1" Low Density Dwelling District. [Ward 7]

ATTACHMENTS:

Type	Description
▢ Backup Material	F18-02 packet

Staff Workflow Reviewers

REVIEWERS:

Department	Reviewer	Action	Date
City Clerk	Wille, Wayne	Approved	3/2/2018 - 11:07 AM



City of Davenport
Community Planning & Economic Development Department
FINAL STAFF REPORT

PLAN AND ZONING COMMISSION

Preview Date: MARCH 06, 2018
Request: F18-02 Final Plat – Bett's Corporation 2nd Addition
Address: Abutting to the East of 741 West 53rd Street
Applicant: Charles Easton

INTRODUCTION

Request of Charles Easton for a final plat on 1.21 acres, more or less, located abutting to the east of 741 West 53rd Street containing one (1) lots. The property is zoned "R-1" Low Density Dwelling District. There is a concurrent preliminary plat. [Ward 7]

Recommendation:

Staff recommends the plat be resubmitted as a two lot subdivision incorporating the remainder of the unplatted land.

Aerial Photo:



0 100 200 400
Feet



Zoning (R-1 Low Density Dwelling)



Land Use 2035 (Residential General)



[illegible]

Comprehensive Plan:

Residential General (RG) - Designates neighborhoods that are mostly residential but include, or are within one-half mile (walking distance) of scattered neighborhood-compatible commercial services, as well as other neighborhood uses like schools, churches, corner stores, etc generally oriented along Urban Corridors (UC). Neighborhoods are typically designated as a whole. Existing neighborhoods are anticipated to maintain their existing characteristics in terms of land use mix and density, with the exception along edges and transition areas, where higher intensity may be considered.

1. *Strengthen the existing built environment.*
 - b. *Reduce the number of underoccupied, abandoned, or vacant buildings / properties through adaptive reuse and infill.*
2. *Identify and reserve land for future development*

Technical Review:

Streets. No new streets are proposed with this request.

Storm Water. The property is surrounded by developed area which contains stormwater infrastructure. Drainage on this lot is overland to the east to Candlelight Creek.

Sanitary Sewer. Sanitary sewer service is located along the east property line.

Other Utilities. This is an urban area and normal utility services are available.

Parks/Open Space. The proposed plat does not impact any existing or planned parks or public open spaces.

PUBLIC INPUT This is a subdivision plat. No notification is required.

DISCUSSION

The apparent reason for the plat is for the owner at 741 West 53rd Street (abutting to the west of the plat) to obtain additional land area.

Since this plat divides an existing parcel the remaining land should be made part of this plat, in essence a two (2) lot plat. Staff has notified the engineer/surveyor.

16.08.010 Submission of subdivision plat - Required.

Every owner of any tract or parcel of land within the city, within two miles of the limits thereof, who may hereafter subdivide the same into two or more lots, sites of other divisions of land for the purpose, whether immediate or future, of creating an addition to the city, or part thereof, or suburban lots, shall submit a subdivision plat to the city plan and zoning commission in accordance with this chapter.

The Staff has allowed a one-lot plat typically in agricultural areas where the residence (farmstead) has been rezoned and the remaining farm ground (still agriculturally zoned) comprises the quarter-quarter section (38 acres). In this case the entire property is zoned residential and comprises approximately 7.9 acres.

A significant drainage way is located along the east property line of this plat. Section 13.34.160 of the Davenport City Code entitled "Stormwater Management – Easements" states:

B. Whenever any stream or water course is located in an area that is being subdivided, the subdivider shall dedicate a public right-of-way or drainage easement conforming substantially with the lines of such stream or water course and shall include such additional area adjoining both edges of such stream or water course that has been affected by damaging flood waters and/or inundated by the one hundred-year flood waters, as determined by the city engineer. This easement area shall also contain a minimum thirty foot vegetated buffer area on both sides of the stream as measured landward horizontally on a line perpendicular to a vertical line marking the top of the existing banks of the stream or drainage way. This buffer shall be planted with vegetation native to the mid-west region of the United States and maintained as a prairie as outlined in the Davenport Stormwater Manual.

C. Unless deeded to the city of Davenport, maintenance of the stream, streambanks and easement areas described in Section 13.34.160B shall be the responsibility of the landowners upon which the stream is located. This maintenance responsibility shall be clearly defined in a recorded document. The subdivider shall also provide reasonable public easements for access. Procedures for transfer of the property described in Section 13.34.160B to the city for maintenance or ownership are described in the Davenport Stormwater Manual.

D. It shall be noted on the final plat, "Owners of lots on which a drainage easement has been established as a stormwater passageway shall maintain said easement as a lawn, planted in grass and free of structures, fences, fill, bushes, trees, shrubs, or other landscaping that would impede the flow of water" and "Owners of lots on which a stream buffer is required shall maintain said buffer as a native planting area, not allowing vegetation to be cut to a height of less than nine-inches without authorization of the city of Davenport natural resources division." In the event that the area established as a drainage easement is reshaped or otherwise restricted for use as a drainage easement, the city will cause the restrictions to be removed at the expense of the parties causing the restriction.

STAFF RECOMMENDATION

Findings:

The plat as proposed does not conform to the requirements of the Title 16 "Subdivisions".

Recommendation:

There are three options for the Commission:

- a. Forward the plat to the City Council for approval subject to the following condition:
 1. That the plat be modified to incorporate the remaining portion of the parcel as a two-lot subdivision.
 2. The plat be tied to two existing lot corners in an existing subdivision (only one corner is noted).
- b. Forward the plat to the City Council for approval as proposed
- c. Return the plat to be resubmitted as a two lot subdivision incorporating the remainder of the unplatted land (no additional fee will be charged).

Prepared by:

Wayne Wille, CFM – Planner II
Community Planning Division

FINAL PLAT
BETTS CORPORATION 2ND ADDITION

To the City of Davenport, Iowa,
Being part of the Southwest Quarter
of Section 11, Township 78 North,
Range 3 East of the 5th Principal
Meridian, Scott County, Iowa.

APPROVED BY:
CITY OF DAVENPORT, IOWA

BY: _____

DATE: _____ ATTEST: _____

CITY PLAN & ZONE COMMISSION

BY: _____ DATE: _____

MEDIACOM

DATE: _____

IOWA - AMERICAN WATER COMPANY

DATE: _____

CENTURY LINK

DATE: _____

MIDAMERICAN ENERGY
APPROVED SUBJECT TO ENCUMBRANCES OF RECORD BY MIDAMERICAN

NOTES:
SIDEWALKS SHALL BE CONSTRUCTED ALONG STREET RIGHT
OF WAYS WHEN SO ORDERED BY THE CITY.

NO WATER DETENTION REQUIREMENTS ARE PROPOSED
WITH THIS SUBDIVISION.

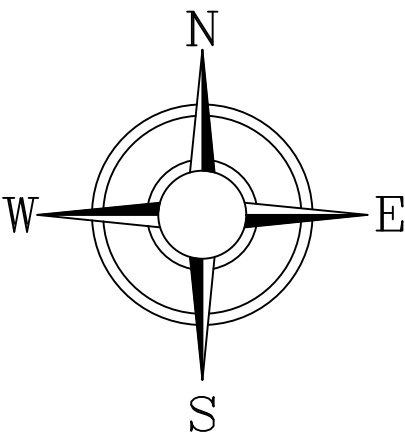
DIMENSIONS ALONG CURVES ARE ARC DISTANCES.

MEASUREMENTS ARE SHOWN IN FEET AND DECIMAL PARTS
THEREOF.

ALL PUBLIC UTILITIES SHALL BE LOCATED WITHIN EASEMENTS
OR PUBLIC RIGHT-OF-WAY.

THIS SURVEY IS NOT VALID WITHOUT THE SURVEYOR'S
ORIGINAL SIGNATURE AND SEAL.

BLANKET UNDERGROUND EASEMENTS GRANTED FOR
SEWER, WATER, GAS, ELECTRIC, TELEPHONE, AND CABLE T.V.
SERVICES TO INDIVIDUAL STRUCTURES WITHIN THE LOT
WHERE THE STRUCTURE IS LOCATED.

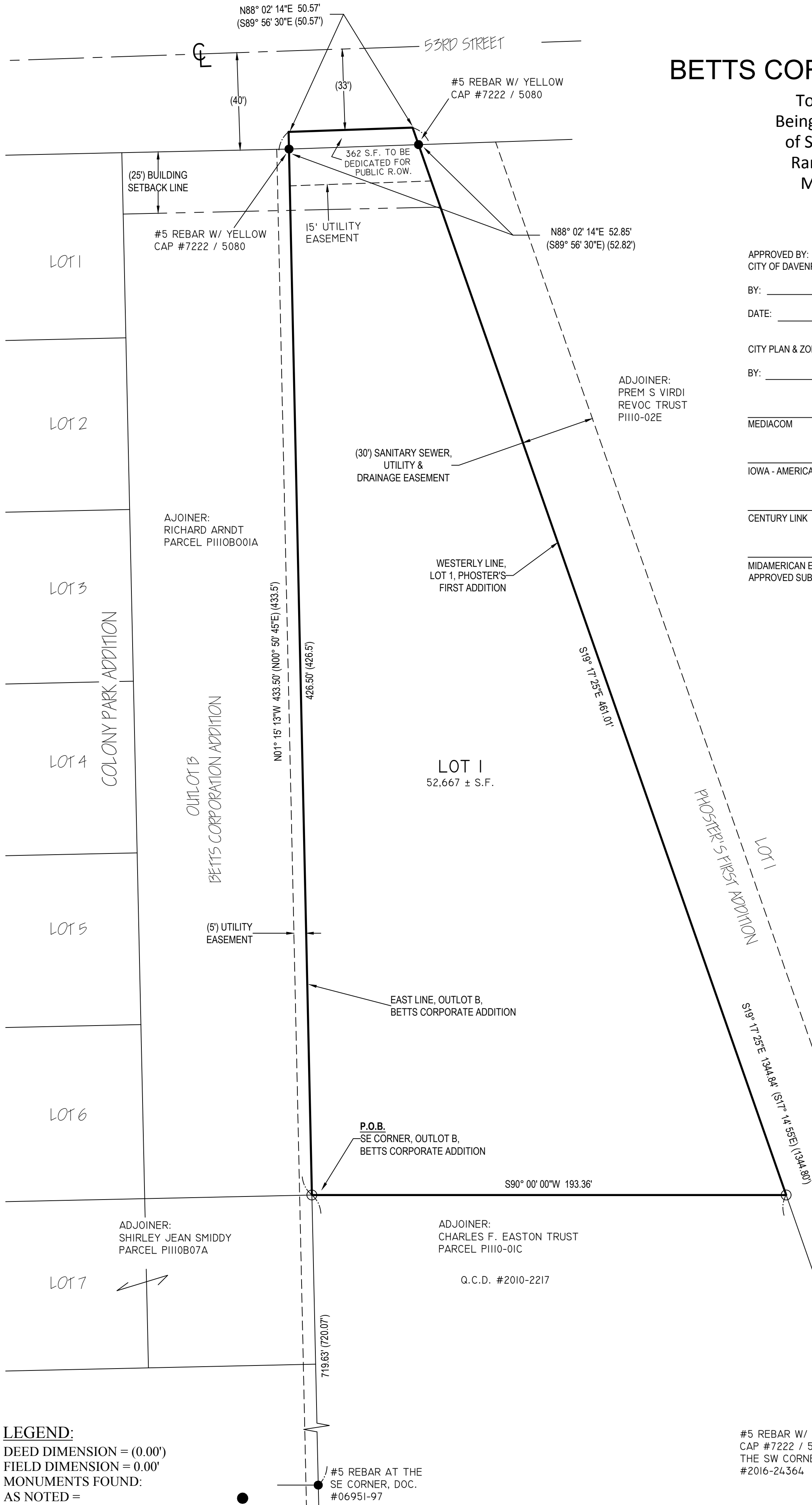


THE MEASURED BEARINGS SHOWN HEREON ARE BASED
ON THE IOWA REGIONAL COORDINATE SYSTEM, ZONE 11
NAD '83 (2011) EPOCH 2010.00



I hereby certify that this land surveying document was prepared and
the related survey work was performed by me or under my direct
personal supervision and that I am a duly licensed Land Surveyor
under the laws of the State of Iowa.

MICHAEL D. RICHMOND
Iowa License Number: 23503
My license renewal date is December 31, 2019
Pages or sheets covered by this seal: 1



LEGEND:
DEED DIMENSION = (0.00')
FIELD DIMENSION = 0.00'
MONUMENTS FOUND:
AS NOTED =
MONUMENTS SET:
#5 REBAR W/ YELLOW CAP #23503 = ○
BOUNDARY LINE = _____
ROAD CENTER LINE = _____
EASEMENT LINE = _____
SETBACK LINE = _____
SECTION LINE = _____

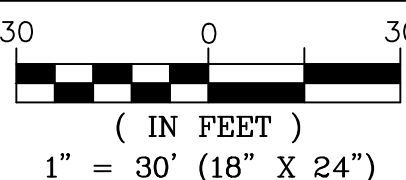
1. Area of Subdivision-
Total: 1.217 Total Acres +/-
0.008 in R.O.W. +/-
1.209 Net Acres +/-
2. Engineer:
Townsend Engineering
2224 East 12th Street
Davenport, Iowa 52803
Ph: (563) 386-4236
3. Surveyor:
Michael D. Richmond
2224 East 12th Street
Davenport, Iowa 52803
Ph: (563) 286-4236
4. Attorney:
Jeff Cook
Nagle & Cook
1313 Harrison
Davenport, IA 52803
Phone 563 323 8054



DATE: 2/5/18
TE PROJECT NO: BROWN STREET
563 386.4236 office 386.4231 fax
2224 East 12th Street, Davenport, IA 52803

DRAWN BY: KLC
DRAWN BY: MDR
LOCATION: S: EASTON-CHARLES

REVISIONS:		
NO.	DESCRIPTION	DATE



PREPARED FOR
CHARLES EASTON
4941 BROWN STREET
DAVENPORT, IOWA

SHEET NO.
1 OF 1

City of Davenport
Plan and Zoning Commission

Department: Community Planning and Economic Development
Contact Info: Matt Flynn, 888-2286

Date
3/6/2018

Subject:

Case No. CP18-01: Adoption of the Elmore Corners Area Plan as an Element of the City of Davenport Comprehensive Plan [6th Ward].

Recommendation:

No recommendation.

Relationship to Goals:

Welcome Investment

Background:

The Elmore Corners Area Plan was developed in anticipation of continued growth of the North Elmore Corridor and the desire to achieve a high level of quality and coordinated development in the future.

The Area Plan contains development standards that are proposed to be integrated into the zoning ordinance.

See attachments for additional information.

ATTACHMENTS:

Type	Description
▣ Backup Material	Staff Report
▣ Backup Material	Elmore Corners Area Plan Document
▣ Backup Material	Feb 22 2018 Presentation

Staff Workflow Reviewers

REVIEWERS:

Department	Reviewer	Action	Date
City Clerk	Flynn, Matt	Approved	2/28/2018 - 11:25 AM



PLAN AND ZONING COMMISSION

Meeting Date: March 6, 2018
Request: Case No. CP18-01: Adoption of the Elmore Corners Area Plan as an Element of the Davenport Comprehensive Plan.
Case No.: CP18-01
Applicant: City of Davenport
Contact: Matthew G. Flynn, AICP
Senior Planning Manager
matt.flynn@ci.davenport.ia.us
563-888-2286

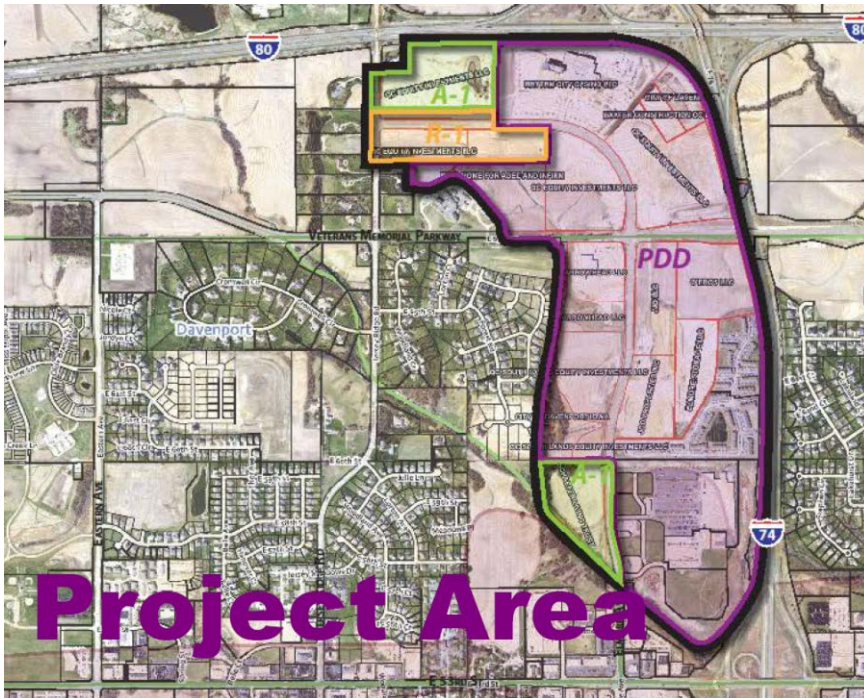
Recommendation:

There is no recommendation at this time.

Introduction:

Case No. CP18-01: Adoption of the Elmore Corners Area Plan as an Element of the Davenport Comprehensive Plan.

Background:



See the attached Scope of Work for the basis and overview of the project.

Also attached is the Draft Plan Document

Public Input:

Public input has been sought throughout the process.

There was a kick off meeting on July 18, 2017, where key stakeholders were interviewed and provided an overview of the project.

On October 25, 2017, the draft plan was presented to the steering committee. Comments were incorporated into the final draft.

On January 22, 2018, a public meeting was held to review the draft plan. Approximately 80 people attended.

On January 29, 2018, the draft plan was discussed at a 6th Ward meeting.

On February 22, 2018, a follow up public meeting was held.

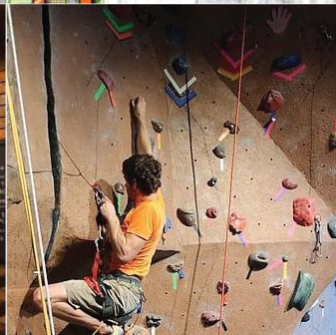
Next steps:

Staff will give a presentation on the Elmore Corners Area Plan and Design Standards at the March 20, 2018 Plan and Zoning Commission Public Hearing.



Elmore Corners Plan

Davenport, Iowa



Draft: November 3, 2017

Planning assistance by:

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Milwaukee, WI 53204
(414) 988-8631
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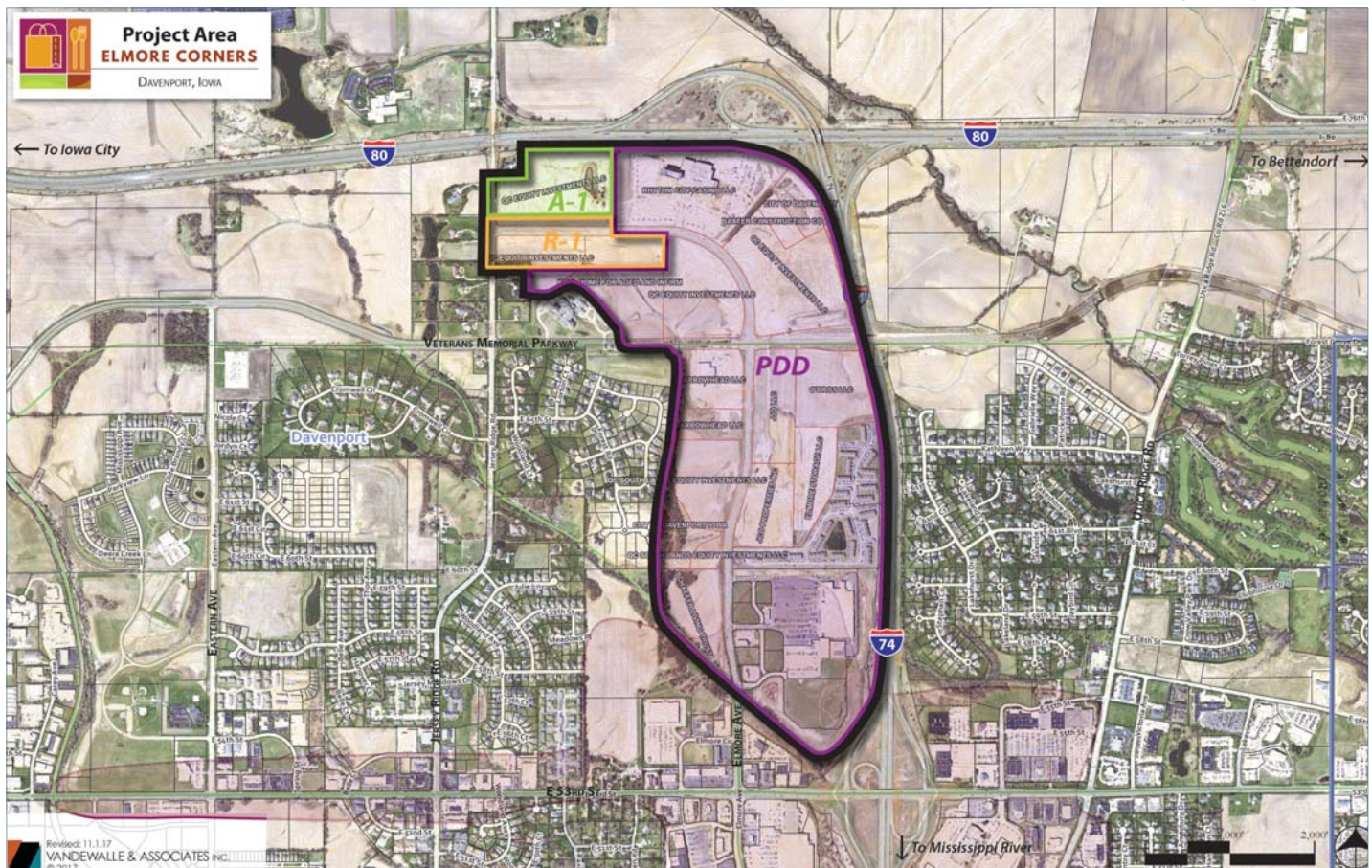
I. Introduction

PROJECT AREA

Located on the northeast side of Davenport, Iowa, at the intersection of Interstate 80 and Interstate 74, the approximately 264-acre project area is sited at a strategic community location. The project area serves as a community gateway from the Interstate 80 corridor. Generally, surrounding land uses include Interstate 80 and agricultural land to the north, single-family homes to the west, retail and commercial to the south, and Interstate 74 and single-family homes to the east.

As shown on Map 1, the majority of the planning area is currently zoned Planned Development District (PDD) with a few R-1 and A-1 parcels, and eight property owners control the majority of the properties. The entire project area falls within the Highway Corridor Overlay Zoning District. Current uses in the project area include a casino, automobile sales lot, apartments, retail, warehouse storage, senior living, and agriculture.

Map 1. Project Area



PROCESS

The planning process took place from July to October of 2017 and the City of Davenport Staff served as the project steering committee. Staff provided guidance throughout the planning process and approved all plans, drawings and recommendations. The steering committee reviewed draft plans two times throughout the project before deliverables were finalized.

Project Kick Off Meeting and Stakeholder Interviews – July 18, 2017

Vandewalle & Associates traveled to Davenport to meet with City Staff, City Officials and project stakeholders. During these meetings, stakeholders provided initial project input and suggestions. Property owners provided plans for their properties and future land uses and improvements were brainstormed. Additional stakeholder input was provided via teleconferences throughout the planning process.

Draft Presentation – October 25, 2017

Vandewalle & Associates presented the draft Site Layout Plan, Pedestrian Circulation and Streetscape Concept, Public Street Network, Landscape Concept, Wayfinding Concept, Branding Concept, and Design Development Standards via a web conference. The steering committee provided feedback and asked questions during the meeting. After the presentation, Vandewalle & Associates finalized all drawings and summarized them into this document, *Elmore Corners Plan*.



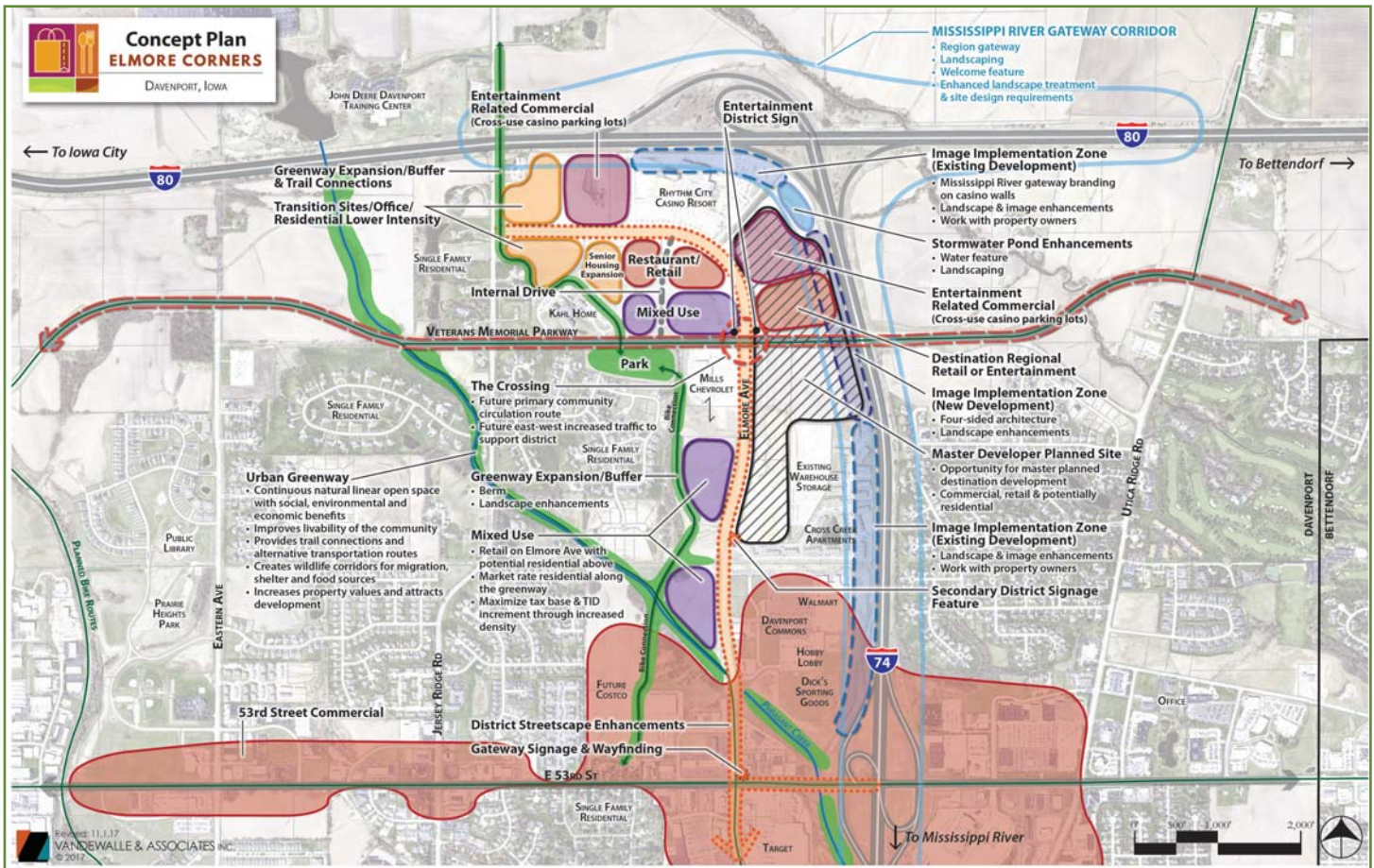
Elmore Corridor



II. Concept Plan

As seen in Map 2, the Concept Plan establishes the planning and development vision for Elmore Corners in the context of adjacent neighborhoods and existing land uses. The map highlights proposed future land uses and public infrastructure. The illustration also identifies important design issues to be considered for a successful development. These are explained further below and in the Design Development Standards section.

Map 2. Concept Plan



POTENTIAL FUTURE DEVELOPMENT

Future development should be a sustainable mix of land uses that are market feasible, serve current community needs, maximize tax base, and support the success of the Rhythm City Casino. Visual access from two interstate highways and traffic generation from the casino create a strong market for retail. While the current retail market is in transition from “brick and mortar” to “on-line” shopping, future development around the casino should be tailored toward experiences and entertainment. Uses that complement the casino should be encouraged in the northern planning area.

An opportunity exists to create a mixed-use neighborhood through a master planned site located between Interstate 74 and Elmore Avenue. This area is sufficient in size to develop retail, commercial, office and residential development along a secondary street system. Lands between the senior housing project and Elmore Avenue should be a mix of commercial related to the casino and residential uses. This large block should be broken up with additional public streets to provide access into the site. Land uses along Jersey Ridge Road should decrease in scale, massing, and vehicular trip generation to create a transition in development intensity between the casino entertainment area and existing single-family neighborhoods to the west.

Along Elmore Avenue, north of the Pheasant Creek corridor, provides a natural break in land use and an opportunity to transition from strictly retail/commercial uses located along 53rd Street to a more integrated neighborhood. These sites should contain mixed-use buildings along Elmore Avenue and around the intersection of Elmore Avenue and Veterans Memorial Parkway. First floor commercial uses will maintain the Elmore Avenue commercial street edge while residential or office above increases development density and tax base. The area between these mixed-use buildings and the proposed greenway is suitable for multi-family residential development. Density could be increased since the residents will have access to the greenway for alternative transportation to public streets.

Potential land uses are further explained in the following Site Layout Plan section of this document.

THE CROSSING

The intersection of Elmore Avenue and Veterans Memorial Parkway will be a major community intersection in the near future. With the completion of Veterans Memorial Parkway construction, a new east/west corridor will run through Davenport, providing vehicles with a new transportation route. These automobile trips will create a commercial market at this intersection and along the parkway. The intersection also becomes an important wayfinding point, identifying the Elmore Corners Entertainment District to the north and retail and commercial to the south.



Example Potential Future Development Uses



IMAGE IMPLEMENTATION ZONES

These areas along the interstate are highly visible and are often the first image of Davenport for visitors. The quality of development along the corridor is important to best represent the City. For this reason, development within the area should be of high quality architecture and site design. Buildings should have four-sided architecture, meaning quality design and materials on all sides of the building. Sites should contain aesthetically pleasing landscaping and storm-water management areas. Parking lots should be screened from the interstate and stormwater detention ponds should be naturalized in shape and landscaping.

MISSISSIPPI RIVER GATEWAY CORRIDOR

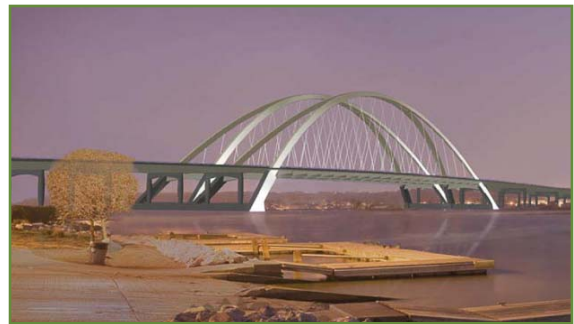
The Image Implementation Zones fall within a larger initiative to unify the image of the Interstate 74 corridor under the Q2030 Regional Plan, specifically the I-74 Gateway Corridor, part of the Prosperous Economy theme/pillar. Projects that fall within this area should highlight the community gateway through both high-quality architecture and community gateway signage. The region may work with the casino operators to use the blank casino walls to highlight region heritage, the Mississippi River, community events, etc. The space should be a lively welcome to visitors.

URBAN GREENWAY

A greenway system would provide numerous benefits to future residents and visitors of Elmore Corners. A greenway is a continuous and linear natural open space with environmental, social and economic roles. The natural role is to provide animals with a natural habitat for nesting, migration and food sources. Greenways can also be used for storm water infiltration and conveyance. The social role is to improve urban recreation opportunities, encourage community interaction, and provide an alternative transportation mode. Greenways also have economic benefits by increasing land values. Businesses located near greenways highlight this feature as an employee amenity and greenways aid in workforce retention and attraction. The greenway is proposed as a 100-foot wide corridor which will provide a buffer space between existing neighborhoods and new development. The public space can also be used to organize development by providing an off-street path accessible from new buildings.



Example Image Implementation Zone



Mississippi Gateway Corridor



Urban Greenway



NEIGHBORHOOD CONTEXT

Established single-family neighborhoods border the Elmore Corners planning area to the west. As development occurs, design attention must be paid to building placement, massing, design, setbacks, and lighting to ensure future development does not negatively impact these neighborhoods. The proposed Urban Greenway along the western project boundary will provide a natural buffer between established and proposed neighborhoods.



Neighborhood Context

STREETSCAPE AND WAYFINDING

The district public space image and signage will be as important as architecture in establishing a high-quality development. Public streets should contain decorative street lights, banners, and plantings. A consistent district wayfinding system should be installed to direct visitors to destinations and activities.



Example Wayfinding Signage

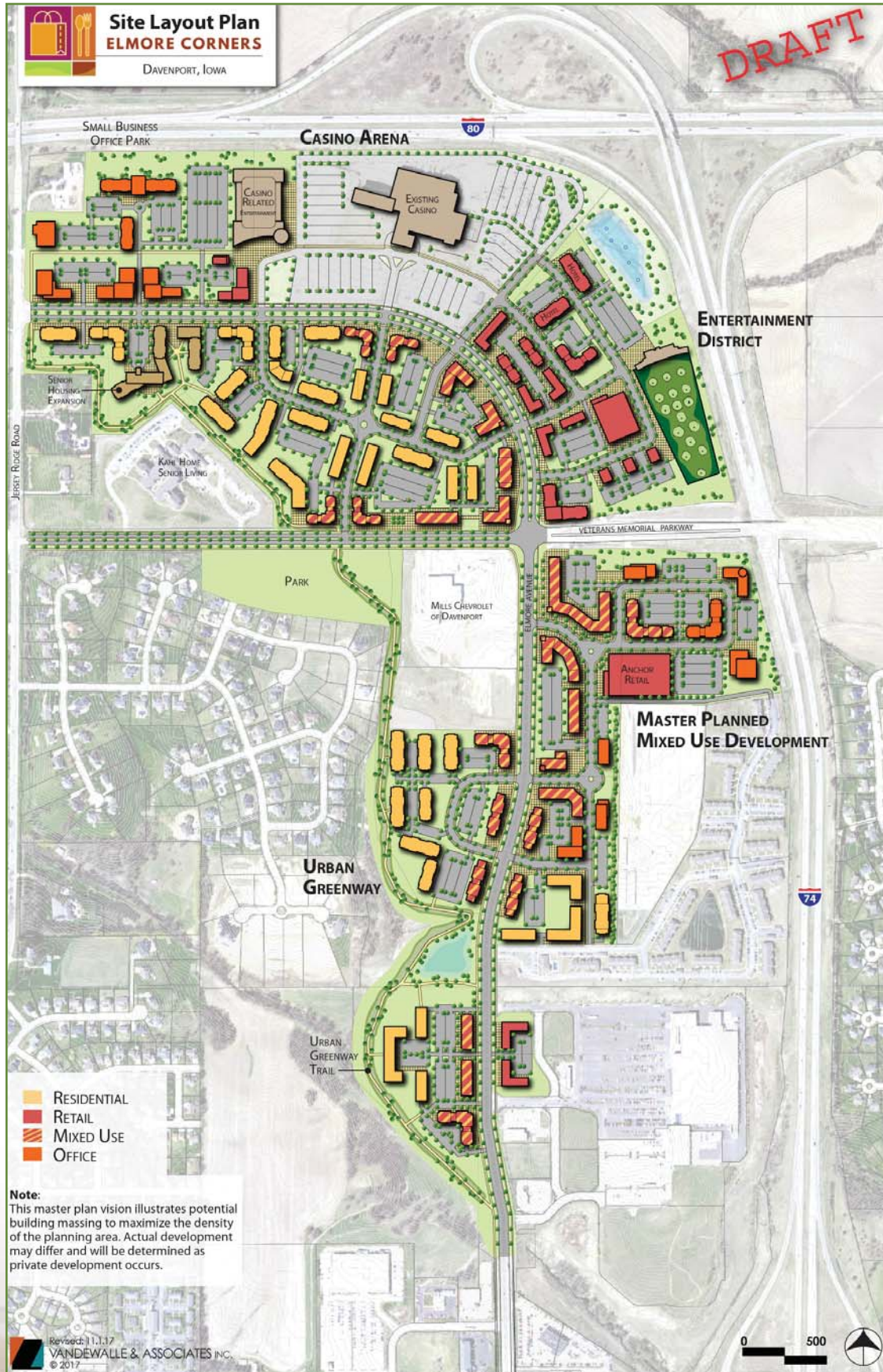


III. Site Layout Plan

The Site Layout Plan provides a vision for how Elmore Corners could develop to provide a community gateway development, maximize tax base, and create walkable and sustainable neighborhoods. The plan illustrates potential building massing, locations, and density, though actual development proposed by private developers will vary from this vision. The plan represents the steering committee desired development pattern.

In addition to future building locations, the Site Layout Plan also contains suggested marketable future land uses. This information is provided by project sub areas for ease of describing potential future development. The subareas are not intended to be branding names. These subareas are described in the following text and summarized on Map 4. Sub Areas on page 19.





ELMORE CORNERS ENTERTAINMENT DISTRICT

Being adjacent to the Rhythm City Casino and highly visible from the junction of two interstate highways, this area has the market potential to become a destination entertainment district. The area is envisioned as an active district providing visitors with numerous activities in a walkable environment. Future streets and walkways connect new development to the casino entrance. Future uses could be a mix of entertainment anchor uses for people of all ages. Parking should be shared between uses to maximize developable area and minimize parking lots and distances between uses. Buildings near the casino may be able to share parking with the casino.

Potential uses could include:

- Hospitality
- Regional retail
- Sports arena
- Sports training facilities
- Sports entertainment
- Indoor activity center of children and/or adults
- Music venue
- Restaurants
- Brew pub/micro brewery
- Health and wellness facility
- Water park resort

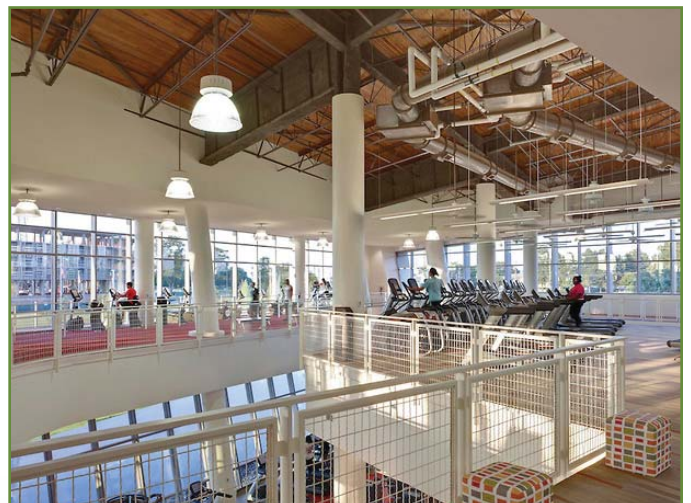
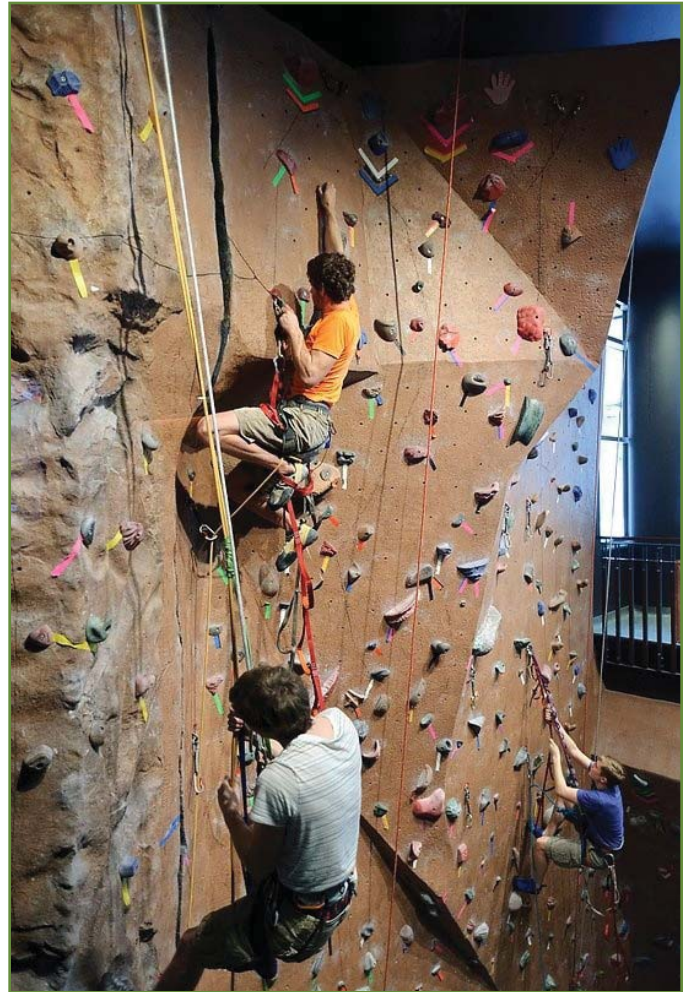
Quality four-sided architectural standards and enhanced landscaping will be especially important within this area, since it is highly visible from the interstate highways and falls within the I-74 Gateway Corridor, as identified in the Q2030 Regional Plan. Buildings in this area are anticipated to be one to five stories in height. The site layout plan shows the following approximate square footage at one story for retail and five stories for hospitality:

Retail/Commercial – 140,000 sf

Entertainment Anchors – 70,000 sf

Hospitality – 300 rooms





MASTER PLANNED DEVELOPMENT

This area provides an opportunity for a large-scale master planned development. With three property owners, the land may be developed jointly into a dense, mixed-use neighborhood. Anchored by a destination retail use, the neighborhood is envisioned to be urban in design with buildings located along new public streets providing pedestrians with an enjoyable and walkable connection between uses. First floor buildings should be commercial with office and residential on above floors. An area along Interstate 74 should be reserved for office sites providing employers with great highway visibility.

Potential uses may include:

- Mixed-use buildings with retail or services on ground floors and residential or offices above
- Retail - destination branded, i.e. "Shops at Elmore Corners"
- Restaurants
- Anchor retail – potential grocery store
- Office sites along Interstate 74 and Veteran's Memorial Parkway
- Structured parking to achieve critical density to create a walkable district

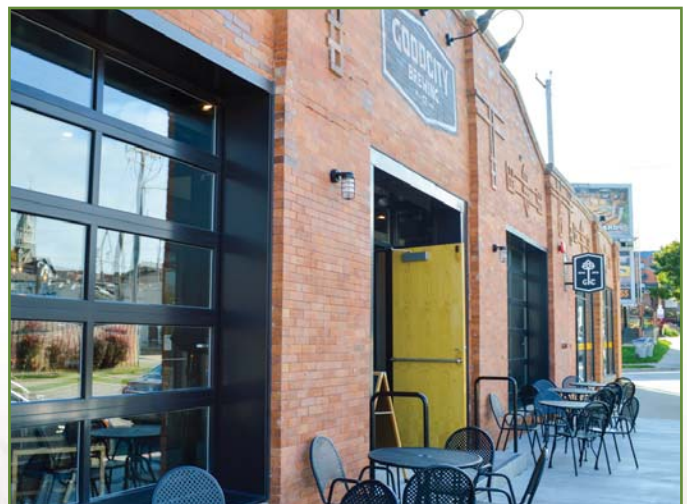
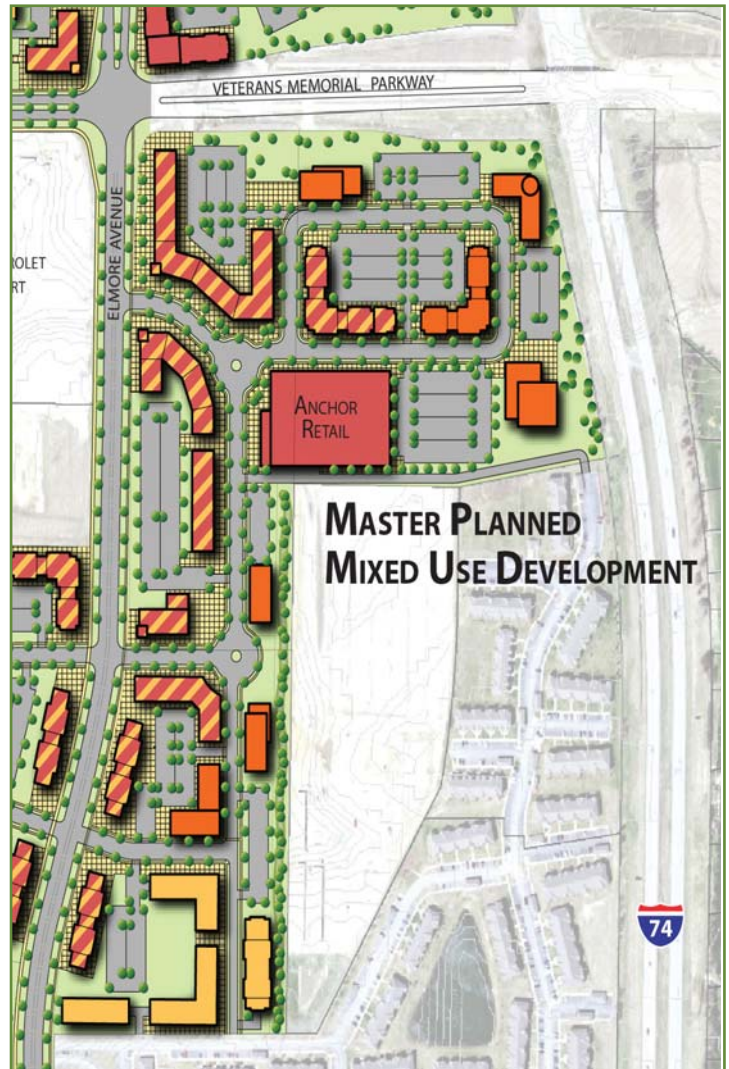
Surface parking lots could be initially constructed and be phased into structured parking as the site densifies. Buildings in this area are anticipated to be one to four stories in height. The site layout plan shows the following approximate square footage/residential units at three stories for office and mixed-use buildings and one story for the anchor retail:

Retail/Commercial – 290,000 sf

Anchor Retail – 95,000 sf

Office – 350,000 sf

Residential – 450 units





ELMORE CORNERS MIXED-USE NEIGHBORHOOD

This area, located between the Rhythm City Casino and senior housing along Veterans Memorial Parkway, should be developed into a residential neighborhood with commercial along the major streets. A future public street connecting the casino entrance to Veterans Memorial Parkway extends the commercial market from the Elmore Avenue/Veterans Memorial Parkway intersection west to the two new intersections. Mixed-use buildings are suggested along the primary streets to serve both district visitors, neighborhood residents, and citizens commuting along Veterans Memorial Parkway. These buildings provide excellent ground floor spaces for local retailers and service providers.

Residential is envisioned as the primary land use west of the casino and proposed north/south street. Future development could be a combination of multi-family housing and an expansion of the senior housing development. All buildings should have trail access to the greenway. The proposed north/south public street also provides a controlled crossing of Veterans Memorial Parkway, connecting the greenway across the street. Buildings in this area are anticipated to be three to four stories in height. The site layout plan shows the following approximate square footage/residential units at three stories:

Retail/Commercial – 140,000 sf

Senior Housing – 150,000 to 200,000 sf

Residential – 1,100 units





NORTH END

This area, located between Jersey Ridge Road and Rhythm City Casino, has excellent visibility from Interstate 80. While retail may not be suitable due to distance away from the intersection of Elmore Avenue and Veterans Memorial Parkway, the area may be attractive for companies to locate an office or headquarters. The master plan shows a large office building along the interstate with smaller office sites to the south, along Elmore Avenue. A small business office park could be established providing employees with access to retail and commercial, as well as, nearby housing. Users are anticipated to be in the 10,000 to 40,000 sf range, providing owner occupied opportunities.

The eastern edge of the area may be developed with a casino-related destination use such as an entertainment facility, arena, or indoor/outdoor water park which would generate traffic and provide market for support retail or restaurants. A destination use could utilize existing parking lots at the casino to reduce the need for additional paved area. A pedestrian connection should be constructed between this area, the casino, and the Elmore Corners Entertainment District. The connection could also extend to the west connecting office and commercial uses to the greenway.

Buildings in this area are anticipated to be one to four stories in height, though a corporate user along the interstate may be taller. Office buildings on the west end should be one to two stories to transition to the existing single-family neighborhood to the west. The site layout plan shows the following approximate square footage at three stories for office use and one story for retail and casino-related destination uses:

Retail/Commercial – 25,000 sf

Office – 420,000 sf

Casino-Related Destination – 60,000 to 120,000 sf





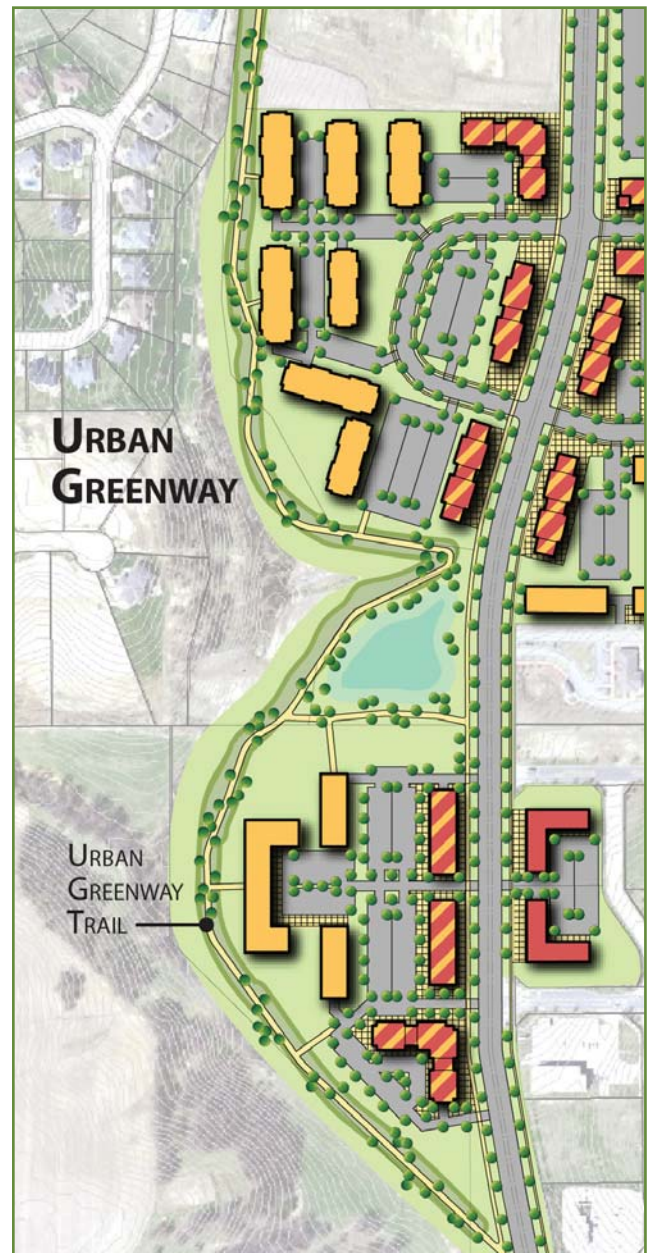
ELMORE CORRIDOR

Uses along Elmore Avenue are currently retail and commercial. As Elmore Corners develops into a sustainable neighborhood with a mix of uses, this area should be reserved for mixed-use buildings along Elmore Avenue and multi-family residential to the west. Mixed-use buildings may contain retail, commercial, or service uses on the ground floors with parking to the rear of buildings. Above floors may be residential or office. All buildings have walkway connections to both sidewalks along Elmore Avenue and the pathway in the proposed Urban Greenway. The latter offers an alternative transportation route, exercise area, and access to the natural environment.

This plan recommends removing the berm, located along the western edge of Elmore Avenue, across from Davenport Commons commercial development. This will allow future buildings along the street to have commercial ground floor uses. If the berm remains, then the uses on the site would be residential as the berm would block views of commercial properties. Buildings in this area are anticipated to be three to four stories in height. The site layout plan shows the following approximate square footage/residential units at three stories:

Retail/Commercial – 100,000 sf

Residential – 600 units



RETAIL INFILL

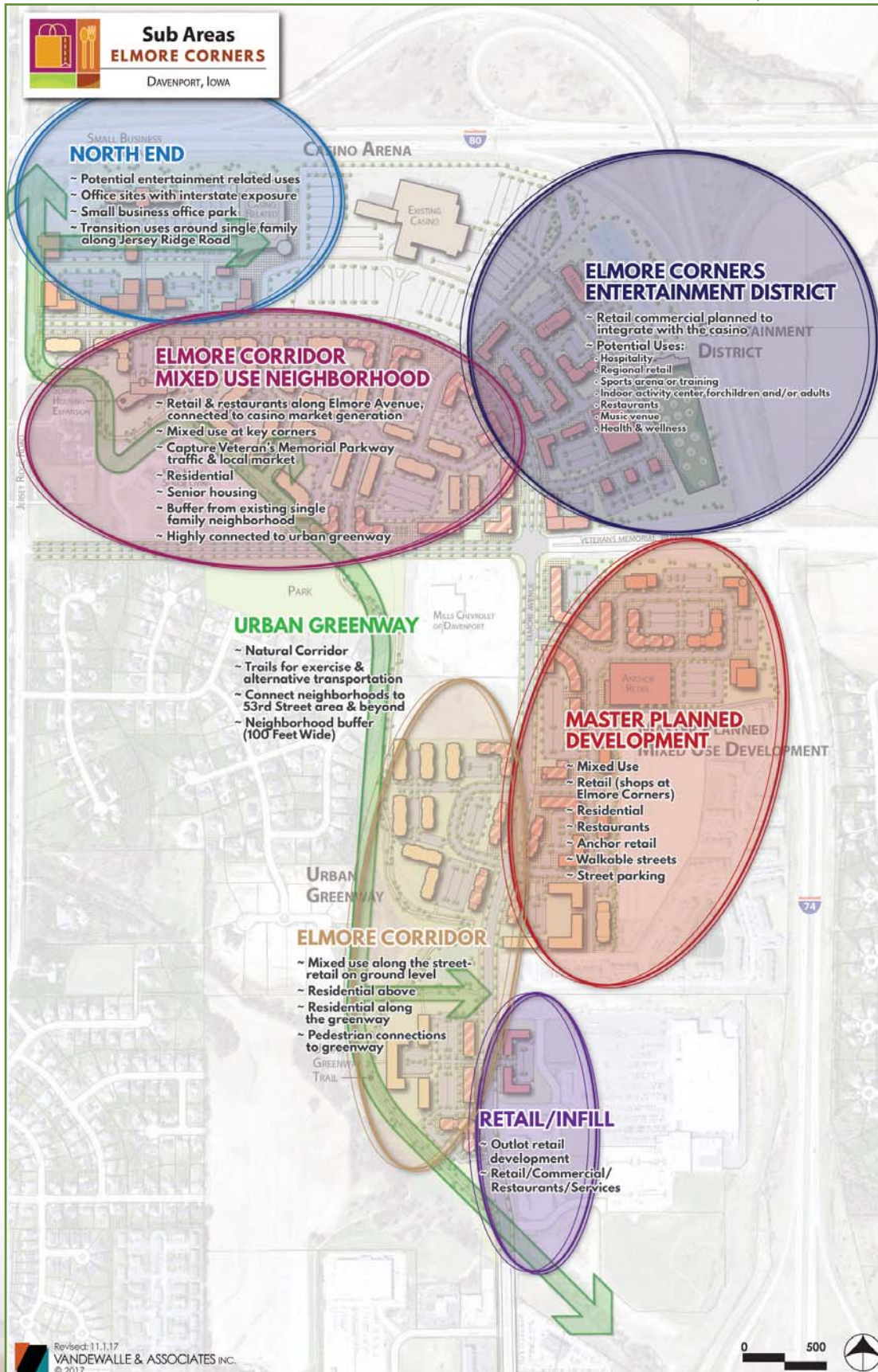
The final area of the Elmore Corners Plan is the out-lot sites along Elmore Avenue within Davenport Commons. These sites will most likely remain as sites for retail, commercial, service, or restaurants. Parking is encouraged to the rear to maintain a building face streetscape along Elmore Avenue. Locating the buildings near the public street will also announce Davenport Commons for visitors.

Buildings in this area are anticipated to be one to two story. The site layout plan shows the following approximate square footage at one story:

Retail/Commercial – 26,000 sf

The development areas and projects identified within the Site Layout Plan are not dependent upon each other to develop. Each project can stand on its own and could be a separate developer and/or phase.

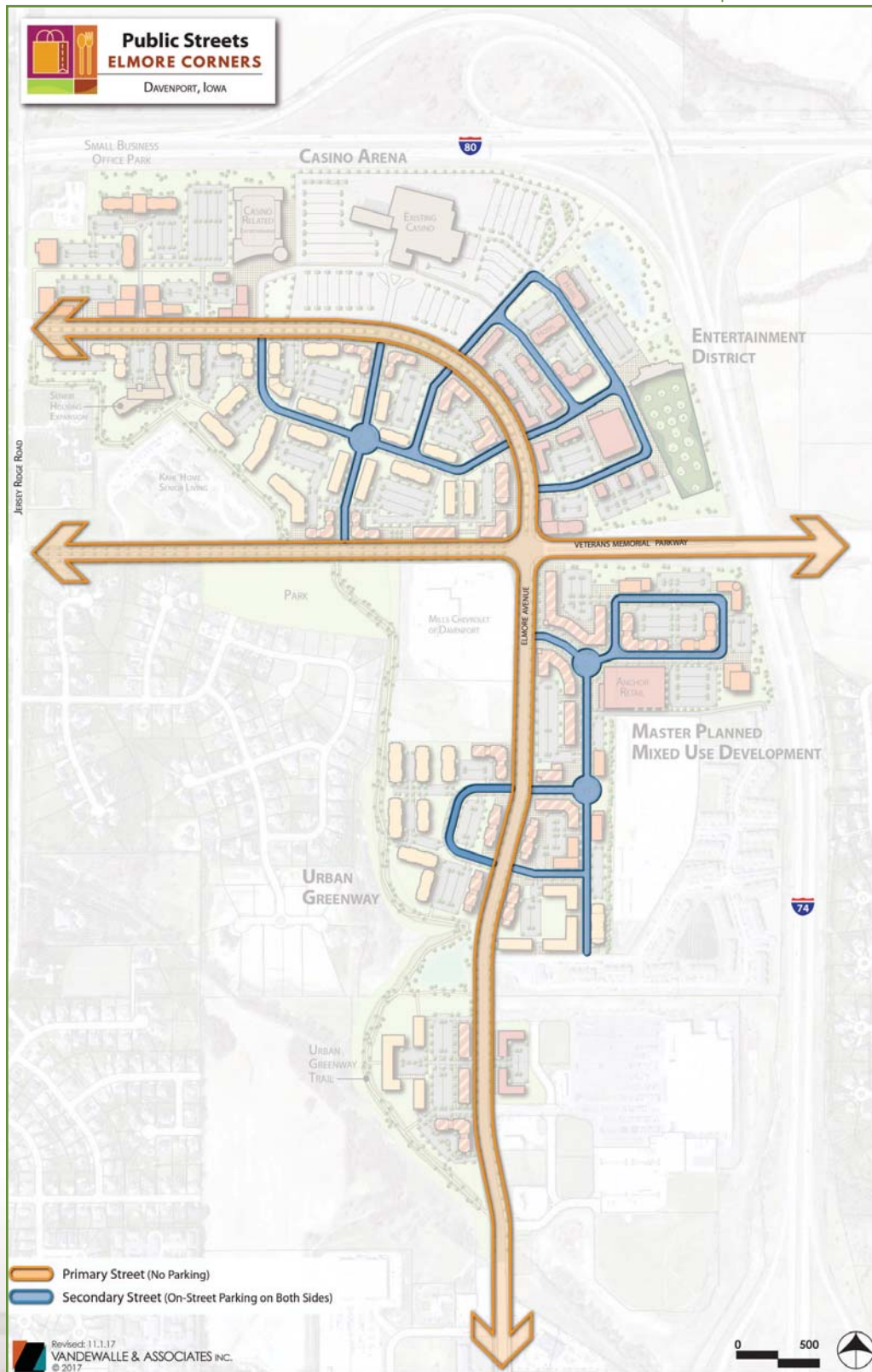




CIRCULATION

Maps 5 & 6 illustrate the circulation vision for Elmore Corners. Map 5 highlights public street types, while Map 6 illustrated pedestrian circulation and streetscape areas.

Map 5. Public Streets



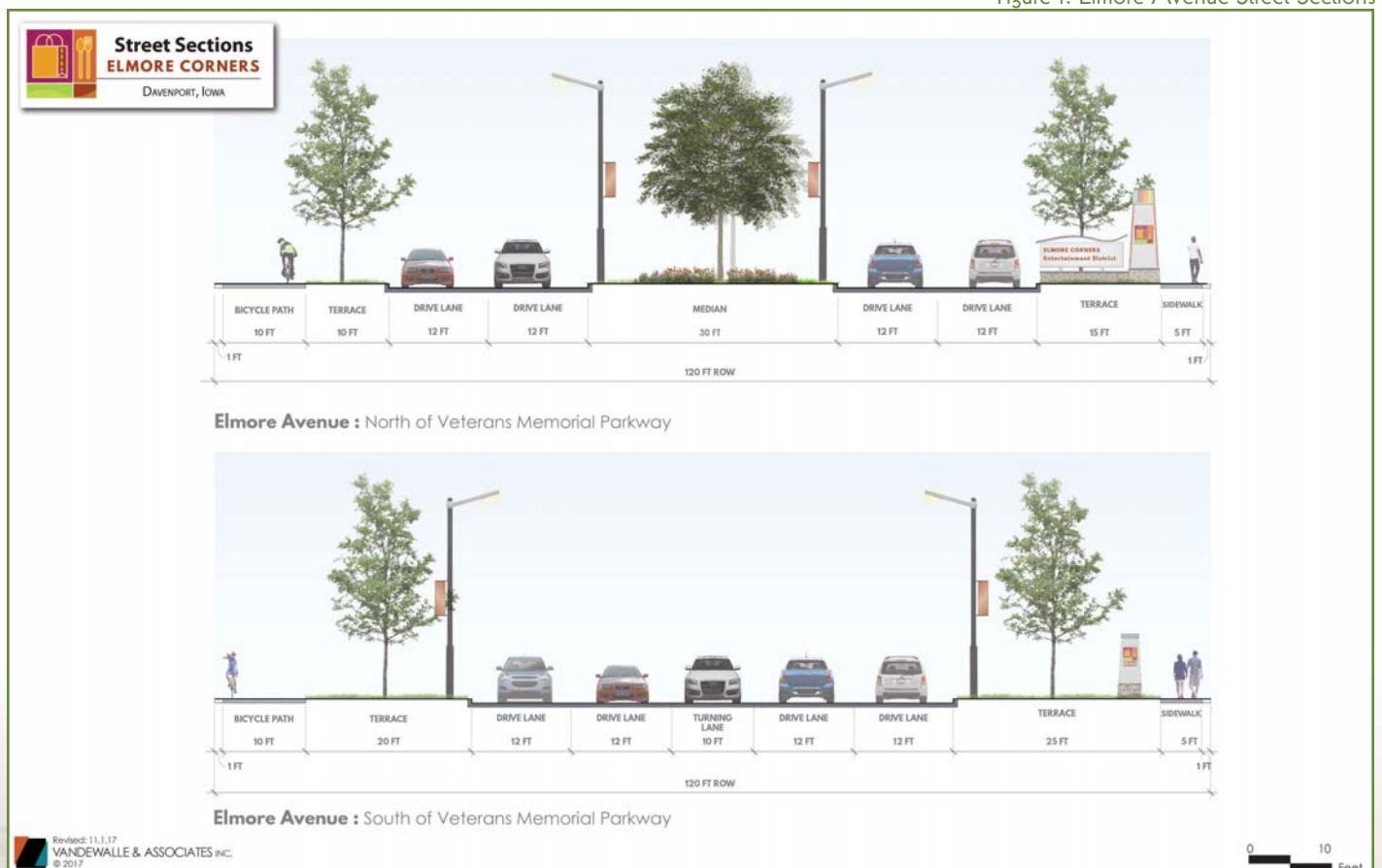
AUTOMOBILE CIRCULATION

The planning area contains two existing public streets, Elmore Avenue running north/south and Veterans Memorial Parkway running east/west. During this planning process, Veterans Memorial Parkway was under construction within the planning area. These streets are designated as Primary Streets on Map 5. Both streets are four-lane streets with two travel lanes in each direction and no on-street parking. To reduce traffic on these two streets, better connect future uses, reduce block size, and provide pedestrian scale streets, additional public streets are proposed. These streets are designated as Secondary Streets. These streets allow access into the existing large blocks, which can increase development intensity and tax base. Secondary streets should have parking on both sides to serve commercial and residential uses, as well as, calm traffic speeds. Terraces in these areas should be considered for stormwater management in the form of stormwater planters and rain gardens.

Figure 1 provides a streetscape vision for Elmore Avenue. The street section north of Veterans Memorial Parkway contains a median, allowing for landscaping. This space could be a combination of climax trees to provide street shade, ornamental trees to provide spring and fall interest, native grasses to provide fauna habitat, and potentially rain gardens to collect and infiltrate street stormwater runoff. The medians may also contain street lights to illuminate the road surface. Median street lights are often found in special districts, highlighting the unique Entertainment District. These tall-mast lights should contain large banners celebrating the Entertainment District and special events. Terraces are proposed as turf grass with shade trees and space for wayfinding.

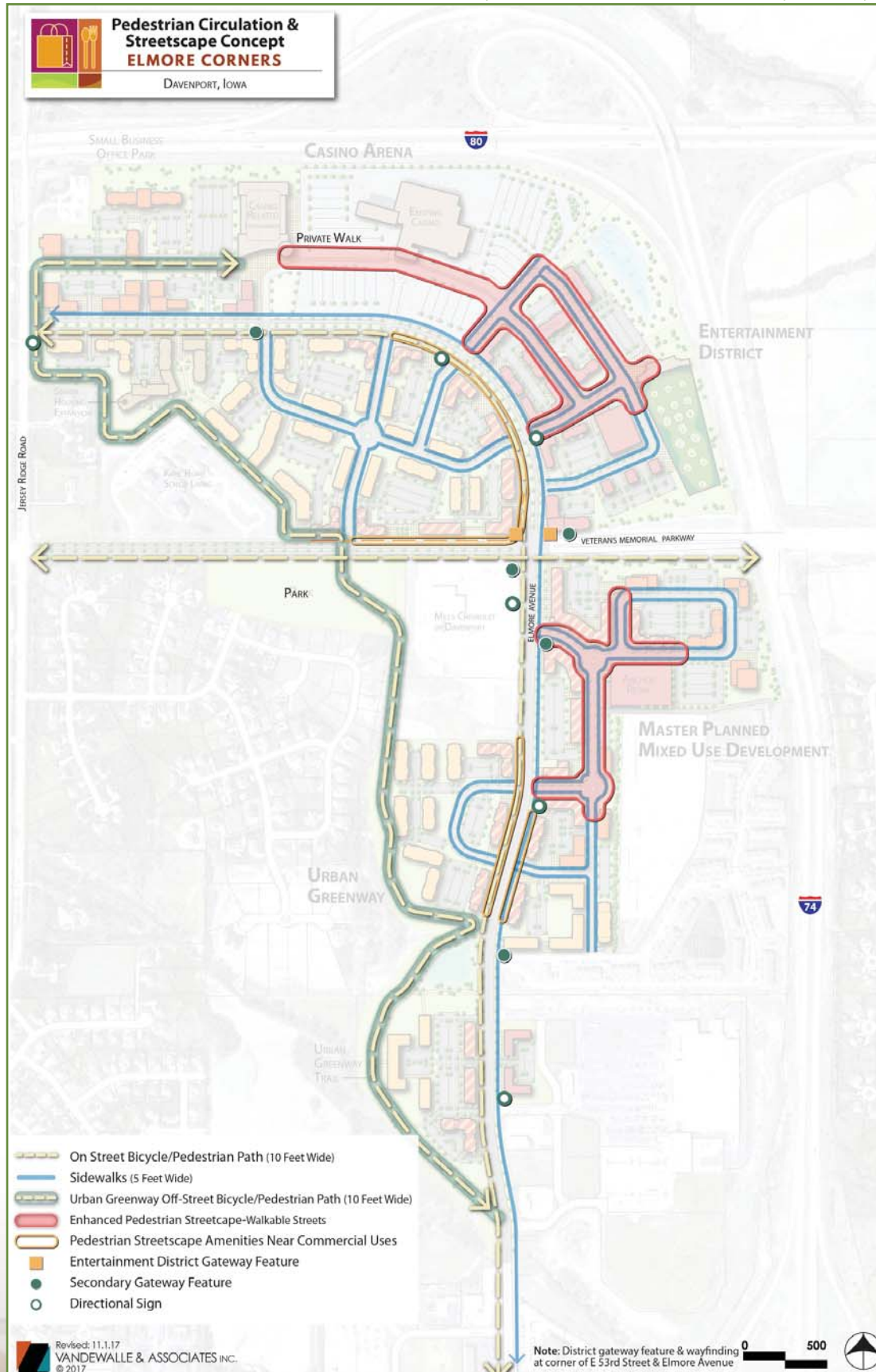


Figure 1. Elmore Avenue Street Sections



BICYCLE/PEDESTRIAN CIRCULATION

Map 6. Pedestrian Circulation & Streetscape Concept

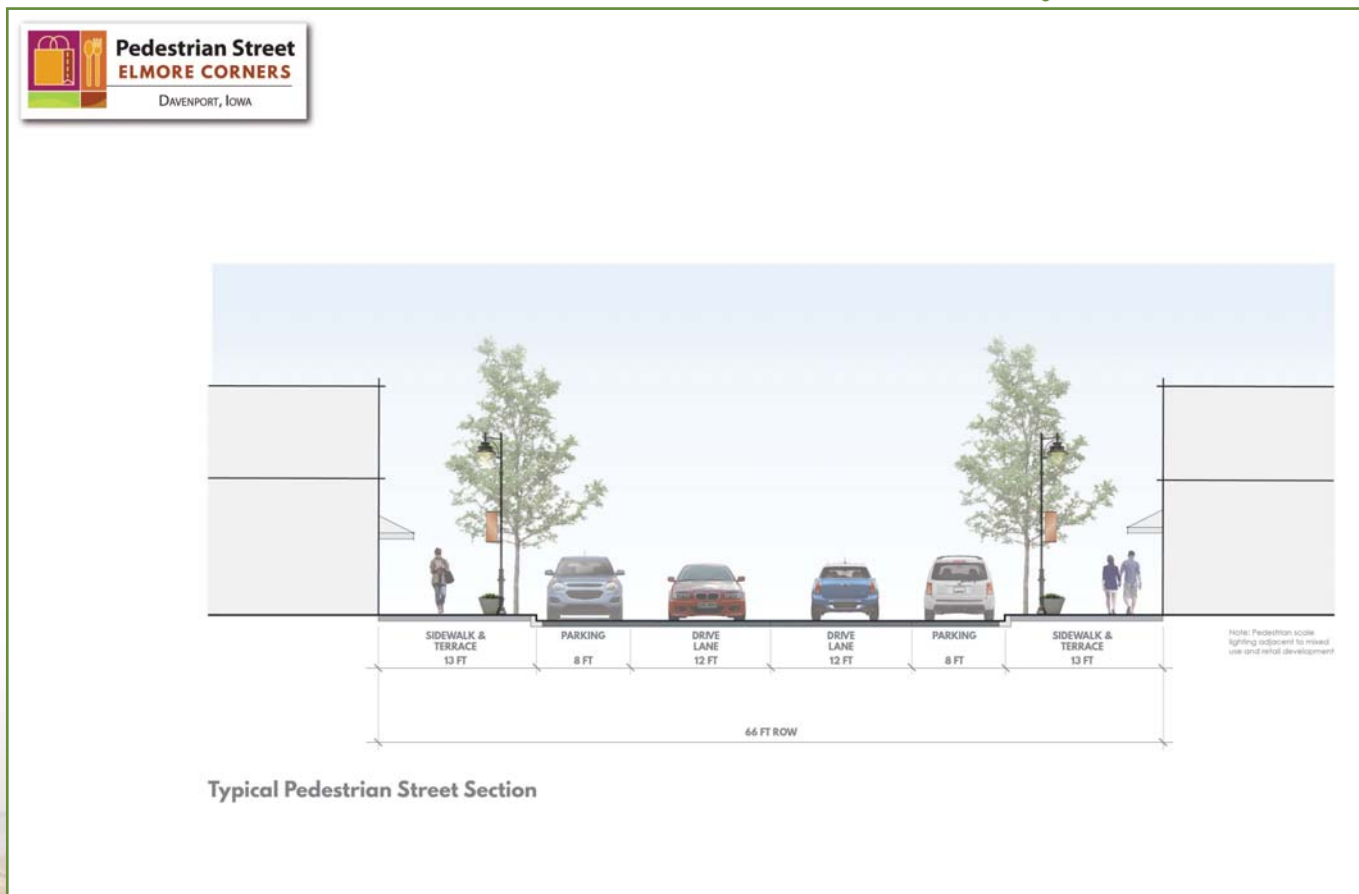


BICYCLE/PEDESTRIAN CIRCULATION

The existing two public streets both contain a sidewalk on one side and a bicycle/pedestrian path on the other side, providing non-motorized circulation options within the planning area. In addition to these facilities, sidewalks are proposed on future streets and a path is proposed within the Urban Greenway, creating a connected pedestrian network. Map 6 illustrates locations for enhanced streetscape areas to create Walkable Streets. These spaces are along secondary streets and have urban cross sections with combined, paved sidewalks and terraces. Figure 2 illustrates a typical street section for these pedestrian streets. The drawing highlights pedestrian scale street lights to illuminate sidewalks, planters, and banners to announce events or destinations. Buildings in these areas are encouraged to interact with the public streetscape by providing outdoor seating and entryways that integrate into the public rights-of-way. Map 6 also suggests location for a wayfinding system, which is explained in the next document section.



Figure 2. Pedestrian Street Section

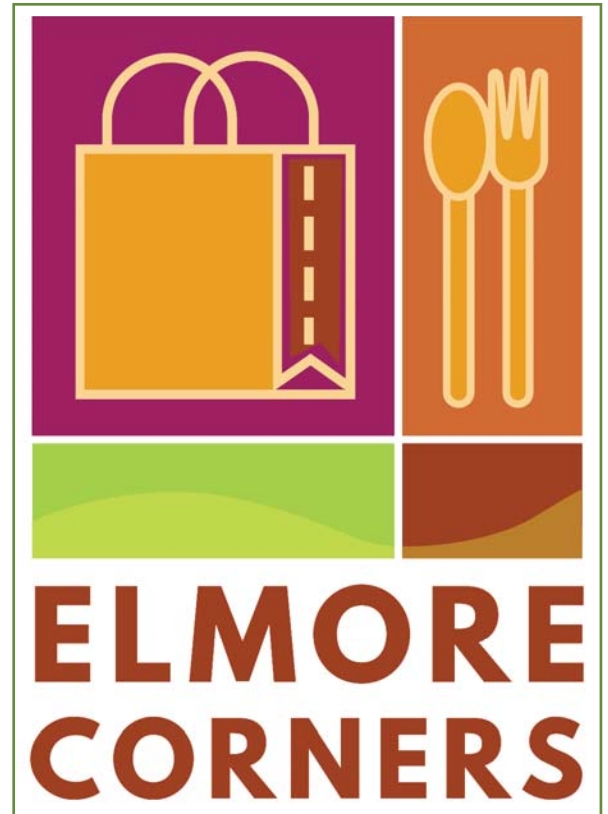


IV. Branding Concept

LOGO

The City created a logo to brand and celebrate the Elmore Corners district. The logo is a graphic representation of an energetic new destination with a range of amenities to attract visitors and serve residents. The following describes the logo design in more detail.

- The four quadrants and the white lines that define them represent the urban corners created by the intersection of two arteries—Elmore Avenue and Veterans Memorial Parkway. The lines are offset to add a dynamic quality
- The upper left corner shows a shopping bag representing retail services
- Upper right bears a fork & spoon representing food and dining—suggesting this is a family-friendly place to spend time, and more than a one-stop destination
- The lower two quadrants show a rolling topography representing the nearby greenspace and path amenities available, and suggesting this will be an attractive place that integrates into its environment. The curving line is also reflective of the casino roof
- Colors were chosen to be fresh and modern while relating to food, nature, and quality built environments. Specifically, berry and pumpkin evoke appetite and foodstuffs; green evokes healthful foods and nature; and terra cotta suggests earth and brick. The split-complementary color scheme (berry-orange-green) adds pop and vibrancy



SIGNAGE

A wayfinding package was developed to celebrate Elmore Corners and direct visitors to destinations. These series of signs identify both the district and destinations within the area.

Gateway Sign

This monument sign is proposed at the intersection of E. 53rd Street and Elmore Avenue to announce the district to the north. The design takes design cues from the Rhythm City Casino. The base is proposed to have stone matching the casino building base material. The top is a curved metal band similar to the casino roofline. The sign materials are primarily stone and metal to reflect the modern Elmore Corners District.

Entertainment District Sign

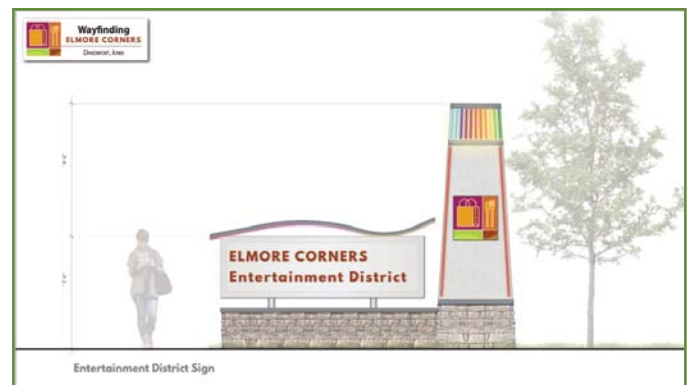
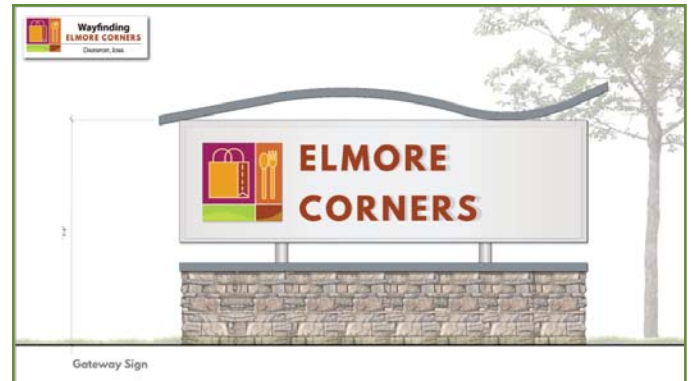
This monument sign is proposed at the intersection of Veterans Memorial Parkway and Elmore Avenue to announce the proposed entertainment district around the casino. The design takes further design cues from the Rhythm City Casino. The left half contains a stone base and curved metal top, similar to the gateway sign, though a band of neon light highlights this curve. The right side of the sign is a pillar of similar materials with the district logo and bands of neon light, matching the casino entrance.

Secondary Sign

Due to the large size of the Elmore Corners District, additional identification signs are recommended throughout the area. These smaller, columnar signs should be located at entryways from all directions along Elmore Avenue and Veterans Memorial Parkway. The design uses similar materials to the other gateway signs.

Directional Sign

The final sign type within the wayfinding package is a pole mounted metal sign directing visitors to specific destination within the district. The sign contains the district logo and a list of destinations with arrows pointing the travel direction. Examples of potential destinations may be buildings such as the Rhythm City Casino, wellness center, or water park resort, or may be a larger development project such as a shopping destination or business park.



V. Design Development Standards

BUILDING DESIGN GUIDELINES

BUILDING COMPOSITION

Guideline: Buildings shall be of a quality design that contribute to the Elmore Corners destination appearance.

Base, Middle, Top

Building facades shall be composed to define base, middle and top elements. The base of the building anchors it to the ground and is the interface between the building and people. This element shall be the most articulated. The transition between the middle of the building and the base and top shall be varied by use of materials, composition, repetition or ornamental features. The top terminates the building against the sky and provides an opportunity to create an interesting silhouette.

Rhythm

Rhythm refers to the regular or harmonious recurrence of building elements. Rhythmic elements need not be boring, but can provide the visual “glue” that links together various buildings on a street. These patterns commonly reflect the building’s repetitive structural bays, often with the end bays given special identity. This articulation of the facade helps provide scale by comprising the facade into smaller visual parts. The variation of rhythm from building to building reaffirms the individuality of each building, while the recurrence of an overall rhythm helps unify the facade. The building’s structural column lines shall in some way be evident or expressed on its facade.

Scale

In general, distinctive compositional elements of buildings (entries, structural bays, roof elements, windows) shall be distinguishable from both near and far distances. The size and shape of these elements shall give the building scale to relate to pedestrians as well as surrounding buildings. Special attention shall be paid to overall building height and massing in order to maintain a scale and character compatible with the area.

Massing

Large buildings shall be comprised of a hierarchy of masses and forms to give the building a more human scale and visual richness. Techniques include using designed recesses or projections, creating distinct building components, and varying occasional roof forms according to individual building components. However, excessive use of changes in form can create a decidedly confused street façade and disharmony.



Proportion

Building massing and components shall demonstrate consistent proportional harmonies. Typically, pedestrian friendly streets have building elements that rely more on vertically proportioned elements than horizontally proportioned elements. While no architectural rule is always valid, the use of vertically proportioned facade components seems to offer a traditionally street-friendly composition (regardless of specific architectural styles). The use of proportion is intended to provide a sense of visual harmony among elements of a building.

Architectural Form

- Buildings shall be designed to create a pedestrian friendly and scale neighborhood
- Buildings shall be primarily rectangular in form and sited parallel to public streets
- Buildings are encouraged to have a mix of tenants and uses
- Entrances shall be provided at public streets and on parking sides
- Connect building entrances to public sidewalks and pedestrian pathways
- Articulate building shapes with window bays, cornices, parapets, and towers
- Buildings shall have primarily flat roofs with varied or broken lines
- Roof mechanicals shall be screened
- Locate service areas away from public streets and provide architectural screening
- Drive through facilities shall be located on rear building façades as to not dominate the streetscape
- Modify franchise architecture to fit the desired Elmore Corners character

Inappropriate Design

- Flat, massive, monolithic geometric shapes
- Plain, undersigned buildings with large blank spaces
- Small buildings with single occupants
- Gabled, hipped, or steeply pitched roofs
- Large, monotonous roof forms
- Visible building mechanicals
- Faux-historic buildings

Building Composition



BUILDING FAÇADE

Guideline: Buildings shall have varied facades composed of high quality lasting materials.

- Façades shall be articulated on all sides to provide visual interest.
- Break up façades and create visual interest and pedestrian scale by:
 - Varying materials and colors
 - Varying façade vertical and horizontal setbacks
 - Incorporate architectural reveals or floor step backs
 - Vary window styles and moldings
 - Design enhanced architectural details on the ground level
- Façades on multiple-tenant buildings shall be broken into design elements that reflect individual tenant spaces through vertical façade plain staggering, window/door groupings, and awnings
- Differentiate ground and above floors through horizontal banding, signage bands, and windows
- First floor façades shall be transparent to allow retail uses to interact with the street and parking-side entrances
- Commercial and retail façades shall have at least 40% transparency on the street façade
- Secondary façades that do not face the public street or parking areas are encouraged to have transparent windows. If this is not possible, then façades shall contain architectural features that provide interest
- Building at corners shall contain interesting architectural features that address the corner such as towers, entrances, and metal or cloth awnings
- Identify building entrances through permanent awnings, overhangs, or signage

Example Façade Design



Inappropriate Façade Treatment

- Large unarticulated wall planes
- Large expanses of walls without windows
- Projecting wall-mounted mechanical units
- Vinyl awnings
- Buildings with no pedestrian scale or defined entryways



Materials

- High quality materials shall be installed on buildings to promote Elmore Corners
- Long lasting materials such as brick and stone shall be used to withstand weather and provide a feeling of permanence
- Primary building materials shall be natural stone, masonry, brick, and glass and be of natural earth tone colors
- Secondary materials such as decorative metal, wood, stucco, and cement board may be incorporated
- Using a mix of materials is recommended to create visual diversity on the building façade
- Enrichment of the pedestrian realm requires building materials at the street level to be of high quality:
 - These could include items such as brick, stone, decorative concrete masonry units, metal panel systems, or other creatively used high quality and durable building materials
 - Decorative finished block systems can be used along the base of the buildings, and as an accent, but are not recommended as the dominant building material on the entire building
 - Utility grade materials shall only be used on facades of the building not visible from publicly accessible areas
- Freestanding commercial structures shall be designed as four-sided architecture with finish grade materials used consistently on all façades:
 - Recommended primary materials include brick, stone and glass
 - Other materials such as precast concrete, decorative concrete block or decorative facade panels may be appropriate if properly detailed and integrated with the architecture
 - Metal and finished wood may be used as accents, but shall not be the primary material for any façade

Example Materials



Inappropriate Materials

- Unfinished exposed concrete block
- Large, unfinished, pre-cast concrete panels
- Unfinished poured-in-place concrete
- Corrugated metal, vinyl, or aluminum siding
- Plywood siding panels
- Exposed treated lumber
- EIFS as a primary material or located on the first floor
- Windows
 - Moderate to highly reflective glazing
 - Strongly colored or darkly-tinted glazing
- Colors
 - Predominant use of bright, high intensity colors
 - Predominant use of metallic and neon colors



SETBACKS

Guideline: To provide a continuous street edge along the Elmore Corners District, buildings shall be sited within setback areas unless not possible due to affecting business operations.

- Along Elmore Avenue and Veterans Memorial Parkway, the primary building façade shall be located within 20-40 feet from the property line
- Along secondary streets, the primary building façade shall be located 0-10 feet from the property line
- To maintain an urban edge that is pedestrian friendly, areas adjacent to parking lots shall be screened with enhanced landscaping

Inappropriate

- Buildings set back over 40 feet from public streets
- Parking lots between public streets and the primary façade of a building



DENSITY

Guideline: Develop land as densely as possible to increase tax base while providing adequate landscaping, stormwater management, and connections to public ways.

- Development shall be designed to an urban density with minimal spaces between buildings
- Install advanced stormwater management practices to increase density
- Density may increase through vertical mixed-use and shared parking arrangements



SIGNAGE

Guideline: Signage shall function as a marketing device, complement building architecture and materials, and not dominate the site. All signs must meet City Sign Codes.

Appropriate and attractively designed site signage is an important component of the overall aesthetic appeal of any commercial or mixed-use area. Good signage based on design and graphic quality is encouraged throughout Elmore Corners.

- Business signage shall be wall mounted (projecting or flat), monument (if located away from the building), and window, canopy or awning style
- Similar materials, colors and styles shall be used to ensure the signage is consistent with the building design
- Signs shall reflect and enhance the nature and appeal of the retail and commercial experience and not be just a list of tenants
- Materials shall be of high quality to prevent premature weathering of the sign
- Signs shall fit within and not overwhelm the architectural features of the buildings
- Signs shall harmonize with their surroundings in terms of size, shape, color, texture, and lighting so that they complement the character of the neighborhood
- Signs shall be professionally constructed using high quality materials such as metal, stone, tile, composites, brass/metal plated, hardwood, and glass
- The creative use of materials, lettering, and interesting use of graphics is allowed and encouraged if the signs work well and complement the overall building and street design
- Signs shall be designed with the purpose of promoting retail and street activity while enhancing the pedestrian experience
- Addresses shall be clearly visible from the public right-of-way
- The use of tastefully designed pedestrian scale directories is encouraged for multi-tenant buildings
- Monument signs shall contain a base and not be pole-mounted
- Monument signs shall contain landscaping at the base that will not grow to cover the sign message
- Lighting shall be designed carefully to avoid excessive glare or over illumination:
 - Letters can be illuminated internally or externally
 - External lighting must be discreet and unobtrusive, and not shine outward into the pedestrian or driver vision path
 - Fixtures shall be simple but attractive
 - Lighting signs and letters shall be done in an attractive and subtle technique
 - Individual letters that are backlit, halo-lit, reverse illumination channel letters, and neon are encouraged



Example Signage

Inappropriate Signage

- Pole signs, billboard style wall-mounted and roof mounted signs are not allowed without special permission from the City Plan Commission
- Monument signs advertising individual businesses
- Tall mast signs
- Large temporary signs that impede pedestrian traffic
- Inflatable signs
- Overly illuminated signs
- Unshielded sign light sources, allowing upward transmission of light
- LED, animated, flashing, blinking, and video signs
- Plastic box signs
- Generic, un-stylized signs
- Large temporary signage located in windows that reduces transparency
- Large-scale street-level signage that overwhelms the pedestrian environment
- Signage that obscures architectural details
- Glaring and directed spotlights that affects neighboring properties

ENTERTAINMENT DISTRICT SIGNAGE

Since the area around the Rhythm City Casino is planned for entertainment purposes, sign design and guidelines may be relaxed to create an exciting sense of place. Signs in this area may be taller along the interstate corridors, contain additional illumination, and may have moving components to advertise this district and its activities.



SITE DESIGN GUIDELINES

PARKING LOTS - DESIGN AND LOCATION

Guideline: Parking lots shall be located behind buildings or interior of sites whenever possible and contain landscaping to buffer views from public ways.

- Parking lots shall be designed to allow for ease of vehicular circulation and meet all parking lot requirements set in the City Zoning Code
- Locating parking lots at corners shall be avoided, when possible
- Safe pedestrian ways shall be provided between parking spaces and building entrances
- Canopy trees shall be planted at a minimum of every 12 parking stalls to reduce heat islands
- Edge landscaping shall be installed to screen parking lots from public streets
- Planting areas shall be incorporated into landscape islands and planting areas between parking bays
- Bio-swales and stormwater best management practices shall be used to collect and infiltrate stormwater run off
- Parking lot lights shall be 18-25 feet in height along pedestrian ways and edges
- Tall pole lights may be installed in parking lot internal bays with a maximum height of 30 feet
- Site lighting along pedestrian walkways is encouraged in bollards to provide safe lighting, but not over-illumination
- Lighting shall cast downward and be full cut-off
- Illumination levels shall not exceed 0.5 foot candles at the property line
- Businesses shall provide bicycle parking
- Short-term bicycle parking areas shall be provided within 20 feet of each commercial primary building entrance when possible
- Long-term bicycle parking areas shall be provided within 100 feet of each commercial primary building entrance when possible
- One bicycle parking space per 2,000 gross square feet shall be provided for retail uses. A minimum of two spaces is required
- One bicycle parking space per 10,000 gross square feet shall be provided for office. A minimum of two spaces is required
- A minimum of one bicycle parking spaces per unit shall be provided. In-building or covered bicycle parking is encouraged



Inappropriate Parking Lot Design

- Parking lots located between buildings and public streets
- Parking areas located on corners
- Individual parking lots for each development/business
- Numerous access drives into disconnected parking areas
- No parking lot landscaping
- No bicycle parking

PARKING MANAGEMENT

Guideline: Connect parking lots between developments and share parking between uses to minimize large parking lots and automobile traffic on public streets.

- Parking management shall promote effective economic activity, maximize the sharing of parking areas for commercial activity, and avoid inefficiencies in parking in which parking occupancy patterns discourage higher density and compact development
- Shared parking strategies are encouraged to eliminate redundant and unnecessarily large parking areas and to provide more land for taxable development. Parking lots are encouraged to be shared between building uses that require parking at separate times such as office parking during the day and residential parking at night
- New uses shall be required to produce a parking design and management study which documents the level of use at regular and peak times, the degree of sharing for parking with nearby users, the way in which the owner/user intends to maximize efficient use, and sharing of the parking
- Cross access and/or joint parking easements may need to be developed in the event of the parcel being subdivided.
- Shared driveways shall be used to serve groups of buildings with shared parking courts between buildings
- Sharing arrangements shall be codified in covenants, deed restrictions, and developer agreements
- Connecting parking lots between development lots is encouraged to reduce traffic congestion on public streets

Inappropriate Parking Management

- Individual parking lots by building, use, or development project
- Parking lots that are overbuilt and unused



SITE CIRCULATION

Guideline: A pedestrian friendly environment is encouraged to provide safe circulation within the site and between sites. Private development shall connect into existing public infrastructure.

- Concrete pedestrian walkways shall connect parking areas to building entrances
- Pedestrian walkways shall connect development sites to existing public sidewalks and paths such as the Urban Greenway
- Walkways shall be located outside of vehicle parking areas
- Walkways shall be safely illuminated

Inappropriate Site Circulation

- Unpaved pedestrian walkways
- Parking lots with no pedestrian walkways
- Dark, unlit walkways



DRIVE-THROUGH AND SERVICE

Guideline: Drive-throughs shall be located at the rear or side of buildings and shall not be placed between a public street/easement and the main building structure.

- Every effort shall be made to coordinate and integrate drive-through facilities into the overall architectural treatment of the main building
- Creative design solutions such as remote kiosks are encouraged to minimize the impact of the drive-through facility on the overall site design
- Clearly defined pedestrian crossings shall be provided where walkways intersect drive-through access lanes. In all cases drive-through facilities shall be designed to operate without endangering the public safety

Inappropriate Drive-Through

- Drive-through window and service line visible from public streets



SERVICE, STORAGE, AND UTILITY AREAS

Guideline: Service, loading, and utility areas, while necessary for business operation, shall be located away from the public view.

- Refuse and recycling areas shall be integrated into the building architecture with high quality design and/or screened with similar building materials to the main architecture. The areas shall be kept clean and avoid accumulation of excess disposal materials and the enclosure shall be locked
- When possible, service and utility areas shall be inside the building or integrated into the architecture of the building
- Loading areas shall be located behind buildings, away from parking areas and public streets
- Loading doors shall not be visible from public streets
- To reduce the amount of area dedicated to these needs, shared service areas between adjacent users and buildings shall be allowed and encouraged
- If indoor reuse is not possible, outdoor refuse areas shall be enclosed with attractive design and materials that match building architecture. Recommended enclosures would be of masonry or stone with gated access and surrounding landscaping. The gate and enclosure opening shall not be visible from public ways
- Rooftop mechanicals shall be screened with materials that match the building architecture

Inappropriate Service Area

- Service areas and utility equipment along public streets
- Prefabricated storage sheds
- Visible garbage dumpsters
- Unscreened service, storage, or utility areas



LANDSCAPING

Guideline: Create pedestrian-friendly landscapes that are functional, visually appealing, and relate to building activities and architecture. Use landscaping to screen undesirable views.

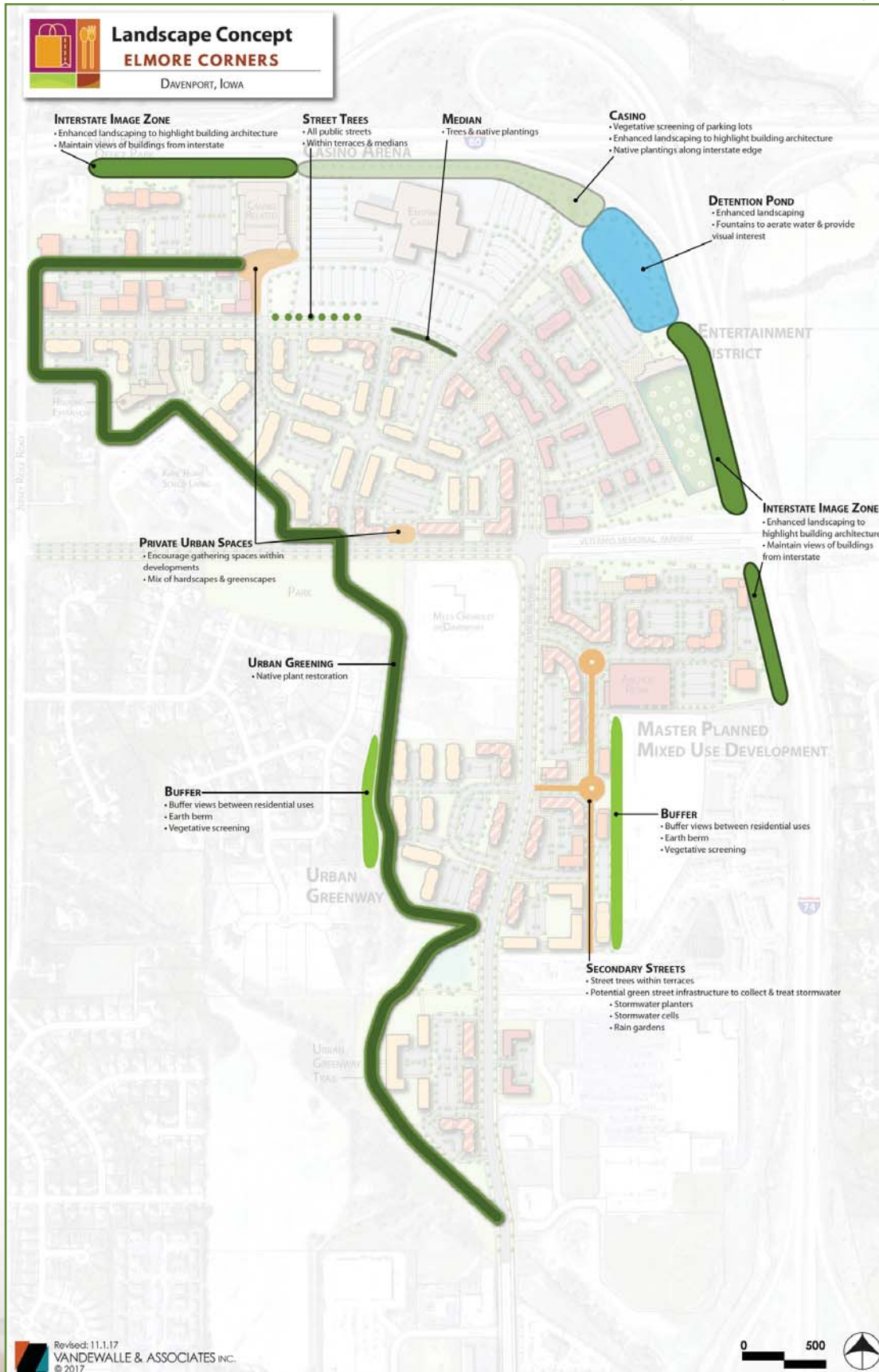
- Incorporate landscape elements that complement the character of the building and provide a pleasing relationship with adjoining properties, the public sidewalk, and the street
- Use appropriate landscape elements to establish continuity between buildings and to define the block face where there are no buildings
- Plant shade trees in surface parking lots to reduce heat islands
- Incorporate mid-level plantings and ground covers into parking planting areas
- Native plants shall be installed whenever possible
- Consider prairie plantings instead of turf to minimize maintenance and establish animal and insect habitat
- Use plant materials that provide year-round interest, add desirable color, texture, and smells to a site's experience
- Install plants to screen parking areas from public streets.
- Install plants to screen building mechanicals
- Incorporate rain gardens and bioretention basins to collect runoff and filter pollutants
- Install bioretention areas in surface parking lots for stormwater infiltration
- Use plant materials that are compatible with urban environments and will maintain their health with the expected amount of care
- Use salt tolerant plants due to the urban pedestrian environment
- See Map 7 for a district-wide landscape concept

Inappropriate Site Landscaping

- Single species planting schemes
- Residential or suburban landscape treatment such as boulders, bark chips, plastic edging, railroad ties, etc.
- Chain link, metal mesh, wire or barbed wire fencing
- Site furnishings located adjacent to common spaces



Map 7. Landscape Concept



URBAN GREENWAY

Guideline: An urban greenway shall be established on the west edge of the planning area to both buffer new development from existing neighborhoods and provide an open space corridor for recreation and natural habitat.

- The greenway corridor shall be a 100 foot wide minimum public corridor
- Naturalized landscaping shall be installed to establish the natural space and buffer
- Low-maintenance native species including trees, shrubs and grasses shall be planted
- A 10 foot wide asphalt path shall be constructed to provide an alternative transportation route and recreation trail
- The path shall connect to private development, public streets, and sidewalks



SITE FEATURES & URBAN SPACES

Guideline: Private development is encouraged to establish urban spaces and install site features to enhance the Elmore Corners District and contribute to a pedestrian friendly streetscape.

- Commercial uses are encouraged to have public plaza spaces to provide space for activities such as outdoor dining or seating areas
- Commercial urban spaces shall be accessible from public ways
- Any security fencing shall consist of decorative materials (such as wrought iron, brick or stone)
- Select and locate site furnishings (benches, trash receptacles, bicycle racks, etc.) to unify the Elmore Corners District and provide a pleasing relationship with adjoining properties, the public sidewalk, and street
- Site furnishings shall be of high-quality materials that last in all weather environments

Inappropriate Site Features & Urban Spaces

- Urban gathering spaces that are closed off from public ways
- Site features of low quality that do not complement building architecture



PUBLIC STREETS AND STREETSCAPING

Guideline: Public streets shall be designed as complete streets serving all modes of transportation users including vehicles, bicycles, and pedestrians. Streets shall contain streetscape elements to enhance the visual aesthetics of these public spaces.

- Primary public streets shall contain streetscape elements of a vehicular scale including:
 - Street lights – tall poles to safely illuminate the street
 - Sidewalk on one side and bicycle path on the other side
 - Turf terraces
 - District gateway features
 - Wayfinding sign directing visitors to district destinations
 - Light pole banners advertising special events
- Secondary streets shall contain streetscape elements of a pedestrian scale including:
 - Street lights – pedestrian scale with decorative fixtures
 - Concrete sidewalks on both sides of the street
 - Decorative brick, paver, or stamped concrete terraces
 - Wayfinding signs or kiosks directing visitors to district locations
 - Light pole banners advertising locations, uses, or events
 - Landscape planters with flowers and grasses
- All street lights shall contain LED energy efficient fixtures
- Street lights shall contain decorative poles and bases
- Street lights shall be down casting to reduce light pollution
- Stormwater management could be incorporated into the street terrace through stormwater planters, inlets, and swales
- Secondary streets may designate bicycle access through painted sharrows on street lanes



STORMWATER MANAGEMENT

Guideline: All future developments must meet City Stormwater Ordinance requirements. In general, stormwater best management practices are encouraged and shall be incorporated into site designs, limiting large, monolithic retention ponds.

- Incorporate stormwater systems into development site plans and landscape plans
- Encourage regional ponds or ponds that serve multiple developments to density development and allow development to occur in urban form
- Utilize stormwater management techniques that maximize infiltration and filter runoff
- Incorporate bio-swales into parking lots to collect surface water and infiltrate on-site
- Install urban stormwater management systems where appropriate such as stormwater planters, rain gardens, underground cisterns and cells, porous pavement, and green street terraces
- Green roofs are encouraged to aid in reducing the amount of impervious surfaces
- Collect roof water into “purple pipe” grey water systems to reuse for irrigation, toilets or other non-potable uses
- If water does run off buildings, direct it into planting areas and rain barrels
- Include stormwater pond aeration systems and fountains to improve water quality and appearance

Inappropriate Stormwater Management

- Direct stormwater runoff into the Pheasant Creek
- Large, unnaturally shaped stormwater detention ponds with no landscaping or aeration



HIGHWAY CORRIDOR OVERLAY DISTRICT

This planning area lies within the City of Davenport Highway Corridor Overlay District. Project proposed within this area must meet the requirements set in the City Zoning Code.

PREFERRED LAND USES

Table 1: Elmore Corners Preferred Land Uses lists the currently allowable uses within the planning area based on the underlying Planned Development District Zoning. The chart indicates the preferred uses by area based on the Elmore Corners Site Layout Plan vision. The area numbers correspond with the following subareas (see Map 4. Sub Areas on page 19).

- Area 1 - North End
- Area 2 - Elmore Corners Mixed-Use Neighborhood
- Area 3 - Elmore Corners Entertainment District
- Area 4 - Master Planned Development
- Area 5 - Elmore Corridor

Table 1. Elmore Corners Preferred Land Uses

Elmore Corners Preferred Land Uses					
Area	1	2	3	4	5
Planned Development District					
Principal Uses					
Banking facilities	Y	Y	Y	Y	Y
Business parks	Y	N	N	N	N
Hotel and motels	Y	N	Y	Y	N
Indoor recreation uses	Y	N	Y	N	N
Indoor recreation uses and outdoor uses that relate to other activities developed on the site	Y	N	Y	N	N
Medical, dental, research testing, and experimental laboratories	Y	Y	N	Y	Y
Offices	Y	Y	N	Y	Y
Restaurants and food services	Y	Y	Y	Y	Y
Retail sales of merchandise	Y	Y	Y	Y	Y
Retail sales that do not include vehicle sales or rental, or public garages	Y	Y	Y	Y	Y
Condominiums	Y	Y	N	Y	Y
Independent, assisted and congregate care elderly housing	Y	Y	N	N	Y
Apartments	Y	Y	N	Y	Y
Row houses	Y	Y	N	Y	Y
Single-family attached	Y	Y	N	Y	Y
Single-family detached	N	N	N	N	N
Warehouse distribution uses (provided that all storage activities are carried out completely within building)	N	N	N	N	N
Other Uses Not Listed in Planned Development Districts					
Grocery store and supermarkets	N	N	N	Y	N
Outdoor storage	N	N	N	N	N
Large format retail over 40,000 square feet	N	N	Y	Y	N
Legend					
Y	Preferred				
N	Discouraged				





**ELMORE
CORNERS**

Master Plan & Design Guidelines

2nd Public Meeting February 22, 2018

The City recognizes the unique and strategic opportunity North Elmore presents and is seeking a proactive approach to guide development within the corridor for years to come.

Why Plan?



Not a rezoning effort! Zoning already in place
Provides a vision of what is possible, not will be
Will be used for marketing purposes
**Illustrations will be used to frame specific
development discussions in the future**
**Design Standards will be applied to individual
projects to ensure quality, compatible
development**


Master Plan



**Land Use Concept
Site Layout Plan
Branding and Marketing Concepts
Development of Draft Design Standards
Public Realm Improvements**

Master Plan Components



A collage of various food items including bread, vegetables, and meat.

This effort does not target or recruit specific national retailers to Davenport. Retail marketing is very scientific and sites are selected on the basis of buying power, anticipated sales and growth potential. However, many national retailers are influenced by a municipality's effort to ensure quality development through planning efforts such as this.

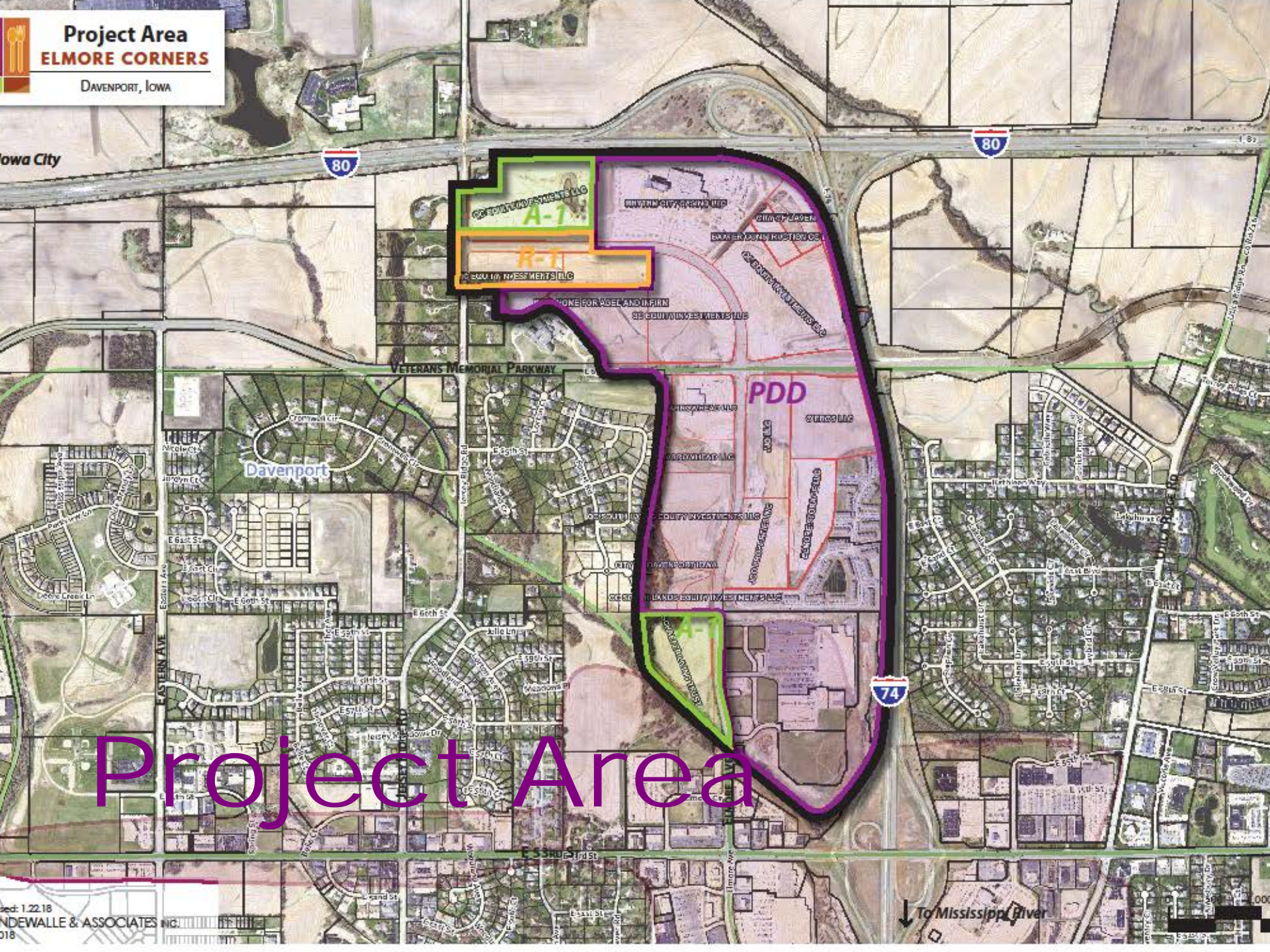
Some retailers, however, monitor social media and listen to direct pleas by consumers wanting a certain store or restaurant in their town!



Project Area ELMORE CORNERS

DAVENPORT, IOWA

Iowa City



Project Area

- Region gateway
- Landscaping
- Welcome feature
- Enhanced landscape treatment & site design requirements

← To Iowa City



Greenway Expansion/Buffer & Trail Connections

Transition Sites/Office/Residential Lower Intensity

Entertainment Related Commercial
(Cross-use casino parking lots)

Entertainment District Sign

Image Implementation Zone (Existing Development)

- Mississippi River gateway branding on casino walls
- Landscape & image enhancements
- Work with property owners

Stormwater Pond Enhancements

- Water feature
- Landscaping

Entertainment Related Commercial
(Cross-use casino parking lots)

Destination Regional Retail or Entertainment

Image Implementation Zone (New Development)

- Four-sided architecture
- Landscape enhancements

Master Developer Planned Site

- Opportunity for master planned destination development
- Commercial, retail & potentially residential

Image Implementation Zone (Existing Development)

- Landscape & image enhancements
- Work with property owners

Secondary District Signage Feature

The Crossing

- Future primary community circulation route
- Future east-west increased traffic to support district

Greenway Expansion/Buffer

- Berm
- Landscape enhancements

Mixed Use

- Retail on Elmore Ave with potential residential above
- Market rate residential along the greenway
- Maximize tax base & TID increment through increased density

Urban Greenway

- Continuous natural linear open space with social, environmental and economic benefits
- Improves livability of the community
- Provides trail connections and alternative transportation routes
- Creates wildlife corridors for migration, shelter and food sources
- Increases property values and attracts development

District Streetscape Enhancements

Gateway Signage & Wayfinding

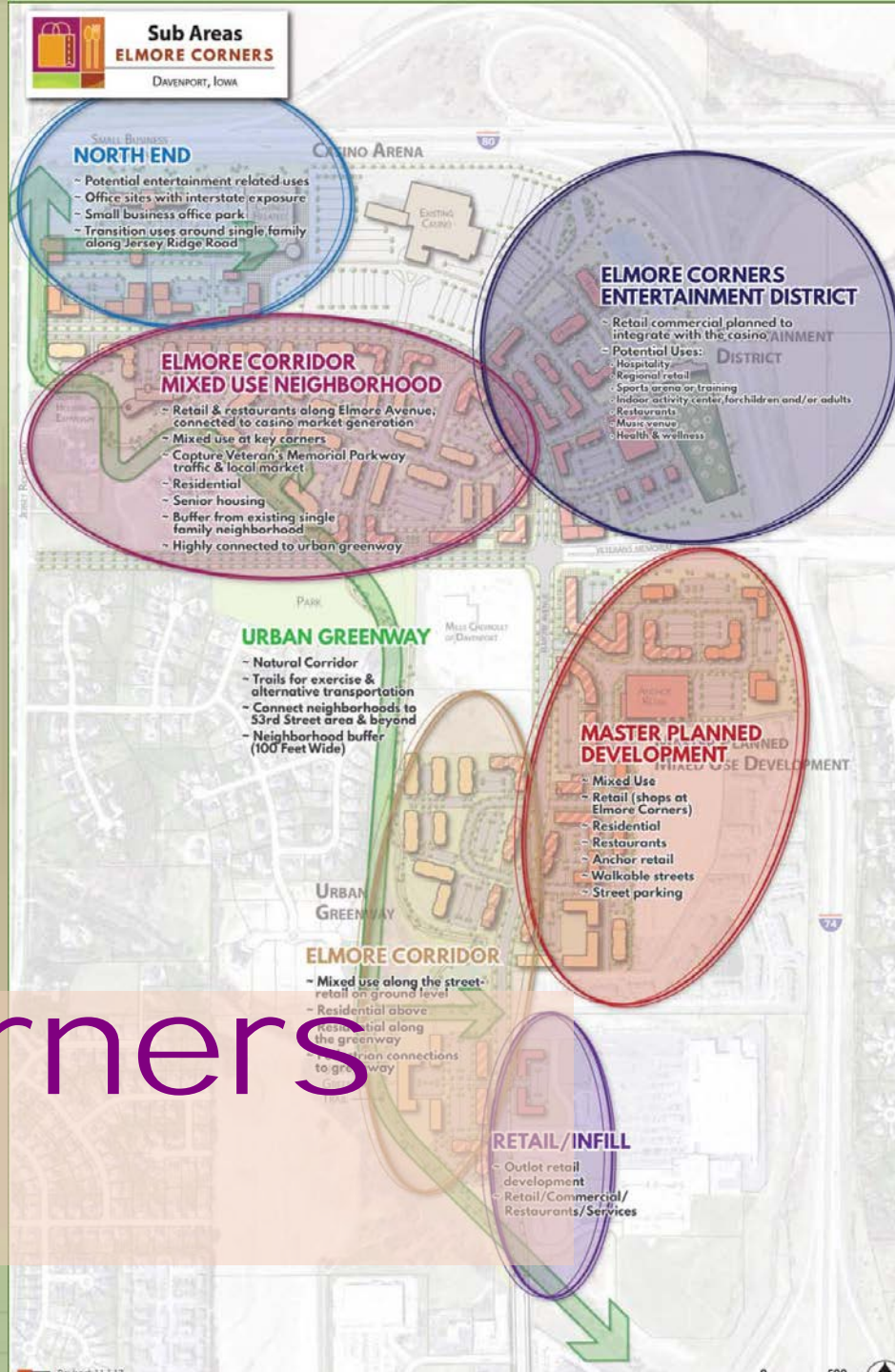
SINGLE FAMILY RESIDENTIAL

TARGET

↓ To Mississippi River

Concept Plan

Elmore Corners Sub Areas



CASINO ARENA

EXISTING CASINO

ENTERTAINMENT
DISTRICT

Site Layout Plans – Entertainment District

VETERANS MEMORIAL PARKWAY





KAHL HOME
SENIOR LIVING

Mixed Use Neighborhood

Master Planned Mixed Use Development



SMALL BUSINESS
OFFICE PARK

CASINO ARENA

CASINO
RELATED
ENTERTAINMENT

North
End

SENIOR

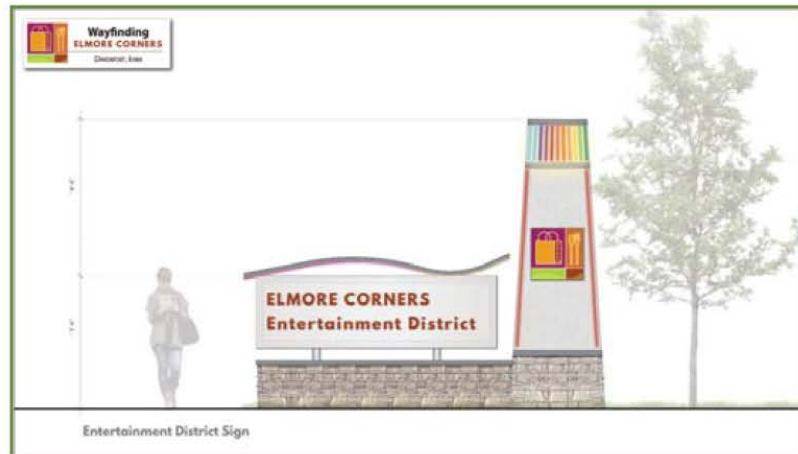
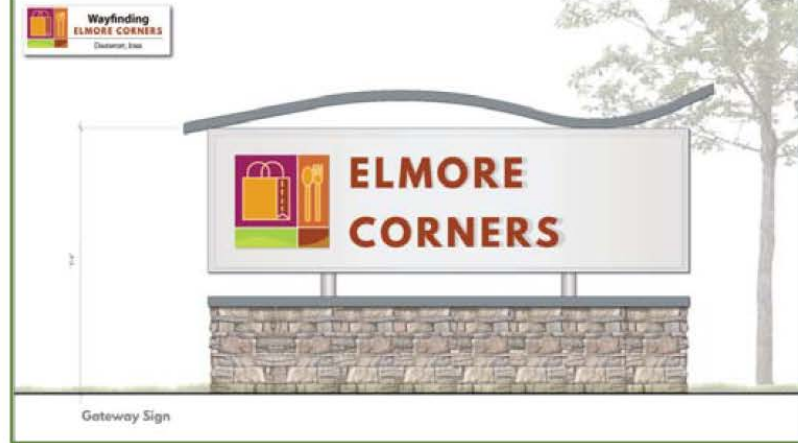


Elmore Avenue





Branding and Marketing Concepts



**Proposed to Ensure Quality
Lasting Materials
Anti-Monotony
No Corporate Architecture
Good site planning and coordination
between individual developments**

**Design
Standards**





Building Composition

Base, Middle, Top

Rhythm

Scale

Massing

Proportion

Facade

Architectural Form

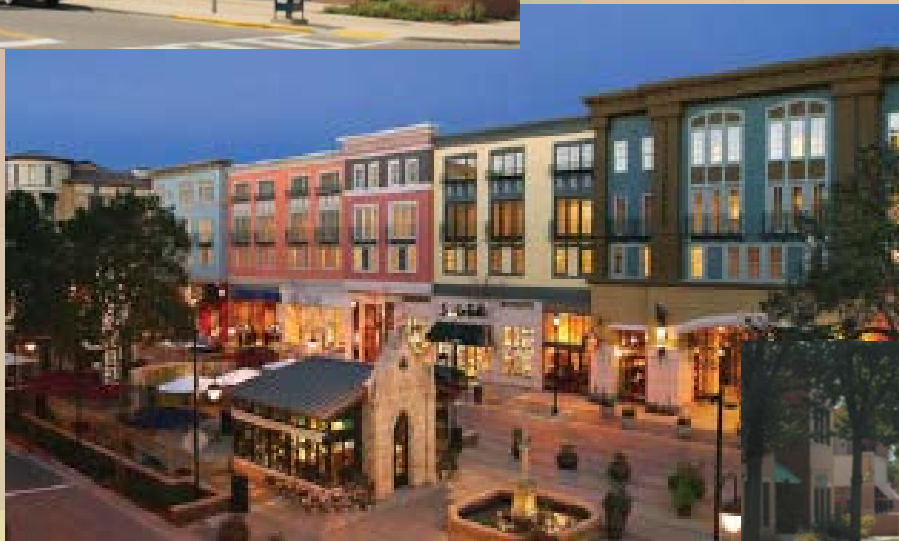


Design
Standards -
Architectural





Building Orientation
Setbacks
Density/Massing/Height



Design
Standards-
Site Planning



**Quality
Materials
Interest
Complimentary to Building's
Architecture
Designed to Enhance
the Experience**



**Design
Standards-
Signage**





**Design
Location
Integration into Site
Parking Management
Site Circulation
Drive-Throughs
Service and Utility Areas**

**Design
Standards-
Parking**



**Screening
Enhance Architecture
Environmental Benefits
Compliment Pedestrian
Experience**



**Design
Standards-
Landscaping**



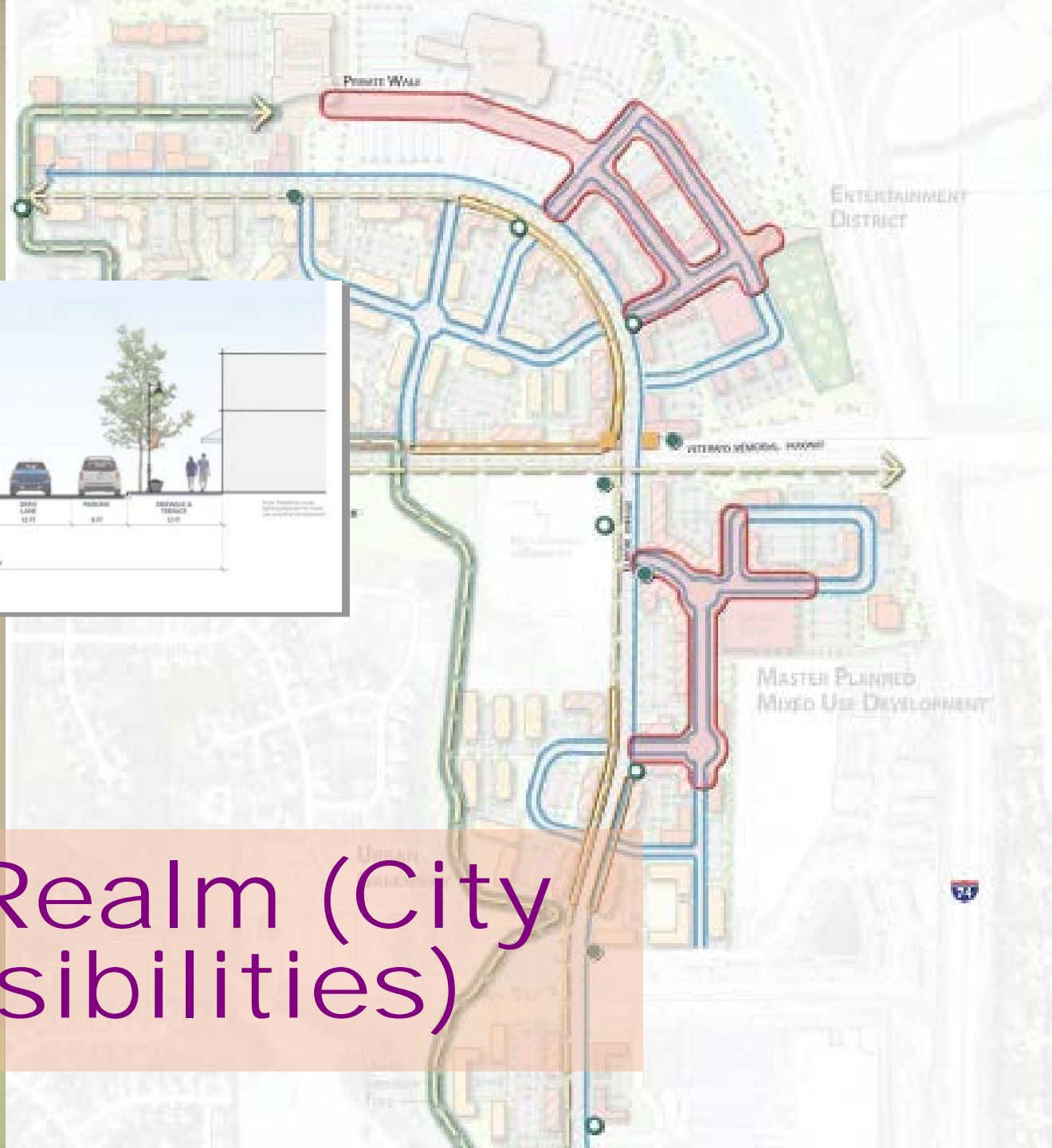


**Ensures good site planning and
coordination between individual
developments**
Streetscaping, Lighting
Public Amenities – Wayfinding
Signage, Trails, etc.

Design
Standards-Public
Spaces



Public Realm (City Responsibilities)



Draft Design Standards Ordinance

P&Z Public Hearing 3-20

P&Z Recommendation 4-3

COW Public Hearing 4-18

Council First Reading 4-25

Next Steps



Discussion



City of Davenport
Plan and Zoning Commission

Department: Community Planning and Economic Development
Department
Contact Info: Matt Flynn, 888-2286

Date
3/6/2018

Subject:

Case No. ORD18-01: Proposed Amendment to Title 17.41 of Davenport Municipal Code (Zoning) creating an Elmore Corners Overlay District and establishing design standards for development within the district [6th Ward]

Recommendation:

Staff recommends the Plan Commission forward the case to the City Council for approval.

Relationship to Goals:

Welcome Investment

Background:

Adoption of Design Guidelines for the area is the key implementation tool of the planning effort. See attachments for additional information.

ATTACHMENTS:

Type	Description
▣ Backup Material	Staff Report
▣ Ordinance	Draft Ordinance

Staff Workflow Reviewers

REVIEWERS:

Department	Reviewer	Action	Date
City Clerk	Wille, Wayne	Approved	3/2/2018 - 12:00 PM



PLAN AND ZONING COMMISSION

Meeting Date: March 6, 2018
Request: Proposed Zoning Ordinance Text Amendment adopting Design Standards for Elmore Corners.
Case No.: ORD18-01
Applicant: City of Davenport

Contact: Matthew G. Flynn, AICP
Senior Planning Manager
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Recommendation:

There is no recommendation at this time.

Introduction:

Case No. ORD18-01: Zoning Ordinance Text Amendment adopting Design Standards for Elmore Corners.

Background:

See Case No. CP18-01 for background information.

The preliminary draft of the design standards is attached.

Public Input:

Public input has been sought throughout the process.

There was a kick off meeting on July 18, 2017, where key stakeholders were interviewed and provided an overview of the project.

On October 25, 2017, the draft plan was presented to the steering committee. Comments were incorporated into the final draft.

On January 22, 2018, a public meeting was held to review the draft plan. Approximately 80 people attended.

On January 29, 2018, the draft plan was discussed at a 6th Ward meeting.

On February 22, 2018, a follow up public meeting was held.

Next steps:

Staff will give a presentation on the Elmore Corners Area Plan and Design Standards at the March 20, 2018 Plan and Zoning Commission Public Hearing.

Chapter 17.41 HCOD HIGHWAY CORRIDOR OVERLAY DISTRICT

Prior ordinance history includes portions of Ordinance No. 91-509 and 97-137.

Sections:

- 17.41.010 Intent.**
- 17.41.020 Applicability.**
- 17.41.030 Use regulations.**
- 17.41.040 Height, yard, lot width and area regulations.**
- 17.41.045 Design review.**
- 17.41.050 Performance standards.**
- 17.41.060 ECOD Elmore Corners Overlay District**

17.41.010 Intent.

This district is intended to establish a physically attractive pattern of development on certain major entrance corridors into the city of Davenport, to protect these areas from the negative effects of incompatible development and to protect the traffic carrying of important highways within the city. (Ord. 2011-471 § 6 (part)).

17.41.020 Applicability.

The regulations set forth in this chapter, or set forth elsewhere in this title when referred to in this chapter, are the regulations in the "HCOD" highway corridor overlay district. (Ord. 2011-471 § 6 (part)).

17.41.030 Use regulations.

Permitted uses shall be all uses allowed in the underlying zoning districts. All future rezonings in the "HCOD" highway corridor overlay district other than single-family residential shall be limited to "PUD" planned development district, "PCP" planned commerce park district, and "M-3" planned industrial district, as these districts allow the degree of review and control necessary to achieve the goals of the "HCOD" highway corridor overlay district and allow a complete range of allowable uses for low density residential to retail to industrial.

This chapter shall not apply to single-family detached homes or single-family subdivisions. (Ord. 2011-471 § 6 (part)).

17.41.040 Height, yard, lot width and area regulations.

All requirements of the underlying zoning districts concerning building height, lot area, and yard depths shall remain applicable with the following exceptions:

A. A twenty-five foot landscape buffer shall be required on all major arterial or minor arterial streets. Parking lot circulation may cross the landscape buffer area but parking may not be developed within the required setback. On all other streets a similar ten foot landscape buffer shall be required. In cases where other provisions of the zoning ordinance requires more restrictive standards, the more restrictive standards shall apply.

B. All immediate structures for non-residential uses shall be set back from any immediately contiguous property used for residential development a minimum of twenty-five feet for a one and one-half story building (maximum height fifteen feet) and forty feet for a two story building (maximum height twenty-five feet). Structures shall be set back an additional one foot for each foot of building height over twenty-five feet to a maximum setback of seventy feet from any immediately contiguous residential property line or residential zone. Property separated by public rights-of-way shall not be considered immediately contiguous property.

The zoning board of adjustment may grant variances in cases where the above setback regulations (subsections A and B) unduly impact the developability of any lot subdivided for development prior to the passage of this chapter. The Board may consider the feasibility of complying with the additional setback requirements based upon all of the following:

1. Existing lot size.
2. Compatibility with adjacent development.
3. Locational and topographical factors. (Ord. 2011-471 § 6 (part)).

17.41.045 Design review.

Prior to the issuance of a building permit for the construction of a building, structure, or sign, as defined in Chapter 17.04, the plans shall be administratively reviewed and approved by the planning and economic director or his/her designee, to ensure that the building, structure, or sign complies with the performance standards and guidelines established in, and pursuant to, Section 17.41.050, **Performance Standards (HCOD)** and Section 17.41.055, **Performance Standards (ECOD)**.

A. Exceptions.

1. Single-family dwellings and accessory structures thereto are not subject to review.
2. The review of public utility structures (except for public utility buildings which are subject to architectural review) shall be limited to requirements for berming and landscaping. Requirements may be extensive and shall include a mix of plant materials having year round effectiveness.
3. Exempted signs, as enumerated in Section 17.45.025 of the zoning ordinance.
4. Temporary signs, which are in compliance with the regulations enumerated Section 17.45.050 of the zoning ordinance.

Notwithstanding the above, the development official, may require the replacement of any of the above signs listed in subsection A, 4 above after one year, if in his or her opinion the sign has become unattractive or deteriorated. Appeals of such administrative decisions shall be made to the zoning board of adjustment within ten days of receipt of the notification of any defect.

B. Authority.

1. As part of the review by the commission of a final development plan, as required by the provisions of Chapter 17.32, "PDD" planned development district, Chapter 17.40, "M-3" planned industrial district, or Chapter 17.50, "PUD" planned unit development, the commission shall review the materials referenced in subsection C, 1 of this section and make recommendations to the commission and the city council relative to the commission and the city council relative to the compliance of the application with the criteria set forth in Section 17.41.050.

2. For other construction subject to the provisions of this chapter but not subject to final development plan approval, the community planning and economic director or his/her designee shall review the materials referenced in subsection C, 1 of this section and approve, conditionally approve, continue or deny any application, basing its decisions on the criteria set forth in Section 17.14.050. Approvals shall be valid for a period of one year from the date of approval. If no building permit has been issued pursuant to the development of the approved project within one year of approval, the approval shall expire.

C. Procedure. In the interests of neighborhood harmony and courtesy, a meeting with adjacent property owners and neighbors is strongly encouraged.

1. Prior to the preparation of final architectural drawings and prior to the issuance of any building permit for any proposal, the applicant shall submit the following information and materials:

- a. The completed application form;
- b. A dimensioned site plan, including the locations of any proposed or existing buildings on the subject parcel and on surrounding parcels, if the buildings are within one hundred feet of the subject parcel;
- c. A verifiable legal description, or a land survey, where deemed necessary by the development official;
- d. A map showing the existing topography of the subject parcel and the topography of other properties at two foot contour intervals, extending one hundred feet from the subject parcel;
- e. A preliminary grading plan showing before and after grades at two foot contour intervals;
- f. A landscape plan (although the authority of staff to approve site plans under Section 17.56 of the Zoning Ordinance remains in effect).
- g. Elevation drawings, in color and drawn to scale, of the front, sides, rear, and roof lines of all proposed buildings or structures, illustrating the appearance and treatment of required screening elements for roof-mounted equipment. Outdoor storage areas that are part of any submittal shall be reflected in the elevation drawings submitted and shall show their relationship to the building elevations as well as the material and treatment proposed that would accurately reflect

the screening of the storage areas. A second set of elevations (not colored) shall be submitted on eight and one-half by eleven inch paper.

h. Samples of each type of exterior building materials, including the color.

i. Samples of the materials, including the color, along with scaled, accurately colored elevations, of any proposed sign and/or sign package.

2. Prior to issuance of a building permit, the development official shall determine that the submitted plans for the building permit are in substantial conformance with those approved as part of the final development plan, as stated in subsection B, 1 of this section, or by the community planning and economic development director or his/her designee, as stated in subsection B, 2 of this section, and that the time period for approvals has not expired. The stamping of the plans and the signature of the development official, and the date of the signature shall indicate that the plans are in conformance.

3. Prior to any external change, including, but not limited to, a remodeling or elevation alteration which includes changes in materials or colors, the property owner or his or her designated representative shall present the change for review to the development official. The development official may approve a sign, a change to the color scheme or an existing building, a building addition or structure, or a revised landscape plan based upon the following criteria:

a. Building addition or structure:

The addition of accessory structure shall not substantially alter the appearance of the site as viewed from off the site; or be visually incompatible with the existing building or structure.

(1) An addition to an existing building or accessory structure of one thousand square feet or less or;

(2) Which shall not increase the area of coverage by more than fifteen percent increase the number of stories or propose substantial changes to building materials. In no case shall increases greater than fifteen percent of the area of the building coverage, increases in the number of stories in a building, or substantial changes in exterior building materials be considered minor.

b. Landscaping:

(1) The quantity, density, and quality of the plant material is increased and upgraded; and

(2) The changes reflect a substantial improvement of the plan.

Exterior changes not meeting the criteria of subsection C, 3a. and b. shall be reviewed pursuant to the provisions of subsection B of this section.

4. Any conditions prescribed through the approval of the final development plan or by the community planning and economic development director or his/her designee shall be considered an integral part of the construction plans. The conditions shall be noted on all plans as may be required to applicable city departments.

5. Any building, structure, or sign which has been approved, constructed, or installed in accordance with the approval of a final development plan or of the community planning and economic development director or his/her designee may be removed. However, it shall not be modified, altered, or changed in any manner without additional review by the development official, who shall decide whether the proposed change shall be reviewed pursuant to the provisions of subsection C of this section.

6. Initiating construction or development prior to the issuance of building permits or noncompliance with approved plans and conditions shall be grounds for either stopping work on the project or denial of a certificate of occupancy.

7. Upon completion, prior to issuance of a certificate of occupancy, the property owner or general contractor shall certify in writing that, to the best of his or her knowledge, the building, structure, and site detail have been completed in accordance with the approved design and applicable codes.

8. Lack of maintenance of any portion of an approved project approved through the approval of a final development plan or by the community planning and economic development director or his/her designee of this code.

D. Appeal.

1. The applicant for design review, may appeal any decision of the community planning and economic development director or his/her designee to the design review board by filing written notice of appeal to the community planning and economic development department within ten working days of the date of the decision. Notice of such appeal and the date for its consideration.

2. The design review board shall base its decision to affirm, modify, or reverse the decision of the community planning and economic development director or his/her designee based on the information and materials previously submitted to the community planning and economic development director or his/her designee.

E. Violations and penalties. It shall be unlawful to construct, erect, install, alter, change, maintain, or permit the construction, erection, installation, alteration, change, maintenance of any building, structure, sign, wall, or landscaping, or use or permit the use of any lot or other land contrary to, or in violation of, any of the provisions of this chapter, and is punishable as set forth in Chapter 17.62 of this code. (Ord. 2011-471 § 6 (part)).

17.41.050 Performance standards.

All uses and buildings permitted within the "HCOD" highway corridor overlay district shall comply in total with the following performance standards:

A. Open space and landscaping requirements. The total ground area devoted to open space for natural landscape and landscape beautification shall not be less than twenty percent on any development site within the "HCOD" highway corridor overlay district. Such open spaces will be free of all drives, parking areas, structures, buildings, except for those walkways, monuments, and ornamental features considered to be necessary and essential to the central landscape theme. In the case of underlying zones which require more restrictive standards, the more restrictive standard shall apply.

The minimum site landscaping requirement shall be as required in Chapter 17.56 entitled "Site Plans." Additional plantings, and larger caliper sizes and heights may be required by the development official to achieve the goals of this chapter. This landscaping shall be designed to minimize the adverse effects of long expanses of wall, exposed parking, and service areas.

Tree planting shall include a combination of evergreen and deciduous trees and shrubs. In the required front yard, canopy trees shall have a minimum caliper of two and one-half inches, multi-stem clumps shall have a minimum height of ten feet, understory trees shall have a minimum height of five feet, deciduous shrubs shall have a minimum height of twenty-four inches, and evergreen shrubs shall have a minimum height of eighteen inches.

"Caliper" shall be defined according to the standards of the American Association of Nurserymen.

Where fences are necessary for commercial projects, masonry, wrought iron/aluminum, moisture resistant wood (other than stockade) and hedges are encouraged. If visible from a public street, galvanized chain link fences are not to be allowed unless complemented by continuous tall shrubbery completely obscuring the fence.

B. Architectural standards. Buildings within the "HCOD" highway corridor overlay district shall be constructed of quality materials that have strength and permanence. Permanence means that buildings will age without deteriorating, given a minimum level of maintenance. The development shall recognize the strength and permanency of stone, brick, concrete, and steel as opposed to the frailty of light gauge sheet metal and constant maintenance of wood veneer. While no specific materials are disallowed, certain materials will be approved for exterior use only if they are an integral part of a design of unusual merit. Among those materials are architectural metal wall panels, plywood and composition siding such as hardboard. Other materials may be appropriate in one use or situation but be totally inappropriate in another.

The city council shall establish design guidelines to help guide the decisions by the design review board, city staff and to aid petitioners with design considerations.

C. Restricted uses. There are some uses that require additional restrictions within the "HCOD" highway corridor overlay district to lessen their negative impact and make them more compatible with the goals of this chapter. The following uses are restricted:

1. Automobile salesroom or showroom, or new or used car sales or vehicle storage lots. Design principles shall be incorporated into the design of auto dealerships which minimize the unattractive aspects of large areas of parked cars. One-car-deep display areas along arterial and collector streets and freeways shall be permitted. Display rows shall be screened from public rights-of-way with an eighteen inch earth berm. Additional rows of cars shall be separated from the initial row by a shrubbery hedge that completely conceals the view of these additional rows of cars from the public right-of-way. Five percent of the balance of the parking lot shall be landscaped and permeable in the form of islands and peninsulas. Signs applied to, located in, or painted onto vehicle bodies or windshields on cars in the display area are prohibited with the exception of factory invoices. The use of pennants and tinsel are prohibited. Elevated or tilted vehicle display platforms are prohibited. An earth berm or other form of landscape buffer shall be located around the sides and

rear of the property to shield the view of stored cars from adjoining residences and streets, if any. Berming and landscaping may be required to shield the view from commercial areas depending on sight lines. Lighting design shall be integrated with the design of the dealership. Non-glare, cut off luminaries shall be required so that lighting does not spill over onto adjacent properties. The leasing or renting of trucks intended for short term lease of less than seven days shall be prohibited either as a primary or an accessory use.

2. Public garages, service stations, car washes, repair shops, auto parts installation shops, auto service malls, and other similar uses. Building forms and materials shall emphasize masonry and be compatible with nearby retail buildings. Canopies over pumps and their supports shall have no lettering, logos, or striping, and shall be integrated with the building design. The use of pennants and tinsel are prohibited. Car wash structures shall be a minimum of fifty feet from any residential property line or residential zone. Outdoor storage shall be confined to areas enclosed by masonry walls. Stacks of tires, oil cans, and promotional items are not permitted. Garage bays shall be concealed from view from adjacent streets and parking lots whenever possible. The leasing or renting of trucks intended for short term lease of less than seven days shall be prohibited either as a primary or an accessory use.

3. Outdoor retailing. Outdoor retailing areas, such as garden centers, shall be orderly and attractive, with low screening walls concealing merchandise, except plant materials, from view. Additional landscaping similar to at least a "G" buffer as shown in Chapter 17.56 entitled "Site Plans" and berming may be required with such uses.

4. Contractor's yards and other similar uses involving the outside storage of materials and/or vehicles and equipment. Contractor's yards and other similar uses shall be orderly with screening walls, berms, and landscape buffers similar to at least a "G" buffer as shown in Chapter 17.56 entitled "Site Plans" to completely screen them from adjacent properties and public roads.

5. Outdoor vending machines. For the purposes of this chapter, outdoor vending machines shall also be considered outdoor retailing. Such vending machines shall be screened from public rights-of-way.

D. Environmental performance standards. Complete abatement, elimination, or reduction of all generally offensive characteristics such as odors, gases, noise, vibration, pollution of air or water or soil, excessive lighting intensity, hazardous activity, etc. which may be detrimental to the general welfare of the community shall be accomplished. The standards for compliance with this paragraph shall be as follows:

1. Air pollution. The use shall not emit any smoke, dust, odorous gases, or other matter in such quantities as to be readily detectable at any point beyond the perimeter of the site area.

2. Noise. Davenport Municipal Code, Chapter 8.16, "Noise Abatement," shall be used to regulate noise within the "HCOD" highway corridor overlay district.

3. Vibration. The use shall not include vibration which is discernible without instruments on any adjoining lot or property.

4. Site lighting and glare. All lights, other than street lights, shall be so situated and installed as to reflect away from adjacent streets and residential areas. Light and pole designs shall be compatible with the overall design of the building(s) and site. In large parking lots, poles are to be located within the landscape islands. The scale of the parking area shall be considered in the selection of pole height and spacing. Pole height shall decrease, where possible, near property lines and in low intensity use areas to further reduce glare. Uplighting trees and fountains, accent lighting on shrubs and entrances, and silhouette lighting may be used to create special effects.

5. Traffic hazard. The use shall not involve any activity substantially increasing the movement of traffic on public streets to a point that capacity and safety limitations are exceeded unless procedures are instituted to limit traffic hazards and congestion.

6. Storm water detention. Davenport Municipal Code, Chapter 13.34, "Storm Water Management" shall be used to regulate storm water run-off and provide detention where required.

7. Erosion control. Davenport Municipal Code 13.34.310–360, "Erosion Control" shall be used to regulate erosion on development sites. All development with a disturbed site in excess of five acres shall submit a copy of State of Iowa Department of Natural Resources Permit Number 2 with accompanying plans. Similarly, sites less than five acres shall submit a copy of the permit and plans if they are part of a larger development, the whole of which exceeds five acres. On sites less than five acres, an erosion control plan shall be required if, in the opinion of the city engineer, it is necessary to protect off-site properties. An approved grading plan shall be required prior to the commencement of any grading.

8. Overtax public utilities and facilities. The use shall not involve any activity substantially increasing the burden on any public facilities or utilities, unless provisions are made for necessary adjustments.

9. All utilities provided as new installation shall be underground and comply in full with the regulations set forth in Section 16.24.110 of the city of Davenport's Municipal Code entitled "Subdivision."

F. Off-street parking and loading. The required number of spaces shall remain the same as that of the underlying zoning districts. In all cases, parking lots created in the "HCOD" highway corridor overlay district shall be constructed with concrete or asphalt curbing. In all cases parking areas shall be screened from any public right-of-way or adjacent residential area by proper siting, the use of earth berms and vegetation. Earth berms shall be a minimum of three feet in height (with the exception of those on automobile sales display rows) and shall be supplemented with substantially mature trees and shrubs which have year-round effectiveness. Service areas, loading docks, and garbage facilities shall be located properly and screened as well. Landscaped parking islands and peninsulas shall be included in lot layout to reduce the visual impact and produce shade. In parking lots of twenty or more spaces, an area equal to five percent of the total parking lot area must be landscaped and permeable. This shall not include perimeter plantings. This requirement may be waived in the case of industrial properties where interior landscaping may interfere with the necessary movement of trucks and other equipment.

G. Signage requirements. Signage shall be consistent with the central aesthetic theme of the "HCOD" highway corridor overlay district; sign materials shall be permanent or long lasting in quality. The lighting of signs shall be non-intrusive with back lighting or other methods of indirect lighting required.

Refer to Chapter 17.45 of the city of Davenport's zoning ordinance entitled, "Sign Regulations" Section 17.45.090 K. for a complete description of sign regulations in the "HCOD" highway corridor overlay district. \

17.41.060 Elmore Corners Overlay District (ECOD)

A. Administration

1. Purpose

2. Subject Area. Attached Exhibit "A" depicts the area subject to the ECOD standards.

3. Design Standards. Prior to issuance of a building or development permit involving site alterations, new construction, or exterior change to a building or sign, the following standards must be met. Unless otherwise noted, the standards contained herein are mandatory.

a. Building Composition

(1)Guideline: Buildings shall be of a quality design that contribute to the Elmore Corners destination appearance.

(2)Base, Middle, Top

Building facades shall be composed to define base, middle and top elements. The base of the building anchors it to the ground and is the interface between the building and people. This element shall be the most articulated. The transition between the middle of the building and the base and top shall be varied by use of materials, composition, repetition or ornamental features. The top terminates the building against the sky and provides an opportunity to create an interesting silhouette.

(3)Rhythm

Rhythm refers to the regular or harmonious recurrence of building elements. Rhythmic elements need not be boring, but can provide the visual "glue" that links together various buildings on a street. These patterns commonly reflect the building's repetitive structural bays, often with the end bays given special identity. This articulation of the facade helps provide scale by comprising the facade into smaller visual parts. The variation of rhythm from building to building reaffirms the individuality of each building, while the recurrence of an overall rhythm helps unify the facade. The building's structural column lines shall in some way be evident or expressed on its facade.

(4)Scale

In general, distinctive compositional elements of buildings (entries, structural bays, roof elements, windows) shall be distinguishable from both near and far distances. The size and shape of these elements shall give the building scale to relate to pedestrians as well as surrounding buildings. Special attention shall be paid to overall building height and massing in order to maintain a scale and character compatible with the area.

(5) Massing

Large buildings shall be comprised of a hierarchy of masses and forms to give the building a more human scale and visual richness. Techniques include using designed recesses or projections, creating distinct building components, and varying occasional roof forms according to individual building components. However, excessive use of changes in form can create a decidedly confused street façade and disharmony.

(6) Proportion

Building massing and components shall demonstrate consistent proportional harmonies. Typically, pedestrian friendly streets have building elements that rely more on vertically proportioned elements than horizontally proportioned elements. While no architectural rule is always valid, the use of vertically proportioned facade components seems to offer a traditionally street-friendly composition (regardless of specific architectural styles). The use of proportion is intended to provide a sense of visual harmony among elements of a building.

(7) Architectural Form

- Buildings shall be designed to create a pedestrian friendly and scale neighborhood
- Buildings shall be primarily rectangular in form and sited parallel to public streets
- Buildings are encouraged to have a mix of tenants and uses
- Entrances shall be provided at public streets and on parking sides
- Connect building entrances to public sidewalks and pedestrian pathways
- Articulate building shapes with window bays, cornices, parapets, and towers
- Buildings shall have primarily flat roofs with varied or broken lines
- Roof mechanicals shall be screened
- Locate service areas away from public streets and provide architectural screening
- Drive through facilities shall be located on rear building façades as to not dominate the streetscape
- Modify franchise architecture to fit the desired Elmore Corners character

(8) Inappropriate Design

- Flat, massive, monolithic geometric shapes
- Plain, undersigned buildings with large blank spaces
- Small buildings with single occupants
- Gabled, hipped, or steeply pitched roofs
- Large, monotonous roof forms
- Visible building mechanicals
- Faux-historic buildings

b. Building Facade

(1) Guideline: Buildings shall have varied facades composed of high quality lasting materials.

- Façades shall be articulated on all sides to provide visual interest.
- Break up façades and create visual interest and pedestrian scale by:
 - Varying materials and colors
 - Varying façade vertical and horizontal setbacks
 - Incorporate architectural reveals or floor step backs
 - Vary window styles and moldings
 - Design enhanced architectural details on the ground level

- Façades on multiple-tenant buildings shall be broken into design elements that reflect individual tenant spaces through vertical façade plain staggering, window/door groupings, and awnings
- Differentiate ground and above floors through horizontal banding, signage bands, and windows
- First floor façades shall be transparent to allow retail uses to interact with the street and parking-side entrances
- Commercial and retail façades shall have at least 40% transparency on the street façade
- Secondary façades that do not face the public street or parking areas are encouraged to have transparent windows. If this is not possible, then façades shall contain architectural features that provide interest
- Building at corners shall contain interesting architectural features that address the corner such as towers, entrances, and metal or cloth awnings
- Identify building entrances through permanent awnings, overhangs, or signage

(2) Inappropriate Façade Treatment

- Large unarticulated wall planes
- Large expanses of walls without windows
- Projecting wall-mounted mechanical units
- Vinyl awnings
- Buildings with no pedestrian scale or defined entryways

(3) Materials

- High quality materials shall be installed on buildings to promote Elmore Corners
- Long lasting materials such as brick and stone shall be used to withstand weather and provide a feeling of permanence
- Primary building materials shall be natural stone, masonry, brick, and glass and be of natural earth tone colors
- Secondary materials such as decorative metal, wood, stucco, and cement board may be incorporated
- Using a mix of materials is recommended to create visual diversity on the building façade
- Enrichment of the pedestrian realm requires building materials at the street level to be of high quality:
 - These could include items such as brick, stone, decorative concrete masonry units, metal panel systems, or other creatively used high quality and durable building materials
 - Decorative finished block systems can be used along the base of the buildings, and as an accent, but are not recommended as the dominant building material on the entire building
 - Utility grade materials shall only be used on facades of the building not visible from publicly accessible areas
- Freestanding commercial structures shall be designed as four-sided architecture with finish grade materials used consistently on all façades:
 - Recommended primary materials include brick, stone and glass
 - Other materials such as precast concrete, decorative concrete block or decorative facade panels may be appropriate if properly detailed and integrated with the architecture
 - Metal and finished wood may be used as accents, but shall not be the primary material for any façade

(4) Inappropriate Materials

- Unfinished exposed concrete block
- Large, unfinished, pre-cast concrete panels
- Unfinished poured-in-place concrete
- Corrugated metal, vinyl, or aluminum siding
- Plywood siding panels
- Exposed treated lumber
- EIFS as a primary material or located on the first floor
- Windows
 - Moderate to highly reflective glazing
 - Strongly colored or darkly-tinted glazing
- Colors
 - Predominant use of bright, high intensity colors
 - Predominant use of metallic and neon colors

c. Setbacks

(1)Guideline: To provide a continuous street edge along the Elmore Corners District, buildings shall be sited within setback areas unless not possible due to affecting business operations.

- Along Elmore Avenue and Veterans Memorial Parkway, the primary building façade shall be located within 20-40 feet from the property line
- Along secondary streets, the primary building façade shall be located 0-10 feet from the property line
- To maintain an urban edge that is pedestrian friendly, areas adjacent to parking lots shall be screened with enhanced landscaping

(2)Inappropriate design

- Buildings set back over 40 feet from public streets

Parking lots between public streets and the primary façade of a building

d. Density

(1) Guideline: Develop land as densely as possible to increase tax base while providing adequate landscaping, stormwater management, and connections to public ways.

- Development shall be designed to an urban density with minimal spaces between buildings
- Install advanced stormwater management practices to increase density
- Density may increase through vertical mixed-use and shared parking arrangements

e. Signage

(1)Guideline: Signage shall function as a marketing device, complement building architecture and materials, and not dominate the site. All signs must meet City Sign Codes.

- Appropriate and attractively designed site signage is an important component of the overall aesthetic appeal of any commercial or mixed-use area. Good signage based on design and graphic quality is encouraged throughout Elmore Corners.
- Business signage shall be wall mounted (projecting or flat), monument (if located away from the building), and window, canopy or awning style
- Similar materials, colors and styles shall be used to ensure the signage is consistent with the building design
- Signs shall reflect and enhance the nature and appeal of the retail and commercial experience and not be just a list of tenants
- Materials shall be of high quality to prevent premature weathering of the sign
- Signs shall fit within and not overwhelm the architectural features of the buildings
- Signs shall harmonize with their surroundings in terms of size, shape, color, texture, and lighting so that they complement the character of the neighborhood
- Signs shall be professionally constructed using high quality materials such as metal, stone, tile, composites, brass/metal plated, hardwood, and glass
- The creative use of materials, lettering, and interesting use of graphics is allowed and encouraged if the signs work well and complement the overall building and street design
- Signs shall be designed with the purpose of promoting retail and street activity while enhancing the pedestrian experience
- Addresses shall be clearly visible from the public right-of-way
- The use of tastefully designed pedestrian scale directories is encouraged for multi-tenant buildings
- Monument signs shall contain a base and not be pole-mounted

- Monument signs shall contain landscaping at the base that will not grow to cover the sign message
- Lighting shall be designed carefully to avoid excessive glare or over illumination:
- Letters can be illuminated internally or externally
- External lighting must be discreet and unobtrusive, and not shine outward into the pedestrian or driver vision path
- Fixtures shall be simple but attractive
- Lighting signs and letters shall be done in an attractive and subtle technique
- Individual letters that are backlit, halo-lit, reverse illumination channel letters, and neon are encouraged

(2) Inappropriate Signage

- Pole signs, billboard style wall-mounted and roof mounted signs are not allowed
- without special permission from the City Plan Commission
- Monument signs advertising individual businesses
- Tall mast signs
- Large temporary signs that impede pedestrian traffic
- Inflatable signs
- Overly illuminated signs
- Unshielded sign light sources, allowing upward transmission of light
- LED, animated, flashing, blinking, and video signs
- Plastic box signs
- Generic, un-stylized signs
- Large temporary signage located in windows that reduces transparency
- Large-scale street-level signage that overwhelms the pedestrian environment
- Signage that obscures architectural details
- Glaring and directed spotlights that affects neighboring properties

(3) Entertainment District Signage

Since the area around the Rhythm City Casino is planned for entertainment purposes, sign design and guidelines may be relaxed to create an exciting sense of place. Signs in this area may be taller along the interstate corridors, contain additional illumination, and may have moving components to advertise this district and its activities.

f. Site Design Guidelines

(1) Parking Lots – Design and Location Guideline: Parking lots shall be located behind buildings or interior of sites whenever possible and contain landscaping to buffer views from public ways.

- Parking lots shall be designed to allow for ease of vehicular circulation and meet all parking lot requirements set in the City Zoning Code
- Locating parking lots at corners shall be avoided, when possible
- Safe pedestrian ways shall be provided between parking spaces and building entrances
- Canopy trees shall be planted at a minimum of every 12 parking stalls to reduce heat islands
- Edge landscaping shall be installed to screen parking lots from public streets
- Planting areas shall be incorporated into landscape islands and planting areas between parking bays
- Bio-swales and stormwater best management practices shall be used to collect and infiltrate stormwater run off
- Parking lot lights shall be 18-25 feet in height along pedestrian ways and edges
- Tall pole lights may be installed in parking lot internal bays with a maximum height of 30 feet
- Site lighting along pedestrian walkways is encouraged in bollards to provide safe lighting, but not over-illumination
- Lighting shall cast downward and be full cut-off
- Illumination levels shall not exceed 0.5 foot candles at the property line
- Businesses shall provide bicycle parking

- Short-term bicycle parking areas shall be provided within 20 feet of each commercial primary building entrance when possible
- Long-term bicycle parking areas shall be provided within 100 feet of each commercial primary building entrance when possible
- One bicycle parking space per 2,000 gross square feet shall be provided for retail uses. A minimum of two spaces is required
- One bicycle parking space per 10,000 gross square feet shall be provided for office. A minimum of two spaces is required
- A minimum of one bicycle parking spaces per residential unit shall be provided. In-building or covered bicycle parking is encouraged

(2) Inappropriate Parking Lot Design

- Parking lots located between buildings and public streets
- Parking areas located on corners
- Individual parking lots for each development/business
- Numerous access drives into disconnected parking areas
- No parking lot landscaping
- No bicycle parking

(3) Parking Management Guideline: Connect parking lots between developments and share parking between uses to minimize large parking lots and automobile traffic on public streets.

- Parking management shall promote effective economic activity, maximize the sharing of parking areas for commercial activity, and avoid inefficiencies in parking in which parking occupancy patterns discourage higher density and compact development
- Shared parking strategies are encouraged to eliminate redundant and unnecessarily large parking areas and to provide more land for taxable development. Parking lots are encouraged to be shared between building uses that require parking at separate times such as office parking during the day and residential parking at night
- New uses shall be required to produce a parking design and management study which documents the level of use at regular and peak times, the degree of sharing for parking with nearby users, the way in which the owner/user intends to maximize efficient use, and sharing of the parking
- Cross access and/or joint parking easements may need to be developed in the event of the parcel being subdivided.
- Shared driveways shall be used to serve groups of buildings with shared parking courts between buildings
- Sharing arrangements shall be codified in covenants, deed restrictions, and developer agreements
- Connecting parking lots between development lots is encouraged to reduce traffic congestion on public streets

(4) Inappropriate Parking Management

- Individual parking lots by building, use, or development project
- Parking lots that are overbuilt and unused

(5) Site Circulation Guideline: A pedestrian friendly environment is encouraged to provide safe circulation within the site and between sites. Private development shall connect into existing public infrastructure.

- Concrete or similar material pedestrian walkways shall connect parking areas to building entrances
- Pedestrian walkways shall connect development sites to existing public sidewalks and paths such as the Urban Greenway
- Walkways shall be located outside of vehicle parking areas

- Walkways shall be safely illuminated

(6) Inappropriate Site Circulation

- Unpaved pedestrian walkways
- Parking lots with no pedestrian walkways
- Dark, unlit walkways

(7) Drive-Through and Service Guideline: Drive-throughs shall be located at the rear or side of buildings and shall not be placed between a public street/easement and the main building structure.

- Every effort shall be made to coordinate and integrate drive-through facilities into the overall architectural treatment of the main building
- Creative design solutions such as remote kiosks are encouraged to minimize the impact of the drive-through facility on the overall site design
- Clearly defined pedestrian crossings shall be provided where walkways intersect drive-through access lanes. In all cases drive-through facilities shall be designed to operate without endangering the public safety

(8) Inappropriate Drive-Through

- Drive-through window and service line visible from public streets

(9) Service, Storage, and Utility Areas Guideline: Service, loading, and utility areas, while necessary for business operation, shall be located away from the public view.

- Refuse and recycling areas shall be integrated into the building architecture with high quality design and/or screened with similar building materials to the main architecture. The areas shall be kept clean and avoid accumulation of excess disposal materials and the enclosure shall be locked
- When possible, service and utility areas shall be inside the building or integrated into the architecture of the building
- Loading areas shall be located behind buildings, away from parking areas and public streets
- Loading doors shall not be visible from public streets
- To reduce the amount of area dedicated to these needs, shared service areas between adjacent users and buildings shall be allowed and encouraged
- If indoor reuse is not possible, outdoor refuse areas shall be enclosed with attractive design and materials that match building architecture. Recommended enclosures would be of masonry or stone with gated access and surrounding landscaping. The gate and enclosure opening shall not be visible from public ways
- Rooftop mechanicals shall be screened with materials that match the building architecture

(10) Inappropriate Service Area

- Service areas and utility equipment along public streets
- Prefabricated storage sheds
- Visible garbage dumpsters
- Unscreened service, storage, or utility areas

(11) Landscaping Guideline: Create pedestrian-friendly landscapes that are functional, visually appealing, and relate to building activities and architecture. Use landscaping to screen undesirable views.

- Incorporate landscape elements that complement the character of the building and provide a pleasing relationship with adjoining properties, the public sidewalk, and the street
- Use appropriate landscape elements to establish continuity between buildings and to define the block face where there are no buildings
- Plant shade trees in surface parking lots to reduce heat islands
- Incorporate mid-level plantings and ground covers into parking planting areas

- Native plants shall be installed whenever possible
- Consider prairie plantings instead of turf to minimize maintenance and establish animal and insect habitat
- Use plant materials that provide year-round interest, add desirable color, texture, and smells to a site's experience
- Install plants to screen parking areas from public streets.
- Install plants to screen building mechanicals
- Incorporate rain gardens and bioretention basins to collect runoff and filter pollutants
- Install bioretention areas in surface parking lots for stormwater infiltration
- Use plant materials that are compatible with urban environments and will maintain their health with the expected amount of care
- Use salt tolerant plants due to the urban pedestrian environment

(12) Inappropriate Site Landscaping

- Single species planting schemes
- Residential or suburban landscape treatment such as boulders, bark chips, plastic edging, railroad ties, etc.
- Chain link, metal mesh, wire or barbed wire fencing
- Site furnishings not located adjacent to common spaces

(13) Urban Greenway Guideline: An urban greenway shall be established on the west edge of the planning area to both buffer new development from existing neighborhoods and provide an open space corridor for recreation and natural habitat.

- The urban greenway corridor shall be a 100 foot wide minimum public corridor
- Naturalized landscaping shall be installed to establish the natural space and buffer
- Low-maintenance native species including trees, shrubs and grasses shall be planted
- A 10 foot wide asphalt path may be constructed to provide an alternative transportation route and recreation trail
- The path shall connect to private development, public streets, and sidewalks

(14) Site Features and Urban Spaces Guideline: Private development is encouraged to establish urban spaces and install site features to enhance the Elmore Corners District and contribute to a pedestrian friendly streetscape.

- Commercial uses are encouraged to have public plaza spaces to provide space for activities such as outdoor dining or seating areas
- Commercial urban spaces shall be accessible from public ways
- Any security fencing shall consist of decorative materials (such as wrought iron, brick or stone)
- Select and locate site furnishings (benches, trash receptacles, bicycle racks, etc.) to unify the Elmore Corners District and provide a pleasing relationship with adjoining properties, the public sidewalk, and street
- Site furnishings shall be of high-quality materials that last in all-weather environments

(15) Inappropriate Site Features & Urban Spaces

- Urban gathering spaces that are closed off from public ways
- Site features of low quality that do not complement building architecture

(16) Public Streets and Streetscaping Guideline: Public streets shall be designed as complete streets serving all modes of transportation users including vehicles, bicycles, and pedestrians. Streets shall contain streetscape elements to enhance the visual aesthetics of these public spaces.

- Primary public streets shall contain streetscape elements of a vehicular scale including:
 - Street lights – tall poles to safely illuminate the street
 - Sidewalk on one side and bicycle path on the other side
 - Turf terraces

- District gateway features
- Wayfinding sign directing visitors to district destinations
- Light pole banners advertising special events
- Secondary streets shall contain streetscape elements of a pedestrian scale including:
 - Street lights – pedestrian scale with decorative fixtures
 - Concrete sidewalks on both sides of the street
 - Decorative brick, paver, or stamped concrete terraces
 - Wayfinding signs or kiosks directing visitors to district locations
 - Light pole banners advertising locations, uses, or events
 - Landscape planters with flowers and grasses
- All street lights shall contain LED energy efficient fixtures
- Street lights shall contain decorative poles and bases
- Street lights shall be down casting to reduce light pollution
- Stormwater management could be incorporated into the street terrace through stormwater planters, inlets, and swales
- Secondary streets may designate bicycle access through painted sharrows on street lanes

(17) Stormwater Management Guideline: All future developments must meet City Stormwater Ordinance requirements. In general, stormwater best management practices are encouraged and shall be incorporated into site designs, limiting large, monolithic retention ponds.

- Incorporate stormwater systems into development site plans and landscape plans
- Encourage regional ponds or ponds that serve multiple developments to density development and allow development to occur in urban form
- Utilize stormwater management techniques that maximize infiltration and filter runoff
- Incorporate bio-swales into parking lots to collect surface water and infiltrate on-site
- Install urban stormwater management systems where appropriate such as stormwater planters, rain gardens, underground cisterns and cells, porous pavement, and green street terraces
- Green roofs are encouraged to aid in reducing the amount of impervious surfaces
- Collect roof water into “purple pipe” grey water systems to reuse for irrigation, toilets or other non-potable uses
- If water does run off buildings, direct it into planting areas and rain barrels
- Include stormwater pond aeration systems and fountains to improve water quality and appearance
- Inappropriate Stormwater Management
- Direct stormwater runoff into the Pheasant Creek
- Large, unnaturally shaped stormwater detention ponds with no landscaping or aeration

B. Appeals. Appeals of any administrative decision made under this section shall be made in writing by the owner(s) of record of the property to the City Design Review Board no less than thirty (30) days prior to any rescheduled meeting of the Board. Appeals of any decision of the Design Review Board shall be made in writing by the owner(s) of record of the property to the City Council no less than thirty (30) days prior to the scheduled Council meeting.

City of Davenport
Plan and Zoning Commission

Department: Community Planning & Economic Development
Contact Info: Matt Flynn 888-2286

Date
3/6/2018

Subject:

Case No. P18-02: Preliminary plat Seng Meadows located west of Northwest Boulevard and north of 46th Street containing 58 residential lots and one outlot. [Ward 7]

ATTACHMENTS:

Type	Description
▣ Backup Material	P18-02 Seng Meadows packet

Staff Workflow Reviewers

REVIEWERS:

Department	Reviewer	Action	Date
City Clerk	Wille, Wayne	Approved	3/2/2018 - 11:08 AM



City of Davenport
Community Planning & Economic Development Department
PREVIEW REPORT

PLAN AND ZONING COMMISSION

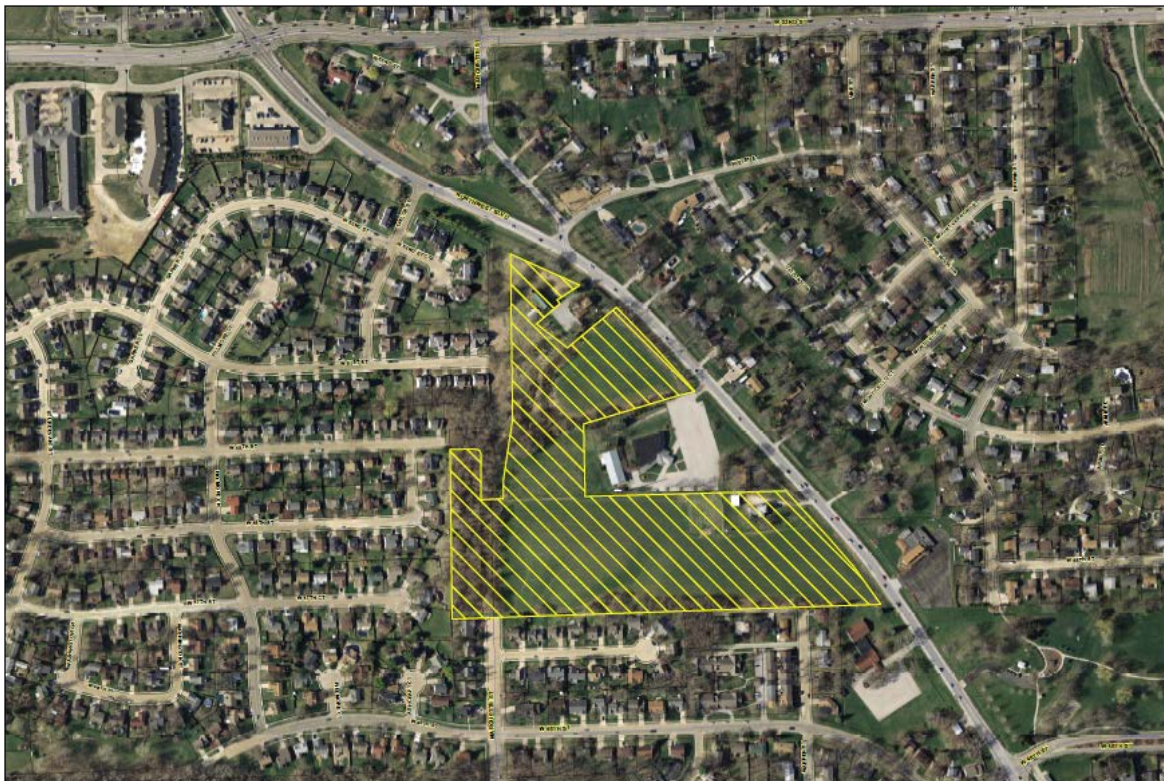
Preview Date: March 06, 2018
Request: P18-02 Preliminary Plat – Seng Meadows
Address: West of Northwest Blvd & North of 46th St
Applicant: Seng Meadows LLC

INTRODUCTION

Request of Seng Meadows LLC for a preliminary plat of 20.9 acres, more or less, located west of Northwest Boulevard and south of Wisconsin Avenue and north of 46th Street, containing fifty-eight (58) residential lots. The property is zoned “R-3” Moderate Density Dwelling District. [7th Ward]

Recommendation: This is a preview report. No recommendation is made at this time.

Aerial Photo:

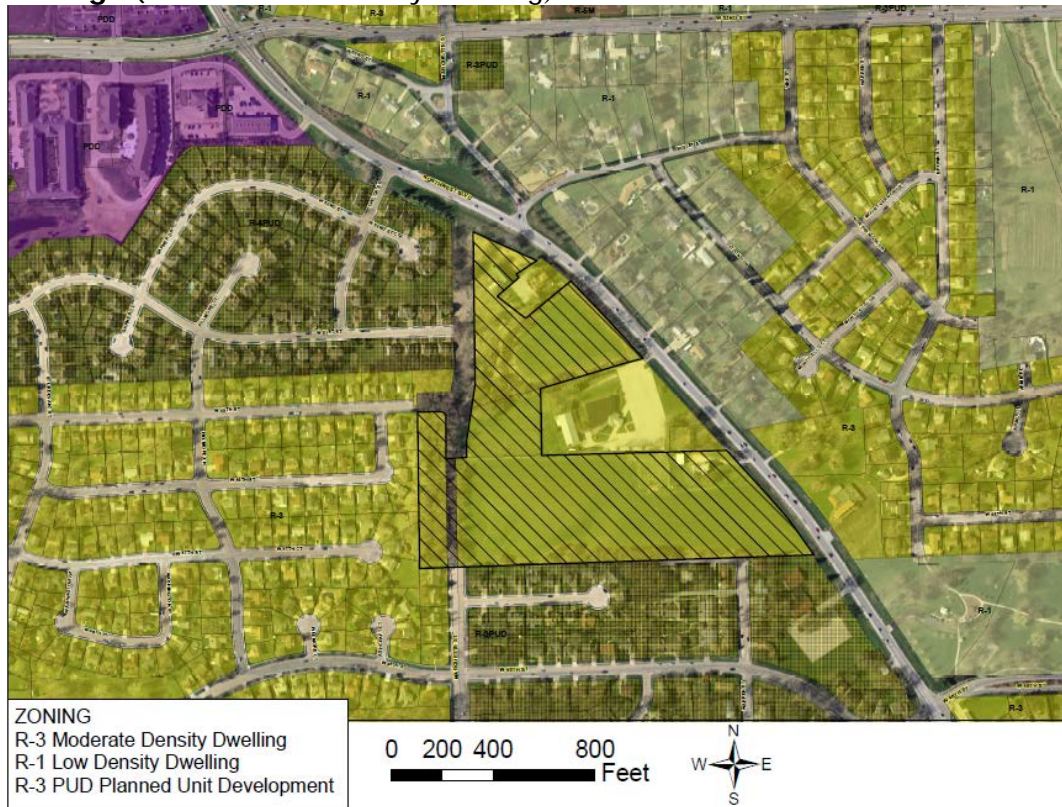


Aerial Photo 2014

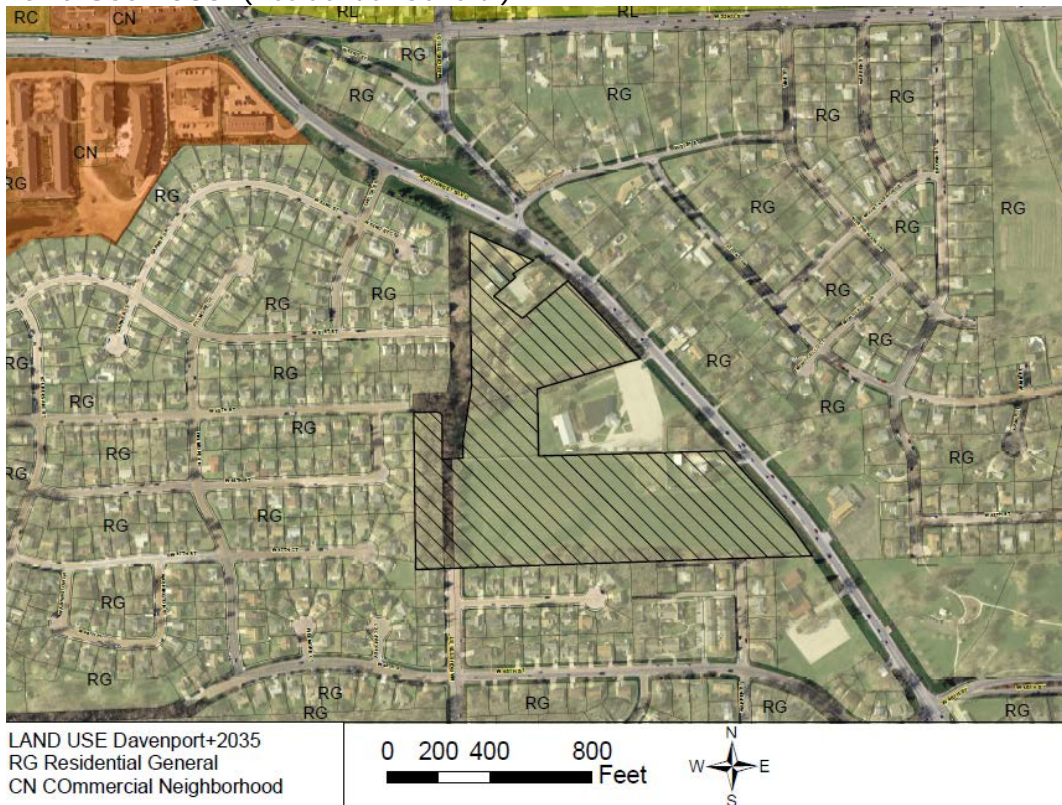
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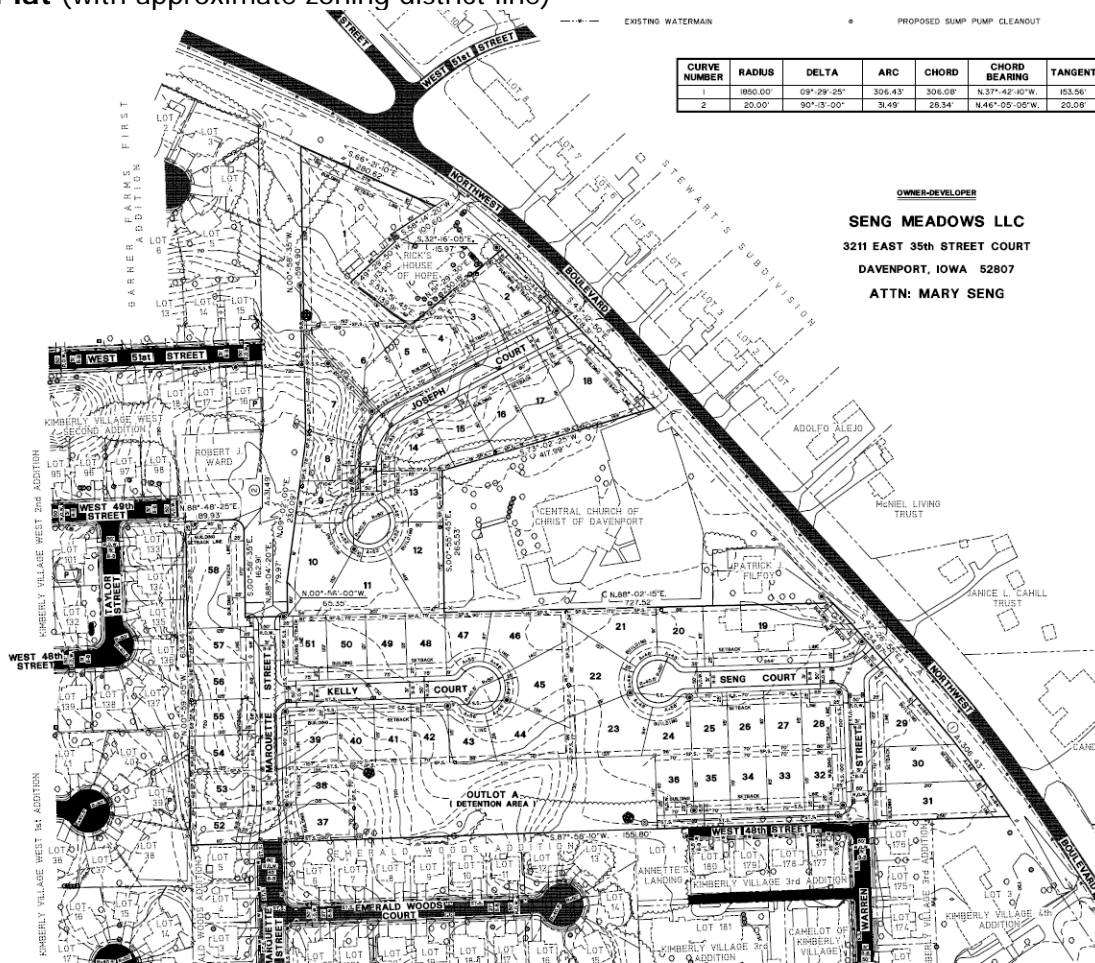
Zoning (R-3 Moderate Density Dwelling)



Land Use 2035 (Residential General)



Plat (with approximate zoning district line)



BACKGROUND

Comprehensive Plan:

Within Urban Service Area (USB35): Yes
 Proposed Land Use Designation: RG - Residential General

Residential General (RG) - Designates neighborhoods that are mostly residential but include, or are within one-half mile (walking distance) of scattered neighborhood-compatible commercial services, as well as other neighborhood uses like schools, churches, corner stores, etc. generally oriented along Urban Corridors (UC). Neighborhoods are typically designated as a whole. Existing neighborhoods are anticipated to maintain their existing characteristics in terms of land use mix and density, with the exception along edges and transition areas, where higher intensity may be considered.

Relevant *Davenport+2035* Goals and Objectives:

1. *Strengthen the existing built environment.*
 - b. *Reduce the number of underoccupied, abandoned, or vacant buildings / properties through adaptive reuse and infill.*
2. *Identify and reserve land for future development*

Technical Review:

Streets. Marquette is proposed to extend into the proposed plat but not the length of the plat. Three cul-de-sacs are also proposed. There already exists a Kelly Court in Irish Woods Additions in the approximately same alignment. To differentiate, the developer may want to consider naming it West Kelly Court or since it aligns with the proposed Seng Court it should carry the same name.

16.24.040 Streets.

A. The arrangement of street rights-of-way in new subdivisions shall make provisions for the extension of the existing system of street rights-of-way (or their proper projection where adjacent property is not subdivided) insofar as they may be necessary for public requirements. The street and alley right-of-way arrangements shall also be such as to cause no hardship to owners of adjacent property when they plat their own land and seek to provide for convenient access to it.

Storm Water. There is no existing stormwater infrastructure in the general area. Storm drainage is overland to the ditch line along Northwest Boulevard. Storm water infrastructure will need to be extended into the area. Detention is proposed along the south boundary of the proposed plat.

Sanitary Sewer. Sanitary sewer service is located in Marquette Street and 48th Street adjacent to the plat. Sanitary is also located in 49th and 51st Street along the west boundary of the property.

Other Utilities. This is an urban area and normal utility services are available.

Parks/Open Space. The proposed plat does not impact any existing or planned parks or public open spaces.

PUBLIC INPUT This is a subdivision plat. No notification is required.

DISCUSSION

The following is excerpted from the City Code containing the ordinance language for a preliminary plat:

16.16.020 Submission of preliminary plat — Contents.

A. The location of present property lines, section lines, easements, corporate limits and other legally established districts, streets, buildings, watercourses, tree masses and other existing features within the area to be subdivided and similar facts regarding existing conditions on land immediately adjacent thereto;

B. The proposed location and width of streets, alleys, lots, building setback lines and easements, if any;

C. Existing sanitary and storm sewers, water mains, culverts and other underground structures within the tract and immediately adjacent thereto. The location and size of the nearest water main and sewer or outlet are to be indicated in a general way upon the plat;

D. Proposed name of the subdivision (which shall not duplicate any previously filed plat), the name of the land owner, land developer, and land surveyor;

E. The names and adjoining boundaries of all adjacent subdivisions and the names of record owners of adjoining parcels of unsubdivided land;

F. Existing contours with intervals of five feet or less;

G. North point, scale and date;

H. A vicinity sketch showing the proposed subdivision in relationship to surrounding development and street systems.

16.16.030 *Conformance with minimum standards.*

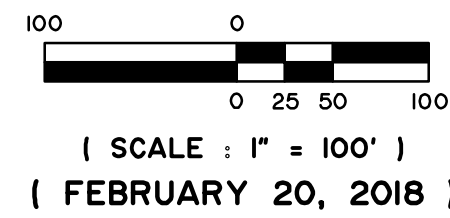
The city plan and zoning commission shall study such preliminary plat to see if it conforms with the minimum standards and requirements as outlined in this chapter. The approval of the preliminary plat does not constitute acceptance with the subdivision by the city, but is deemed to be an authorization to proceed with the preparation of the final plat.

STAFF RECOMMENDATION

This is a preview report. No recommendation is made at this time.

Prepared by:

Wayne Wille, CFM – Planner II
Community Planning Division



LEGEND

- | | | | |
|---------------|------------------------------------|-----------|---------------------------------|
| — 180 — | EXISTING CONTOURS (INDEX) | — S.S. — | PROPOSED SANITARY SEWER |
| — 178 — | EXISTING CONTOURS (INTERMEDIATE) | ⊙ | PROPOSED SANITARY SEWER MANHOLE |
| --- S.S. --- | EXISTING SANITARY SEWER | — ST.S. — | PROPOSED STORM SEWER |
| ⊙ | EXISTING SANITARY SEWER MANHOLE | ⊗ | PROPOSED CATCH BASIN |
| --- ST.S. --- | EXISTING STORM SEWER | ⊢ | PROPOSED FLARED END SECTION |
| ⊗ | EXISTING CATCH BASIN | ⊗ | PROPOSED RIP - RAP |
| ▨ | EXISTING PAVEMENT | ⊙ | PROPOSED DETENTION OUTFALL |
| ⊙ | EXISTING FIRE HYDRANT | — SP.S. — | PROPOSED SUMP PUMP SEWER |
| — W. — | EXISTING WATERMAIN | ⊙ | PROPOSED SUMP PUMP CLEANOUT |

CURVE NUMBER	RADIUS	DELTA	ARC	CHORD	CHORD BEARING	TANGENT
1	1850.00'	09°-29'-25"	306.43'	306.08'	N.37°-42'-10"W.	153.56'
2	20.00'	90°-13'-00"	31.49'	28.34'	N.46°-05'-05"W.	20.08'

OWNER-DEVELOPER

SENG MEADOWS LLC

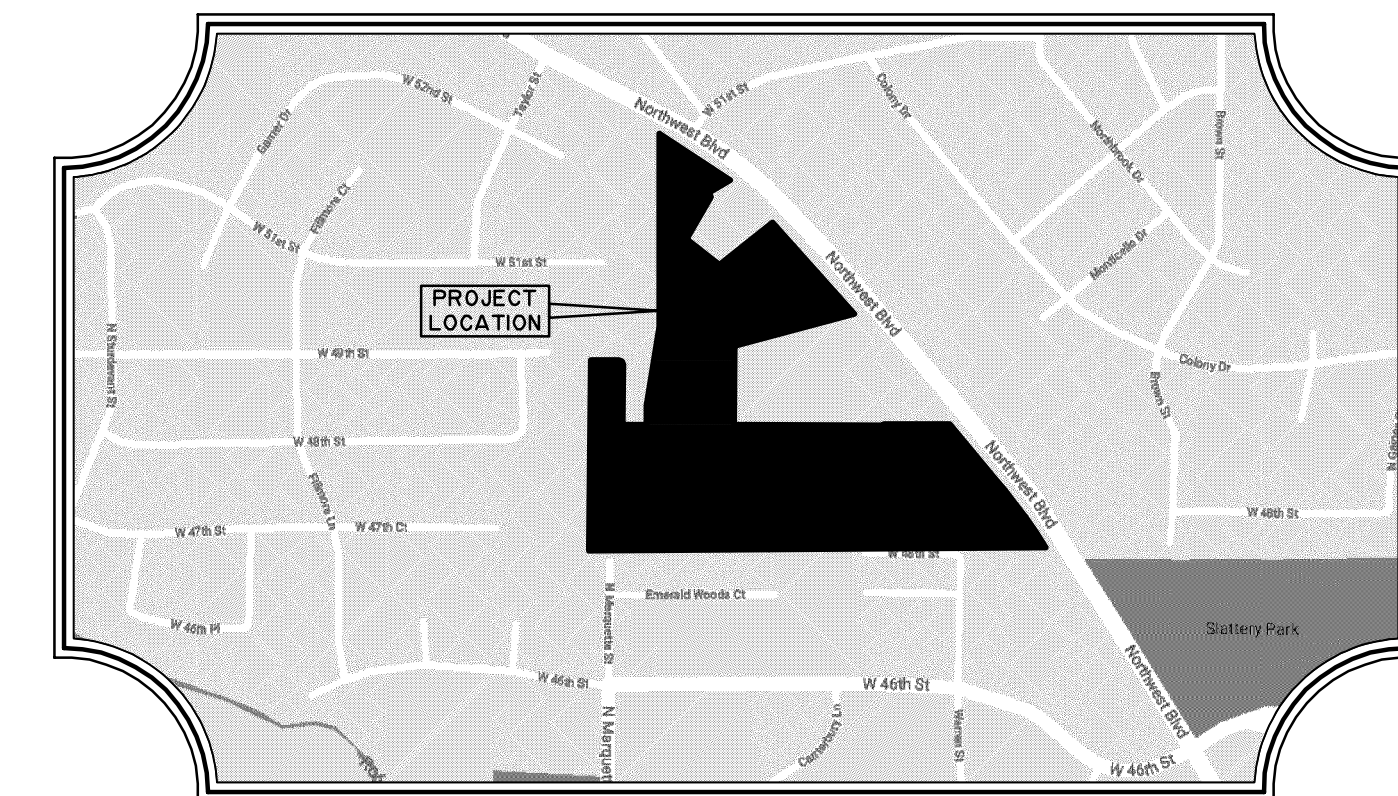
3211 EAST 35th STREET COURT

DAVENPORT, IOWA 52807

ATTN: MARY SENG

PRELIMINARY PLAT OF SENG MEADOWS AN ADDITION TO THE CITY OF DAVENPORT, IOWA

BEING PART OF THE SOUTHEAST QUARTER OF SECTION 10 AND
PART OF THE SOUTHWEST QUARTER OF SECTION 11
TOWNSHIP 78 NORTH, RANGE 3 EAST OF THE 5th P.M.
AND IN PART BEING A REPLAT OF OUTLOT 2
EMERALD WOODS ADDITION



LOCATION MAP

ALL IMPROVEMENTS ARE TO BE INSTALLED IN ACCORDANCE WITH THE LATEST CITY OF DAVENPORT STANDARD SPECIFICATIONS AND DETAIL DRAWINGS.

ALL INTERIOR LOT DIMENSIONS ARE APPROXIMATE.

TOPOGRAPHY WAS DONE BY OTHERS.

SUBDIVISION CONTAINS 20.9 ACRES, MORE OR LESS.

FRONT YARDS WHERE SANITARY IS SHOWN SHALL HAVE A 15 FOOT SANITARY SEWER & UTILITY EASEMENT. FRONT YARDS WITHOUT SANITARY SEWER SHALL HAVE A 15 FOOT WATERMAIN & UTILITY EASEMENT. SIDE LOT LINES WITH SANITARY AND STORM SEWER SHALL HAVE A 30 FOOT SEWER & DRAINAGE EASEMENT. SIDE YARD WITH SUMP PUMP SEWER SHALL HAVE A 10 FOOT DRAINAGE EASEMENT CENTERED ON LOT LINE.

PROPOSED ZONING IS R-3 (MODERATE DENSITY DWELLING DISTRICT).

ALL SANITARY SEWER SHALL BE 8 INCH DIAMETER.

ALL WATERMAIN SHALL BE 8 INCH DIAMETER DUCTILE IRON PIPE.

ALL STORM WATER DETENTION SHALL BE DESIGNED AT THE TIME OF FINAL PLATTING.

THE CITY OF DAVENPORT CURRENTLY OWNS A 40 FOOT STRIP OF MARQUETTE STREET RIGHT OF WAY THAT IS INCLUDED HEREIN. LOTS 52 THROUGH 58 UTILIZE PART OF THAT RIGHT OF WAY.

SURVEYOR

DAVID L. MEYER

VERBEKE - MEYER CONSULTING ENGINEERS, P.C.

4111 EAST 80th STREET

DAVENPORT, IOWA 52807

PREPARED BY

VERBEKE - MEYER

CONSULTING ENGINEERS, P.C.

4111 EAST 80th STREET

DAVENPORT, IOWA 52807

PHONE NUMBER: (563) 359 - 1348

VMCE 16322 - PRELIMINARY PLAT

**CITY OF DAVENPORT
COMMUNITY PLANNING & ECONOMIC DEVELOPMENT (CPED)
SUBDIVISION REFERENCE FILE**

PRELIM / **FINAL** / **PUD** (circle the appropriate designation)

SUBDIVISION NAME: SENG MEADOWS

LOCATION: 4800 Block of Northwest Boulevard, Davenport

DEVELOPER: Name: Seng Meadows, LLC
Address: 3211 East 35th Street Court, Davenport, IA 52807
Phone: 563-359-4662 FAX: 563-355-4005
Mobile Phone: _____ Email: LLevetzow@melfosterco.com

ENGINEER: Name: Verbeke-Meyer Consulting Engineers, P.C.
Address: 4111 East 60th Street, Davenport, Iowa 52807
Phone: 563-359-1348 FAX: 563-359-3295
Mobile Phone: _____ Email: dlim@verbeke-meyer.com

ATTORNEY: Name: Theodore J. Priester Law Firm
Address: 601 Brady Street Suite 220 Davenport, IA 52803
Phone: 563-322-5386 FAX: 563-322-6222
Mobile Phone: _____ Email: tpriester@priesterlaw.com

OWNER: Name: Seng Meadows, LLC
Address: 3211 East 35th Street Court Davenport, IA 52807
Phone: 563-359-4663 FAX: 563-355-4005
Mobile Phone: _____ Email: LLevetzow@melfosterco.com

NUMBER OF LOTS: 58 ACRES: 20.9
SF 2F MF & EST. UNITS COMM IND

STREETS ADDED: 2240 LINEAR FEET

Does the plat contain a drainage way or floodplain area: Yes X No

Fee per Plat	Fee
Ten or fewer lots (< 10 lots)	\$400 plus \$25/lot
Eleven to twenty-five lots (≥ 11 lots ≤ 25 lots)	\$700 plus \$25/lot
More than twenty-five lots (> 25 lots)	\$1,000 plus \$25/lot
Reforestation fee (submit to Land Use Office Prior to City Council review)	\$150 per 50 feet of lineal lot frontage

**NOTE: THE PLAT WILL NOT BE PROCESSED UNTIL THE FEE AND COMPLETED REFERENCE FILE
ARE RECEIVED BY THE PLANNING AND LAND DEVELOPMENT OFFICE.**

City of Davenport
Plan and Zoning Commission

Department: Community Planning & Economic Development
Contact Info: Matt Flynn 888-2286

Date
3/6/2018

Subject:

Time open for citizens wishing to address the Commission on matters not on the established agenda

Staff Workflow Reviewers

REVIEWERS:

Department	Reviewer	Action	Date
City Clerk	Wille, Wayne	Approved	3/2/2018 - 11:07 AM

City of Davenport
Plan and Zoning Commission

Department: Community Planning & Economic Development
Contact Info: Matt Flynn 888-2286

Date
3/6/2018

Subject:

Note: Pursuant to §17.60.030 and §2.64.120 of the Davenport City Code the Commission is required to act on this item within 30 days unless the petitioner waives this requirement. Pursuant to the city code if the Commission does not act and report on this item within 30 days' time this agenda item is to be construed as approved by the Commission.

Note: The Plan and Zoning Commission meeting is not a public hearing. It is time for the commission to discuss the issue(s) with City staff and if questions rise, with the developer. A rezoning or ordinance text amendment has a second public hearing before the City Council at its Committee of the Whole meeting. Notification of that meeting will be sent to surrounding owners following the Plan and Zoning Commission meeting.

Next Public Hearing/Regular Plan & Zoning Meeting:

Tuesday, March 20, 2017 at 5:00 P.M. in the Council Chambers of Davenport City Hall 226 West 4th St

Staff Workflow Reviewers

REVIEWERS:

Department	Reviewer	Action	Date
City Clerk	Wille, Wayne	Approved	3/2/2018 - 11:08 AM